

Notes:

- General
1. All development is to be undertaken in accordance with the Development Approval.
 2. Lot boundaries are to be indicated on the Plan of Development.
 3. The maximum height of buildings shall be in the order of 9 metres.

Setbacks

4. Setbacks are as per the Plan of Development Table unless otherwise specified.
5. Built to boundary walls are optional. The location of the built to boundary walls are indicated on the Plan of Development. Where built to boundary walls are not adopted side setbacks shall be in accordance with the Plan of Development Table.
6. Built to boundary walls are to have a maximum length of 15 metres and a maximum height of 3.5 metres.
7. Boundary setbacks are measured to the wall of the building or edge of balcony.
8. Boundary setbacks are to be built to the boundary of the lot.
9. First floor setbacks on single and multi family allotments must not exceed the minimum ground floor setbacks.
10. Garages must not be setback a minimum of 4.5m from the front property boundary.
11. One on-street car parking space is provided per dwelling.

Site Cover and Amenity

12. Site cover for each lot is not to exceed 70% of the lot.
13. Dwelling with their main living areas located at ground level should have a minimum area of private open space consisting of at least 12m² with a minimum width of 2.4 metres.
14. Dwelling with their main living areas located at ground level should have a minimum area of private open space consisting of a balcony or roof area open to the sky with a minimum area of 5m² and a minimum dimension of 1.2 metres, directly accessible from a living room.

Parking for Multiple Residential developments

- 1 space per dwelling unit on-site
- 0.5 Visitor Space per dwelling unit on-site
- Do not dominate the streetscape

Private Open Space for Multiple Residential developments

- Private Open Space for Multiple Residential developments is to be a minimum of 25% of the area of the site and must provide:
- a minimum dimension of 4.0 m
- a maximum gradient not exceeding 1 in 10
- must be subject to informed surveillance from the dwelling separated from any other private areas by a fence or landscaping

Loft Apartments

15. Each apartment must have a clearly identifiable and enclosed entrance.
16. Incorporated in the design of the main dwelling and private open space areas of the main dwelling.
17. Windows with a minimum sill height of 1.5 metres or privacy screening.
18. Low maintenance building materials and non-reflective finishes.
19. No external drainage or other pipes.
20. Incorporate an area of private open space such as a balcony or verandah with a minimum depth of 1.2 metres and a minimum area of 5m², preferably overlooking the lane, and including a screened drying area.
21. Incorporate at least 2 openings to all habitable rooms, including internal windows and bouver windows to facilitate cross ventilation.
22. Fencing on all Street Frontages, to be 50% transparent or not to exceed 1.2 metres in height.

Definitions:

- A dwelling built above a garage with its own entry door at ground level.
- Allotments serviced by a laneway.

Where Multiple Residential developments encompass more than one lot they are to be designed to address each street frontage

- Multiple Residential sites are not permitted to be developed with only one single dwelling.
- Maximum No. of Dwelling Units per Multiple Residential Site:
 - 450m² - 599m² - Maximum 3 Dwellings
 - 600m² - 749m² - Maximum 4 Dwellings
 - 750m² - Maximum 5 Dwellings

PLANS AND DOCUMENTS referred to in the ULDA APPROVAL dated 21/11/11

Indicative Future Development



Plan of Development Table		Premium Villa Allotments 12 & 12.5m		Courtyard Allotments 14-16m		Traditional Allotments 16m+	
		Ground Floor	First Floor	Ground Floor	First Floor	Ground Floor	First Floor
Front/Primary Frontage	2.4	2.4	2.4	2.4	2.4	2.4	2.4
Garage	4.5	n/a	4.5	n/a	4.5	4.5	n/a
Rear	1.0	1.0	1.0	1.0	1.0	1.0	1.0
Side - General Lots							
Built to Boundary	0.0	1.0	0.0	1.0	0.0	0.0	1.0
Non Built to Boundary	0.9	0.9	1.0	1.0	1.0	1.0	1.5
Corner Lots - Secondary Frontage	1.5	1.5	1.5	1.5	1.5	1.5	1.5
Laneway Lots							
Rear of Lot (from laneway boundary)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Garage (from laneway boundary)	0.0	0.0	0.0	0.0	0.0	0.0	0.0
On site parking requirements (minimum)	1 space to be covered and enclosed	1 space to be covered and enclosed	1 space to be covered and enclosed	1 space to be covered and enclosed	1 space to be covered and enclosed	1 space to be covered and enclosed	1 space to be covered and enclosed
Garage location							
Garage location	Single or tandem garage acceptable.	Single, tandem or double garage acceptable.	Single, tandem or double garage acceptable.	Single, tandem or double garage acceptable.	Single, tandem or double garage acceptable.	Single, tandem or double garage acceptable.	Single, tandem or double garage acceptable.
Garage location	Double garages are only permitted on laneway or two storey allotments.	Double garages are only permitted on laneway or two storey allotments.	Double garages are only permitted on laneway or two storey allotments.	Double garages are only permitted on laneway or two storey allotments.	Double garages are only permitted on laneway or two storey allotments.	Double garages are only permitted on laneway or two storey allotments.	Double garages are only permitted on laneway or two storey allotments.
Garage location	Garages are to be located along the built to boundary wall.	Garages are to be located along the built to boundary wall.	Garages are to be located along the built to boundary wall.	Garages are to be located along the built to boundary wall.	Garages are to be located along the built to boundary wall.	Garages are to be located along the built to boundary wall.	Garages are to be located along the built to boundary wall.
Site Cover (maximum)	70%	70%	70%	70%	70%	70%	70%

A MENDED IN RED 1 & NOV 2011

By: Development (frame) of the ULDA

Legend	
General Details	
Red line	Site Boundary
Blue line	Proposed Stage Boundary
Green line	Proposed Sub-Stage Boundary
Blue dot	Potential Lot Location
Blue star	Proposed Multi-Family Dwelling (Indicative Future Development)
Road	
Orange line	Indicative Footpath Location
Open Space	
Green area	District Park, Green Links & Stormwater Management
Blue area	50m Bundamba Creek Buffer
Allotment Details	
Blue dashed line	Optional Built to Boundary Wall
Blue dashed line	No Vehicle Access
Blue dashed line	Primary Frontage

REVISION

A: 18/11/10 AMEX LOT LAYOUT

B: 07/03/11 AMEX LOT LAYOUT

Note

All dimensions and areas are approximate only, and are subject to survey and council approval.

Dimensions have been rounded to the nearest 0.1 metres.

The boundaries shown on this plan should not be used for final detailed engineers design.

Boundaries taken from DCDB.

Design contours supplied by Cardno.

CLIENT

AMEX

PROJECT

SECONDARY URBAN CENTRE EAST

PROPOSED LAYOUT

STAGE 1

PLAN OF DEVELOPMENT

Drawn

1 MARCH 2011

Drawn By

KCH / NW

Drawn Name

8837-121 PRO STAGE 1

Local Authority

PSMCH CITY COUNCIL

Approved Authority

URBAN LAND DEVELOPMENT AUTHORITY

Locality

RIPLEY

Job Reference

8837-121

Scale

1 : 1000

Sheet

A1

Plan Ref

6837 - 127

Rev

B

RPS

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LAND USE BUDGET STAGE 1		
Land Use Budget	Area (ha)	Percentage
Area of Stage 1	17.689	100.00%
Saleable Area		
Area of Residential Lots	9.106	51.48%
Area of Sales & Convenience Centre	0.553	3.13%
Total Area of Allotments	9.659	54.60%
Roads		
Trunk Collector Road	2.801	15.83%
Local Roads	3.323	18.79%
Total Area of New Road	6.124	34.62%
Open Space		
Area of District Park, Green Links & Stormwater Management	1.906	10.78%
Total Open Space	1.906	10.78%
Road Type	Length (m)	
Total Length of New Road	3305	
6.5m wide lane - 5.5m pavement	253	
12.25m wide road - 8m pavement	606	
14m wide road - 6.5m pavement	791	
16.5m wide road - 8m pavement	657	
17.7m wide road - 8m pavement	101	
23.5m wide road - 10m pavement (plus parking bays)	364	
36.5m wide road - 2 x 11.5m pavement (plus parking bays)	513	



DEVELOPMENT STATISTICS STAGE 1			
Allotment Type	Typical Lot Dimensions	No. of Lots	Percentage
Standard Allotment	10.5m x 32m	8	4.19%
Villa Allotment	12.5m x 32m	12	6.28%
Premium Villa Allotment	15m x 32m	69	36.13%
Courtyard Allotment	18m x 32m	43	22.51%
Premium Traditional Allotment	18m x 32m	132	69.11%
Sub Total			
Sub Total		132	69.11%
Superslot			
Villa Allotment	10.5m x 32m	2	1.05%
Lane-way Villa Allotment	10.5m x 32m	11	5.76%
Premium Villa Allotment	12.5m x 32m	0	0.00%
Lane-way Premium Villa Allotment	12.5m x 32m	29	15.18%
Courtyard Premium Villa Allotment	15m x 32m	15	7.85%
Premium Traditional Allotment	18m x 32m	2	1.05%
Sub Total		59	30.89%
Total		191	100.00%
Density Calculation			
Net Residential Density - 206 dwellings (including residential lots, superslots & multi-family dwellings (duplex) and lodges) / 12216ha (area of local roads and residential lots)			16.86

Note
All dimensions and areas are approximate only, and are subject to survey and council approval. Dimensions have been rounded to the nearest 0.1 metres.
The boundaries shown on this plan should not be used for final detailed engineers design.
Boundaries taken from DCOB. Design contours supplied by Carriro.

CLIENT
AMEX

PROJECT
SECONDARY URBAN CENTRE EAST

PROPOSED LAYOUT
STAGE 1
ALLOTMENT LAYOUT

Level Datum

Origin

Date
1 MARCH 2011

Drawn By
WMM / KCH

DWG Name
8837-121 PRO_STAGE 1

Local Authority
IRSWICH CITY COUNCIL

Approval Authority
URBAN LAND DEVELOPMENT AUTHORITY

Locality
RPLEY

Job Reference
8837-UD

Scale
1 : 1000

Sheet
A1

Plan Ref
6837 - 121

Rev
E



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PLANS AND DOCUMENTS
referred to in the ULDA
APPROVAL dated 21/11/11

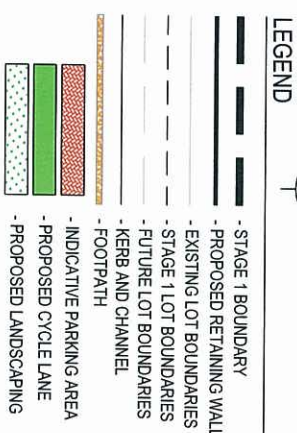
Indicative
Future
Development



FOR DISCUSSION

LEGEND

	50m Bundamba Creek Buffer		Sales Office Precinct		Proposed Detention Basin	1	Sales Office Precinct
	Ripley Valley Boulevard Feature Street Tree		Low Density Residential		Bio-Retention Basin	2	Main Boulevard
	Urban Core / Low Density Street Tree		Dry Creek Bed Swale		Overland Flow Paths	3	District Park
	Proposed Open Space Pathways		Half Court			4	Pocket Park
	Proposed Driveway/Carparks		Open Kick-a-bout Space			5	Open Space (Informal)



SCALE 1:1000 @ A1



Vertical Geometry Grade (%)
Vertical Geometry Length (m)

DATUM RL 39.000

PLANS AND DOCUMENTS	46 194
referred to in the ULDA	
APPROVAL dated 21/11/11	

RIPLEY ROAD - INTERIM UPGRADE WORKS

SCALE: HORIZONTAL - 1:1000

VERTICAL - 1:100



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Quality Endorsed Company	C.I.	24/02/2011
Verified DT	Date	24/02/2011
Approved	RPEQ: 7990	Date

Customer	Client	AMEX PTY. LTD.
B.V	24/02/2011	
Checked D.M	Date 24/02/2011	
Designed C.P	Date 24/02/2011	
Verified	Date	
DT	24/02/2011	
Approved D.T	REC 7360 Date 24/02/2011	
RIPLEY VALLEY - SUCE STAGE 1 - TRUNK ROAD WORKS LAYOUT PLAN & ROAD LONGITUDINAL SHEET 1 OF 3		

NOT FOR CONSTRUCTION

Drawing Number	22/02/2011	A.N.D.	AS SHD
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7901.47.012-111



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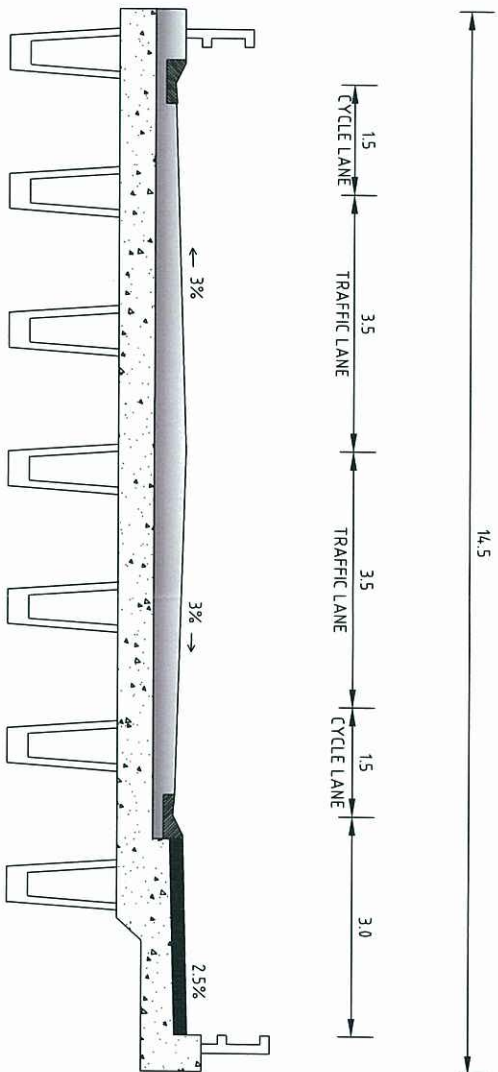
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Drawn	Date	Client	Status
B.V.	24/02/2011	AMEX PTY. LTD.	NOT FOR CONSTRUCTION
Checked	Date		Date
D.M	24/02/2011	RIPLEY VALLEY - SUCE	22/02/2011
Designed	Date	STAGE 1 - TRUNK ROAD WORKS	Drawn
C.P	24/02/2011		A.H.D.
Verified	Date		Scale
DT	24/02/2011		AS SHOWN
Approved	PERC. 7950	INTERIM BRIDGE CROSS SECTION	Size
D.T	24/02/2011		A1
			Revision
			—
			7901.47.012-141



SECTION 12
1:50

TYPICAL BRIDGE SECTION
15.0m WIDE
(BARRAMS ROAD)

PLANS AND DOCUMENTS referred to in the ULDA APPROVAL dated 21/11/11



20.5M SUBARTERIAL ENTRY BOULEVARD WITH PARKING - INTERIM.
(BARRAMS ROAD)

PLANS AND DOCUMENTS referred to in the ULDA APPROVAL dated 21/11/11

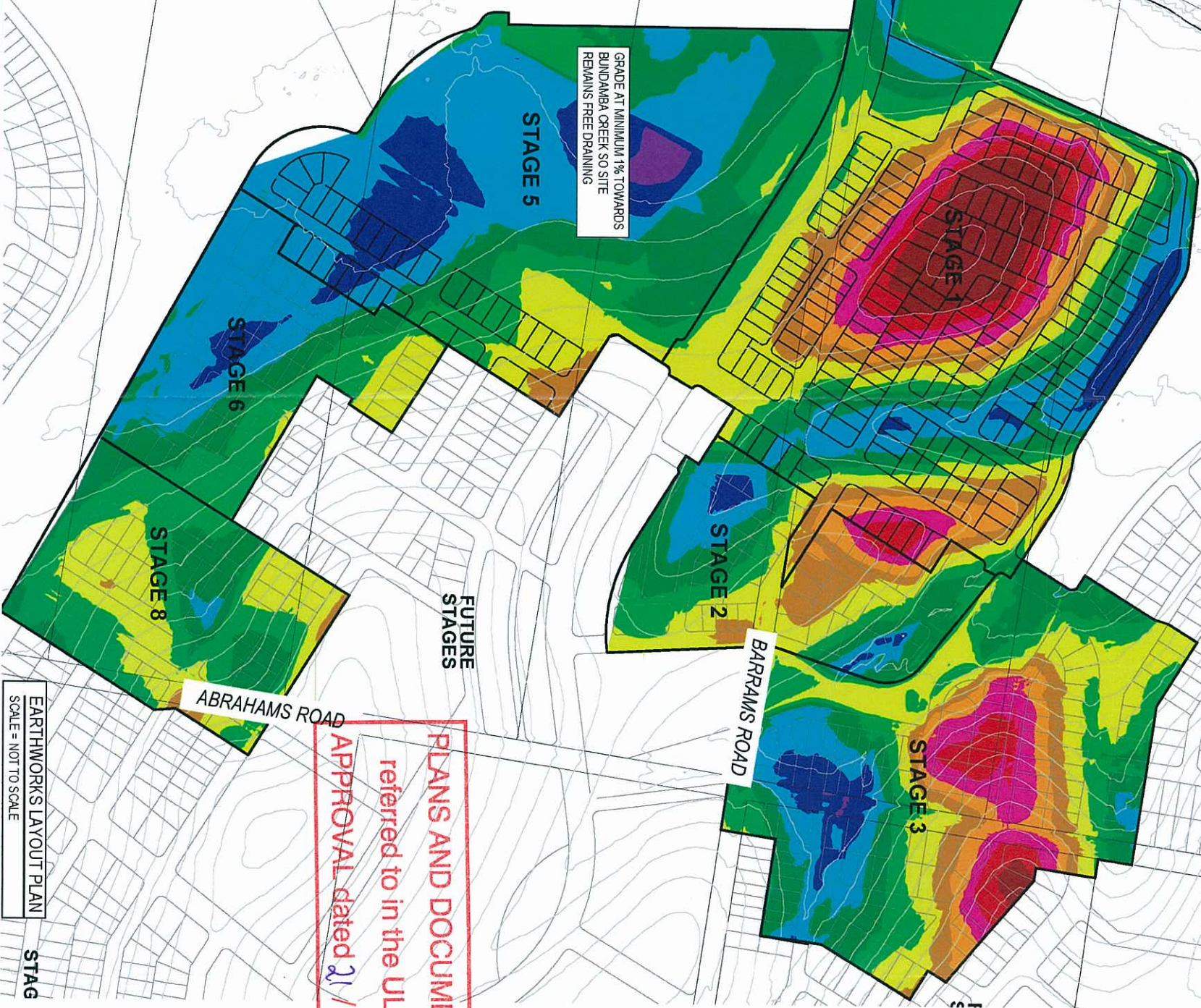
0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150mm

ON A1 ORIGINAL

FUTURE STAGES

**Amended in Red
Works are only to take place within
Lots 59-61 on \$151855 - Any works
shown outside these lots on this plan
do not form part of this approval**

14/11/11



PLANS AND DOCUMENTS referred to in the ULDA APPROVAL dated 21/11/11

EARTHWORK VOLUMES (m³)		
STAGE	Cut	Fill
1	260,000	112,000
2	20,600	42,000
3	75,000	65,000
5	6,000	242,000
6	1,000	77,000
8	11,000	59,000
TOTAL	373,600	597,000

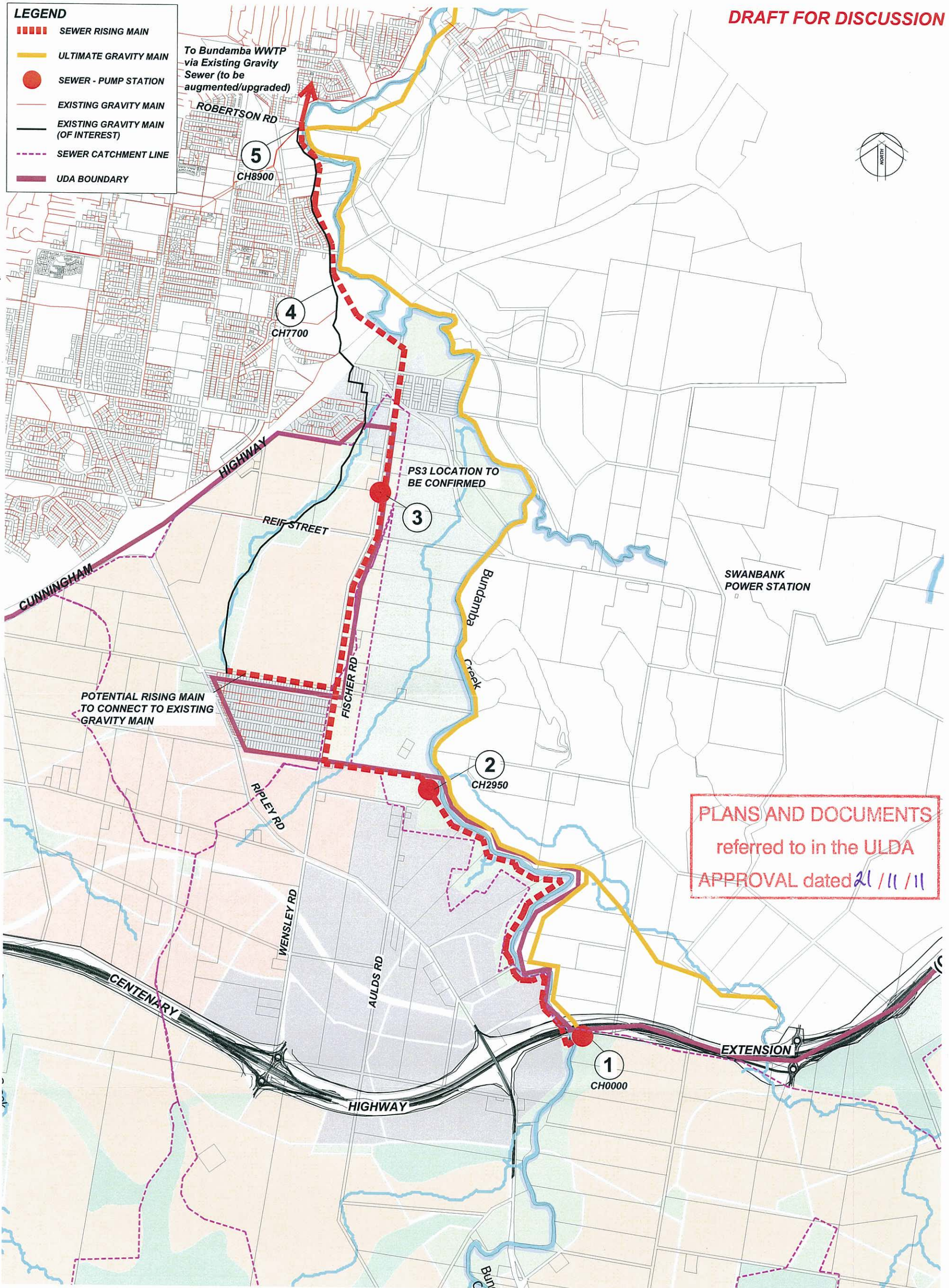
EXCESS CUT FROM STAGE 1 TO BE
PLACED IN STAGES 2, 3, 5, 6 AND/OR
8 TO LEVEL 1 COMPACTION

LEGEND	
Depth +5m	
Depth 4m to 5m	
Depth 3m to 4m	
Depth 2m to 3m	
Depth 1m to 2m	
Depth 0m to 1m	
Depth -1m to 0m	
Depth -2m to -1m	
Depth -3m to -2m	
Depth -4m to -3m	
Depth -5m to -4m	
Depth -5m	

EXISTING CONTOURS SHOWN @ 2m INTERVALS

CONCEPT PLAN
FOR REVIEW ONLY

1. Earthwork volumes detailed on this sheet are indicative only and are subject to change upon completion of detail design and the inclusion of road boxing, trench spoil and bulking factor.
2. A detailed Erosion and Sediment Control Management Plan is to be developed and implemented for stages 1, 2, 3, 5, 6 & 8 prior to any earthworks being undertaken in these areas.
3. The Erosion and Sediment Control Management Plan will ensure Bundamba Creek and surrounding waterways are not adversely affected during, or as a result of, earthwork operations.
4. Areas filled to final level will be stabilised and vegetated as per the Erosion and Sediment Control Management Plan to ensure no bare areas are left exposed.
5. Any filling outside of Stage 1 will be graded to ensure no trapped low points and that the area remains free draining.



RIPLEY VALLEY

INTERIM WASTE WATER OPTIONS RISING MAIN SOLUTION

NOT TO SCALE

The contents of this plan are conceptual only. All areas and dimensions are approximate and subject to relevant studies, Survey, Engineering and Council approval.

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REF NUMBER: 790147/006/SK-Rising Main.1
DATE: 28/09/2011