

OVERALL LAND USE BUDGET		
Land Use Budget	Area	Percentage
Area of Stages 1-3	17.474 ha	100.0%
Saleable Area		
Area of Residential Lots	8.870 ha	50.8%
Area of Sales & Convenience Centre	0.589 ha	3.4%
Total Area of Allotments	9.459 ha	54.1%
Roads		
Trunk Collector Road	2.701 ha	15.5%
Local Roads	3.489 ha	20.0%
Total Area of New Road	6.190 ha	35.4%
Open Space		
Area of District Park, Green Links & Stormwater Management	1.820 ha	10.4%
Total Area of Open Space	1.820 ha	10.4%
Road Type		
Total Length of New Road	3432m	
6.5m wide lane - 5.5m pavement	120m	
8.5m wide lane - 5.5m pavement	195m	
9m wide lane - 5.5m pavement	66m	
12.25m wide road - 6.5m pavement	596m	
14m wide road - 6.5m pavement	786m	
16.5m wide road - 8m pavement	659m	
17.7m wide road - 8m pavement	118m	
23.5m wide road - 10m pavement	380m	
33.7m wide road	248m	
35.5m wide road	264m	



PLANS AND DOCUMENTS
referred to in the PDA APPROVAL

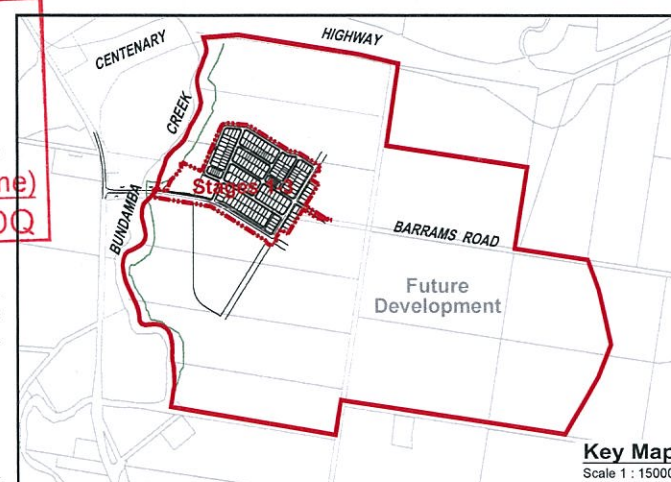
05 FEB 2014

MEDQ

AMENDED IN RED

05 FEB 2014

By: *Alex Haslam* (name)
MEDQ



DEVELOPMENT STATISTICS

STANDARD DWELLINGS									
Allotment Type	Typical Lot Dimensions	Stage 1		Stage 2		Stage 3		Overall	
		No. of Lots	Percentage	No. of Lots	Percentage	No. of Lots	Percentage	No. of Lots	Percentage
Standard Allotment 28m Product									
Terrace Allotment	6m x 28m	0	0.0%	0	0.0%	9	14.5%	9	4.7%
Cottage Allotment	8.5m x 28m	0	0.0%	6	12.8%	2	3.2%	8	4.1%
Villa Allotment	10.5m x 28m	3	3.6%	4	8.5%	2	3.2%	9	4.7%
Premium Villa Allotment	12.5m x 28m	3	3.6%	11	23.4%	12	19.4%	26	13.5%
Courtyard Allotment	14m x 28m	1	1.2%	7	14.9%	3	4.8%	11	5.7%
Traditional	16m x 28m	0	0.0%	1	2.1%	2	3.2%	3	1.6%
Sub Total		7	8.3%	29	61.7%	30	48.4%	66	29.5%
Standard Allotment 32m Product									
Premium Villa Allotment	12.5m x 32m	1	1.2%	0	0.0%	3	4.8%	4	2.1%
Courtyard Allotment	14m x 32m	17	20.2%	4	8.5%	12	19.4%	33	17.1%
Traditional	16m x 32m	29	34.5%	6	12.8%	10	16.1%	45	23.3%
Premium Traditional Allotment	18m x 32m	30	35.7%	8	17.0%	7	11.3%	45	23.3%
Sub Total		77	91.7%	18	38.3%	32	51.6%	127	65.8%
Total Standard Allotments		84	100.0%	47	100.0%	62	100.0%	193	100.0%
Density Calculation									dw/ha
Net Residential Density - 195 dwellings (including residential lots, laneway lots and multiple residential dwellings / 12.384ha (area of local roads and residential lots))									15.7

Scale 1:1000

REVISION
A: 01/10/10 Amend lot layout
B: 18/10/10 Amend lot layout
C: 08/11/10 Amend lot layout
D: 18/11/10 Amend lot layout
E: 01/03/11 Amend lot layout
F: 17/12/12 Amend lot layout
G: 23/01/13 Amend lot layout
H: 05/03/13 Amend lot layout
I: 13/03/13 Amend lot layout
J: 12/08/13 Amend lot layout
K: 26/09/13 Amend lot layout
L: 09/10/13 Amend lot layout
M: 11/10/13 Amend lot layout

Note:
All dimensions and areas are approximate only, and are subject to survey and Council approval.
Dimensions have been rounded to the nearest 0.1 metres.
Areas have been rounded down to the nearest 5m².
The boundaries shown on this plan should not be used for final detailed engineers design.
Contours are to be

Source Information:
Site boundaries: DTS
Adjoining information: DCDB
Contours: Cardno

CLIENT
AMEX

PROJECT
SECONDARY URBAN CENTRE EAST

PLAN OF SUBDIVISION
STAGES 1-3
ALLOTMENT LAYOUT

Level Datum
Origin

Date 11 OCTOBER 2013

Comp By: JLS

DWG Name: 6837-1210

Local Authority IPSWICH CITY COUNCIL

Locality RIPLEY

Job Reference 6837

Scale 1:1000 Sheet A1

Plan Ref 6837-121 Rev 0

Legend

General Details

Site Boundary

Proposed Stage Boundary

Multiple Residential

Road

Indicative Footpath Location

Open Space

District Park, Green Links & Stormwater Management

Bundamba Creek Buffer

Facilities

Not part of this approval



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Notes:

General

- All development is to be undertaken in accordance with the Development Approval.
- The maximum height of buildings shall be in the order of 9 metres.

Setbacks

- Setbacks are as per the Plan of Development Table unless otherwise specified.
- Built to boundary walls are optional. The location of the built to boundary walls are indicated on the Plan of Development. Where built to boundary walls are not adopted side setbacks shall be in accordance with the Plan of Development Table.
- Boundary setbacks are measured to the wall of the building or edge of balcony.
- Eaves (except on a wall that is built to the boundary) should not encroach within 450mm of the side or rear boundaries. Gutters may not extend beyond the boundaries of the lot.
- First floor setbacks on single and multi family allotments must not exceed the minimum ground floor setbacks.
- Garages must not project forward of the front building setback and must be setback a minimum of 4.5m from the front property boundary.

Parking

- Parking for Multiple Residential developments
 - 1 space per dwelling unit on-site
 - 0.5 visitor space per dwelling on-site
 - do not dominate the streetscape

Site Cover and Amenities

- Site cover for each lot is not to exceed 70% of the lot.

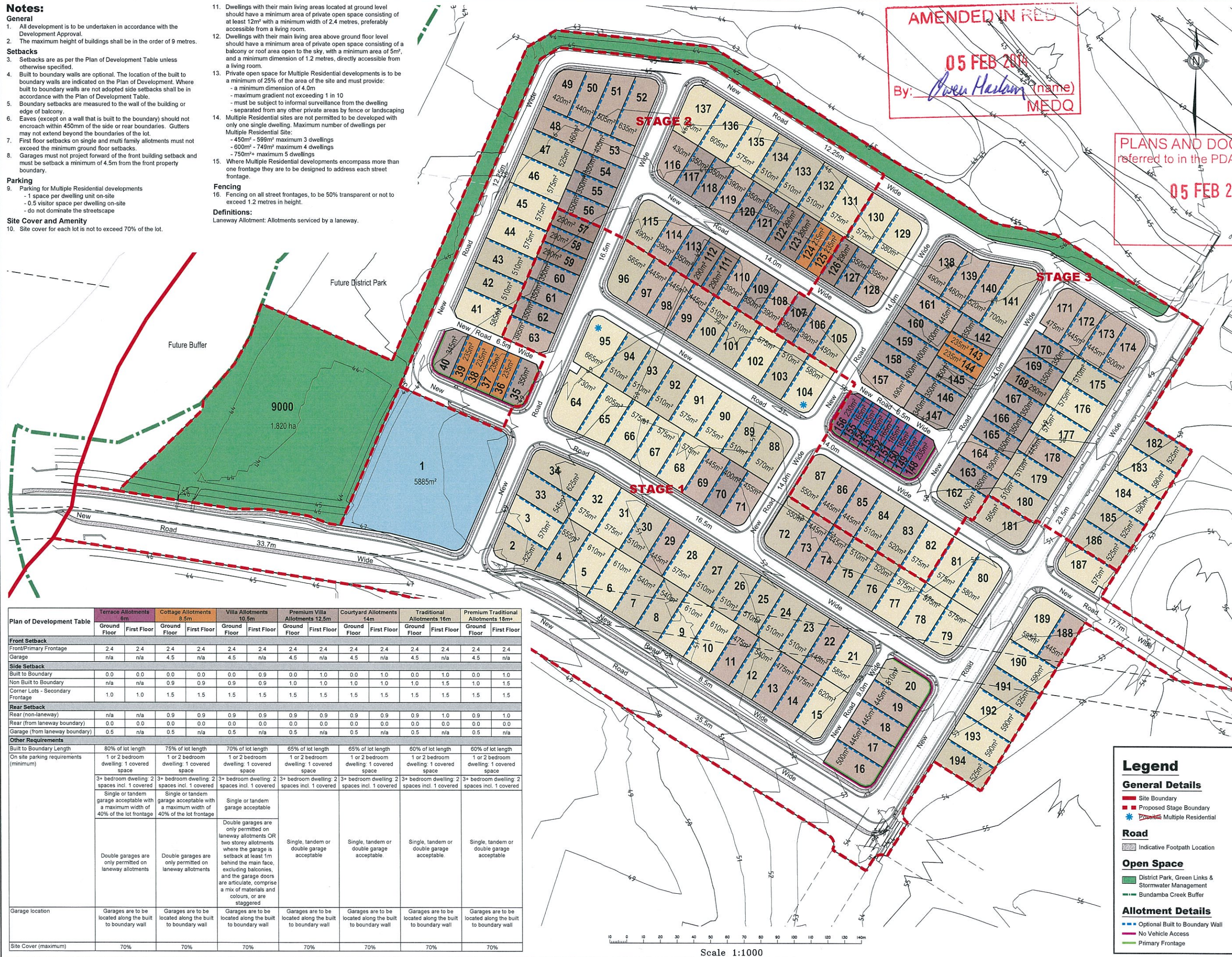
- Dwellings with their main living areas located at ground level should have a minimum area of private open space consisting of at least 12m² with a minimum width of 2.4 metres, preferably accessible from a living room.
- Dwellings with their main living area above ground floor level should have a minimum area of private open space consisting of a balcony or roof area open to the sky, with a minimum area of 5m², and a minimum dimension of 1.2 metres, directly accessible from a living room.
- Private open space for Multiple Residential developments is to be a minimum of 25% of the area of the site and must provide:
 - a minimum dimension of 4.0m
 - maximum gradient not exceeding 1 in 10
 - must be subject to informal surveillance from the dwelling
 - separated from any other private areas by fence or landscaping
- Multiple Residential sites are not permitted to be developed with only one single dwelling. Maximum number of dwellings per Multiple Residential Site:
 - 450m² - 599m² maximum 3 dwellings
 - 600m² - 749m² maximum 4 dwellings
 - 750m² - maximum 5 dwellings
- Where Multiple Residential developments encompass more than one frontage they are to be designed to address each street frontage.

Fencing

- Fencing on all street frontages, to be 50% transparent or not to exceed 1.2 metres in height.

Definitions:

Laneway Allotment: Allotments serviced by a laneway.



AMENDED IN RED
05 FEB 2014
By: *David Haslam* (name)
MEDQ

PLANS AND DOCUMENTS
referred to in the PDA APPROVAL
05 FEB 2014
MEDQ

REVISION

- A: 18/11/10 Amend lot layout
B: 01/03/11 Amend lot layout
C: 17/12/12 Amend lot layout
D: 23/01/13 Amend footpath locations
E: 05/03/13 Amend PDA table & notes
F: 13/03/13 Amend staging
G: 26/09/13 Amend PDA table & notes
H: 26/09/13 Amend Stage 1 lots
I: 09/10/13 Amend Stage 1 lots
J: 11/10/13 Amend Stage 1 lots

Note:

All dimensions and areas are approximate only, and are subject to survey and Council approval. Dimensions have been rounded to the nearest 0.1 metres. Areas have been rounded down to the nearest 5m². The boundaries shown on this plan should not be used for final detailed engineers design.

Source Information:

Site boundaries: Registered Survey Plans.
Adjoining information: DCDB.
Contours: Aerial photography.
Environment constraints: Flood.

CLIENT

AMEX

PROJECT

SECONDARY URBAN
CENTRE EAST

PLAN OF
DEVELOPMENT
STAGES 1-3

Level Datum

Origin

Date: 11 OCTOBER 2013

Comp By: JLS

DWG Name: 6837-1210

Local Authority: IPSWICH CITY COUNCIL

Locality: RIPLEY

Job Reference: 6837

Scale: 1:1000

Sheet: A1

Plan Ref: 6837-127

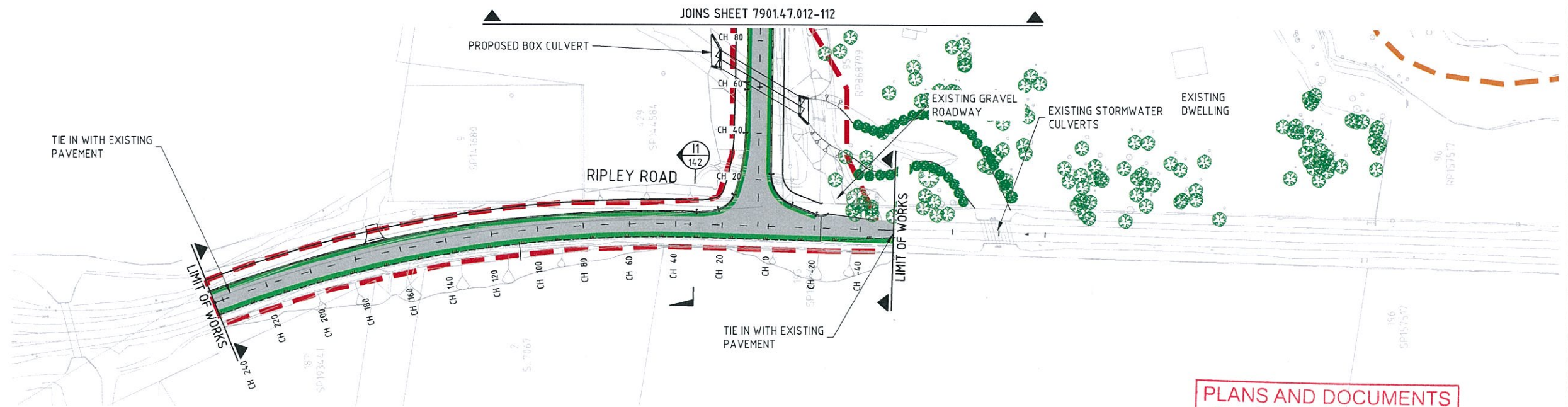
Rev: J



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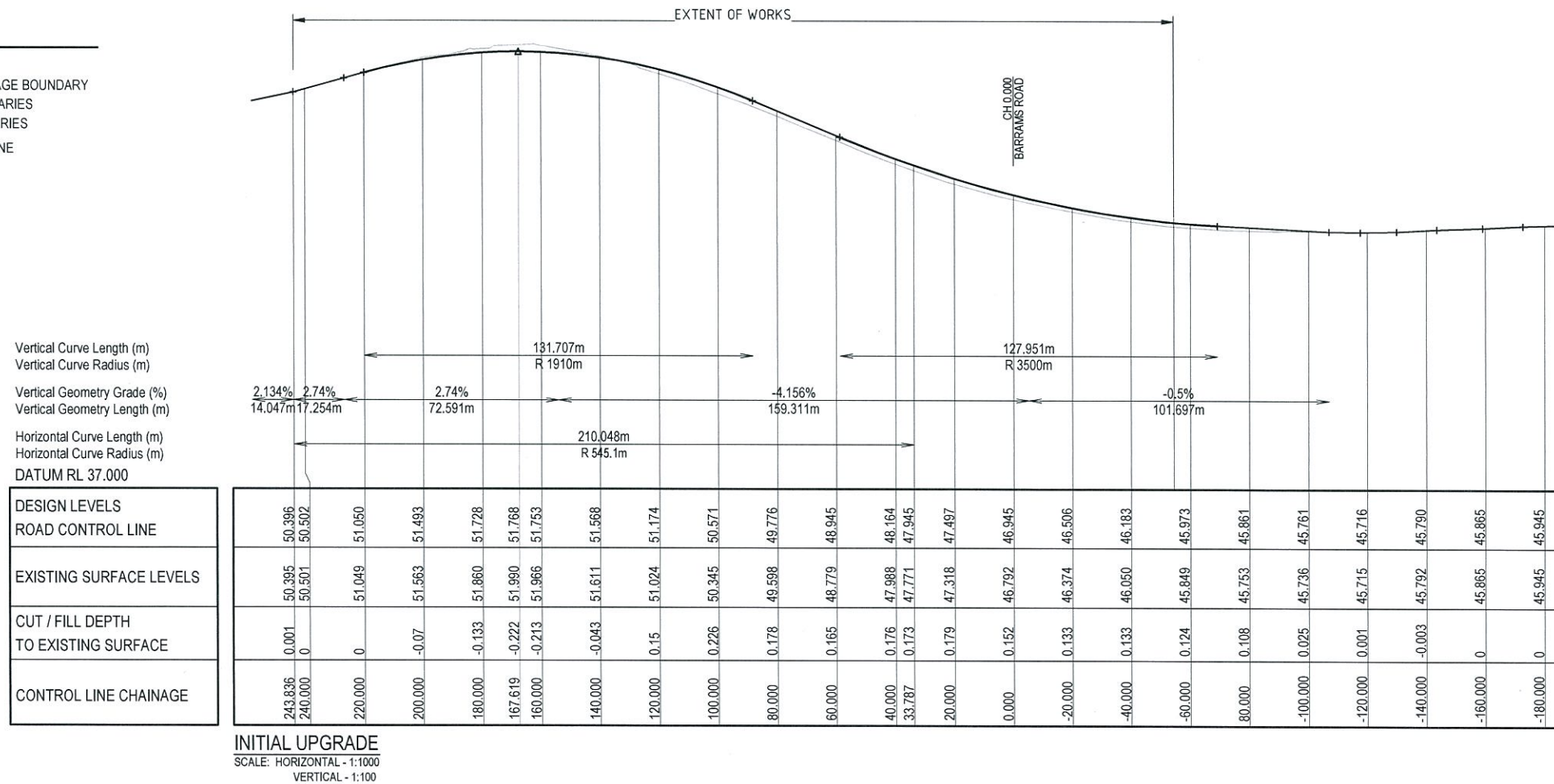
Plan of Development Table	Terrace Allotments 9m		Cottage Allotments 8.5m		Villa Allotments 10.5m		Premium Villa Allotments 12.5m		Courtyard Allotments 14m		Traditional Allotments 16m		Premium Traditional Allotments 18m+	
	Ground Floor	First Floor	Ground Floor	First Floor	Ground Floor	First Floor	Ground Floor	First Floor	Ground Floor	First Floor	Ground Floor	First Floor	Ground Floor	First Floor
Front Setback														
Front/Primary Frontage	2.4	2.4	2.4	2.4	2.4	2.4	2.4	2.4	2.4	2.4	2.4	2.4	2.4	2.4
Garage	n/a	n/a	4.5	n/a	4.5	n/a	4.5	n/a	4.5	n/a	4.5	n/a	4.5	n/a
Side Setback														
Built to Boundary	0.0	0.0	0.0	0.0	0.0	0.9	0.0	1.0	0.0	1.0	0.0	1.0	0.0	1.0
Non Built to Boundary	n/a	n/a	0.9	0.9	0.9	0.9	1.0	1.0	1.0	1.0	1.0	1.5	1.0	1.5
Corner Lots - Secondary Frontage	1.0	1.0	1.5	1.5	1.5	1.5	1.5	1.5	1.5	1.5	1.5	1.5	1.5	1.5
Rear Setback														
Rear (non-laneway)	n/a	n/a	0.9	0.9	0.9	0.9	0.9	0.9	0.9	0.9	0.9	1.0	0.9	1.0
Rear (from laneway boundary)	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Garage (from laneway boundary)	0.5	n/a	0.5	n/a	0.5	n/a	0.5	n/a	0.5	n/a	0.5	n/a	0.5	n/a
Other Requirements														
Built to Boundary Length	80% of lot length		75% of lot length		70% of lot length		65% of lot length		65% of lot length		60% of lot length		60% of lot length	
On site parking requirements (minimum)	1 or 2 bedroom dwelling: 1 covered space		1 or 2 bedroom dwelling: 1 covered space		1 or 2 bedroom dwelling: 1 covered space		1 or 2 bedroom dwelling: 1 covered space		1 or 2 bedroom dwelling: 1 covered space		1 or 2 bedroom dwelling: 1 covered space		1 or 2 bedroom dwelling: 1 covered space	
	3+ bedroom dwelling: 2 spaces incl. 1 covered		3+ bedroom dwelling: 2 spaces incl. 1 covered		3+ bedroom dwelling: 2 spaces incl. 1 covered		3+ bedroom dwelling: 2 spaces incl. 1 covered		3+ bedroom dwelling: 2 spaces incl. 1 covered		3+ bedroom dwelling: 2 spaces incl. 1 covered		3+ bedroom dwelling: 2 spaces incl. 1 covered	
	Single or tandem garage acceptable with a maximum width of 40% of the lot frontage		Single or tandem garage acceptable with a maximum width of 40% of the lot frontage		Single or tandem garage acceptable		Single, tandem or double garage acceptable		Single, tandem or double garage acceptable		Single, tandem or double garage acceptable		Single, tandem or double garage acceptable	
Double garages are only permitted on laneway allotments		Double garages are only permitted on laneway allotments		Double garages are only permitted on laneway allotments OR two storey allotments where the garage is setback at least 1m behind the main face, excluding balconies, and the garage doors are articulate, comprise a mix of materials and colours, or are staggered										
Garage location	Garages are to be located along the built to boundary wall		Garages are to be located along the built to boundary wall		Garages are to be located along the built to boundary wall		Garages are to be located along the built to boundary wall		Garages are to be located along the built to boundary wall		Garages are to be located along the built to boundary wall		Garages are to be located along the built to boundary wall	
Site Cover (maximum)	70%		70%		70%		70%		70%		70%		70%	



RIPLEY ROAD INTERIM UPGRADE PLAN
SCALE 1:1000 @ A1

LEGEND

- SITE BOUNDARY
- CATALYST TRUNK STAGE BOUNDARY
- EXISTING LOT BOUNDARIES
- STAGE 1 LOT BOUNDARIES
- PROPOSED CYCLE LANE



INITIAL UPGRADE
SCALE: HORIZONTAL - 1:1000
VERTICAL - 1:100

RIPLEY ROAD - INTERIM UPGRADE WORKS
SCALE: HORIZONTAL - 1:1000
VERTICAL - 1:100

HOR. 10 0 10 20 30 40 50m 1:1000
VERT. 1 0 1 2 3 4 5m 1:100
SCALE @ A1

Rev	Date	Description	Drawn	Appr.
B	20/01/2014	RIPLEY RHS FOOTPATH REMOVED & BARRAMS CULVERT UPDATED	TCW	LG
A	23/01/2013	BARRAMS ROAD GRADED TO SUIT NEW FLOOD MODEL	SJT	PJO



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Drawn B.V. Date 24/02/2011
Checked D.M. Date 24/02/2011
Designed C.P. Date 24/02/2011
Verified DT Date 24/02/2011
Approved D.T. RPEQ 7990 Date 24/02/2011

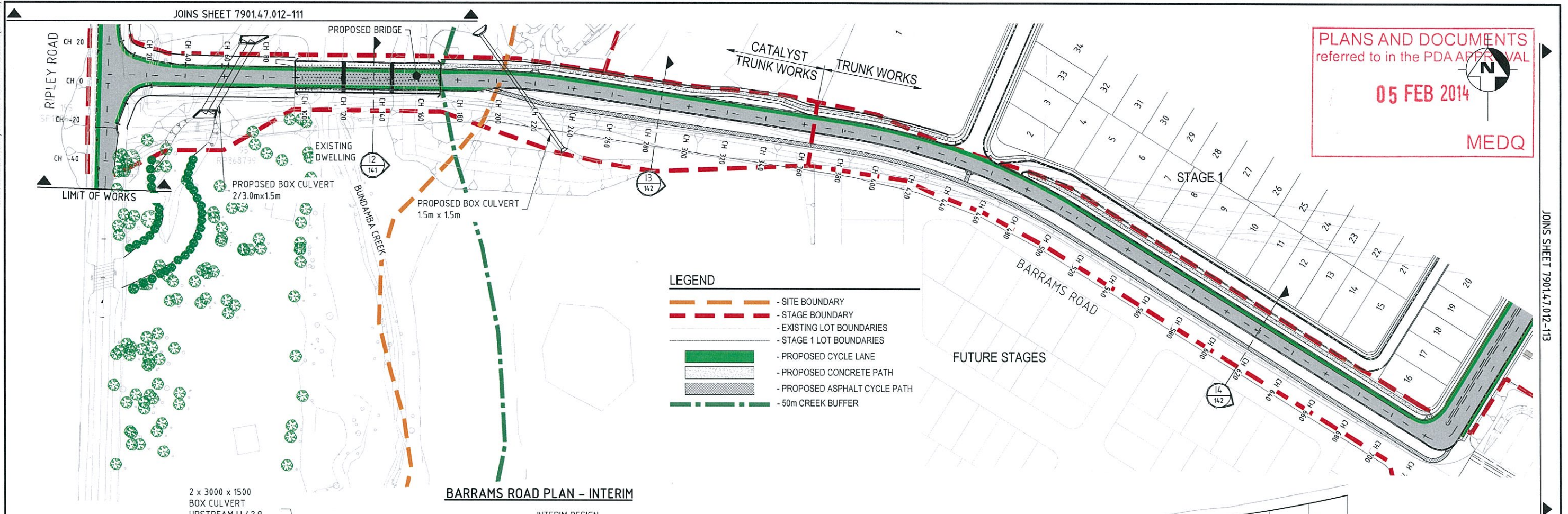
Client **AMEX PTY. LTD.**

RIPLEY VALLEY - SUCE
STAGE 1 - TRUNK ROAD WORKS

LAYOUT PLAN & ROAD LONGITUDINAL SECTION
SHEET 1 OF 3

Status **NOT FOR CONSTRUCTION**

Date	Datum	Scale	Size
22/02/2011	A.H.D.	AS SHOWN	A1
Drawing Number	Revision		
7901.47.012-111	B		



VOLUMES	CUT																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																																									
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HOR. 10 0 10 20 30 40 50m 1:1000
VERT. 1 0 1 2 3 4 5m 1:100
SCALE @ A1

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Drawn B.V. Date 24/02/2011
Checked D.M. Date 24/02/2011
Designed C.P. Date 24/02/2011
Verified DT Date 24/02/2011
Approved D.T. RPEQ 7990 Date 24/02/2011

Client AMEX PTY. LTD.
RIPLEY VALLEY - SUCE
STAGE 1 - TRUNK ROAD WORKS
INTERIM ROAD UPGRADE
LAYOUT PLAN & ROAD LONGITUDINAL SECTION
SHEET 2 OF 3

Status NOT FOR CONSTRUCTION
Date 22/02/2011 Datum A.H.D. Scale AS SHOWN Size A1
Drawing Number 7901.47.012-112 Revision C

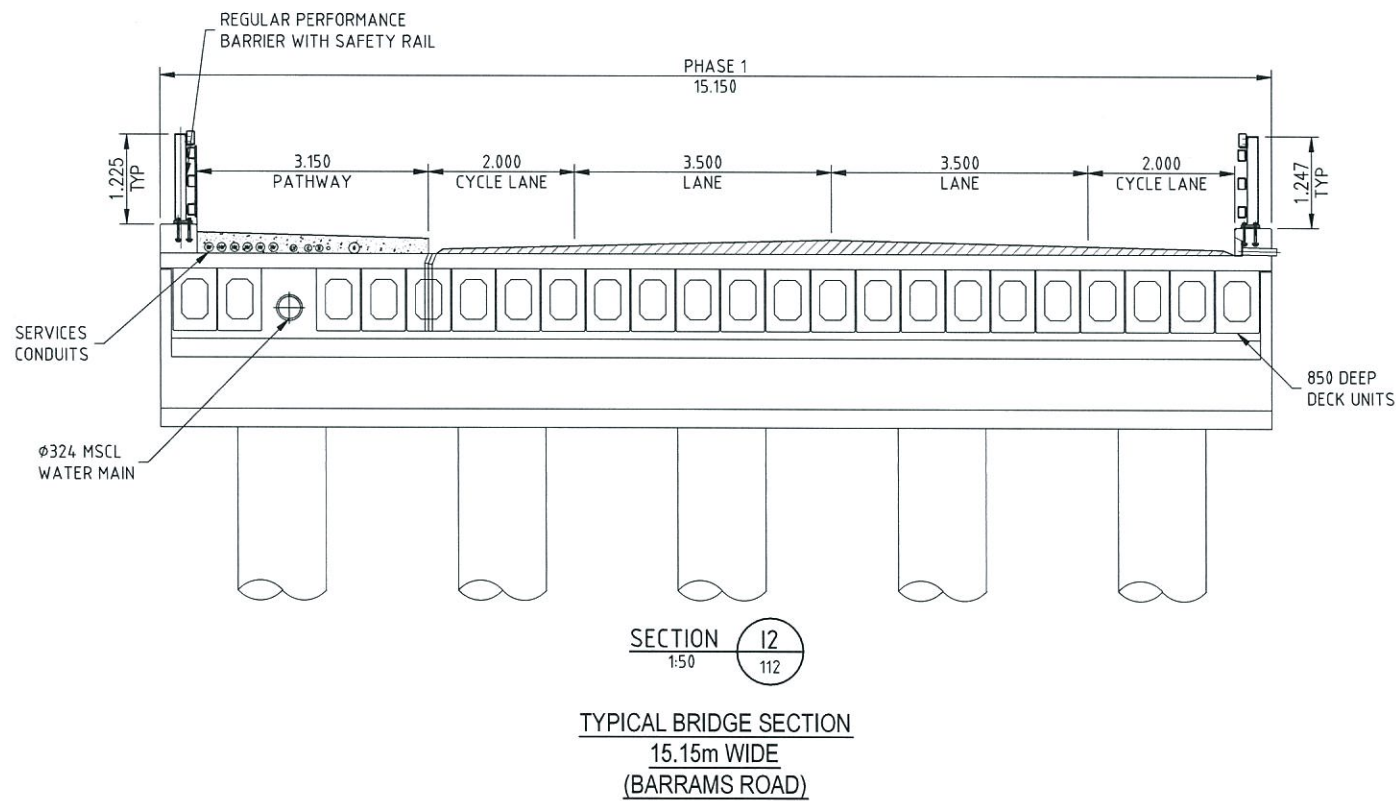
DATE PLOTTED: 20 January 2014 3:37 PM BY: TRAVIS WILLIAMS (BRISBANE)

XREF: X:GPRA1.SHT, X:RNE.IMG, X:7902.46.LOTS, X:7902.46.SURVEY, align: chain, X:7902.46.ROADS
CAD FILE: L:7901.47.AMEX.SUCED12.SUD - Stage 1 DAW12 Civil/Ripley Ultimate and Interim Set/Ripley Interim Drawings/Ripley Valley Interim - 101 to 143.dwg

PLANS AND DOCUMENTS
referred to in the PDA APPROVAL

05 FEB 2014

MEDQ



Rev	Date	Description	Drawn	Appr.
C	20/01/2014	TYPICAL BRIDGE PROFILE REVISED	TCW	LG
B	30/01/2013	LIGHTING COLUMN ADDED TO SECTION	SJT	PJO
A	23/01/2013	BRIDGE PROFILE AMENDED TO INCREASE BIKE LANE WIDTHS	SJT	PJO



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Drawn	B.V	Date	24/02/2011
Checked	D.M	Date	24/02/2011
Designed	C.P	Date	24/02/2011
Verified	DT	Date	24/02/2011
Approved	D.T	Date	24/02/2011

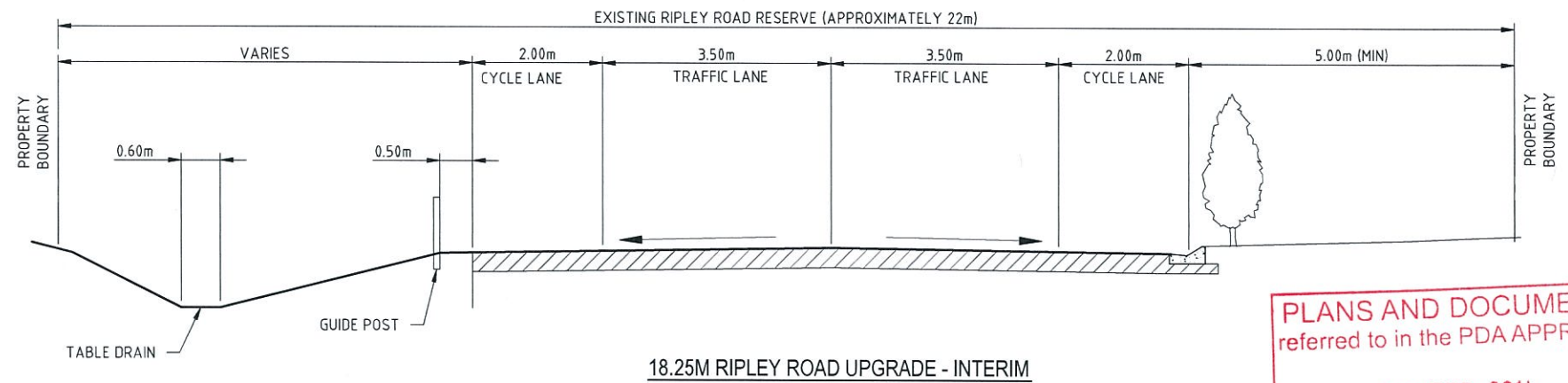
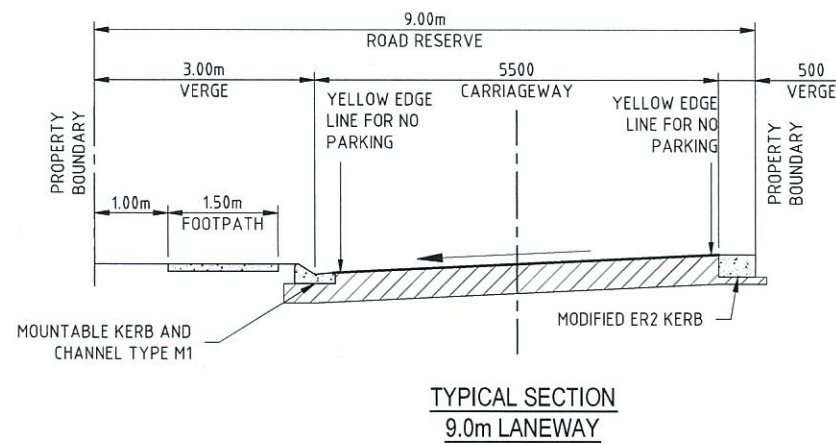
Client **AMEX PTY. LTD.**

RIPLEY VALLEY - SUCE
STAGE 1 - TRUNK ROAD WORKS
INTERIM CONSTRUCTION

BRIDGE CROSS SECTION

Status	NOT FOR CONSTRUCTION			
Date	22/02/2011	Datum	A.H.D.	Scale
				AS SHOWN
Drawing Number	7901.47.012-141			Revision
				A1
				C

0 10 20 30 40 50 60 70 80 90 100 110 120 130 140 150mm ON A1 ORIGINAL

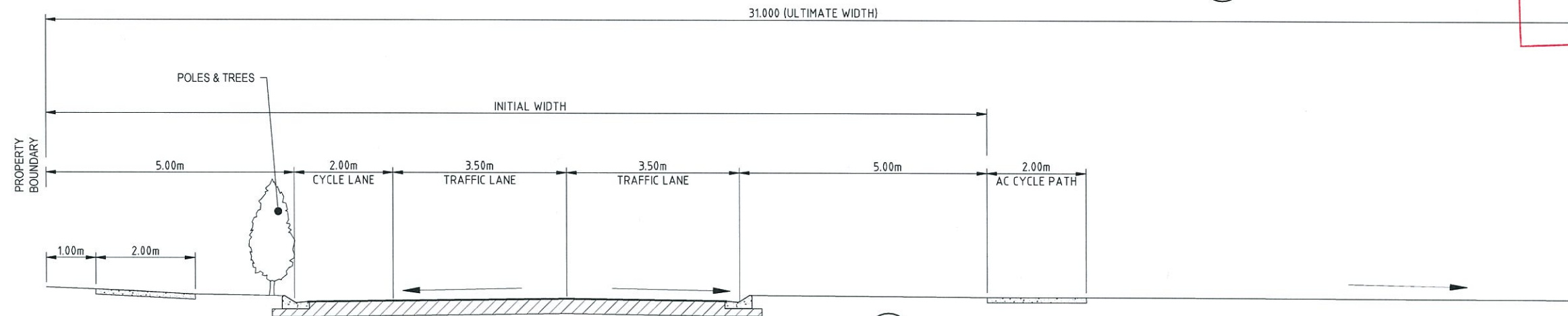


PLANS AND DOCUMENTS
referred to in the PDA APPROVAL

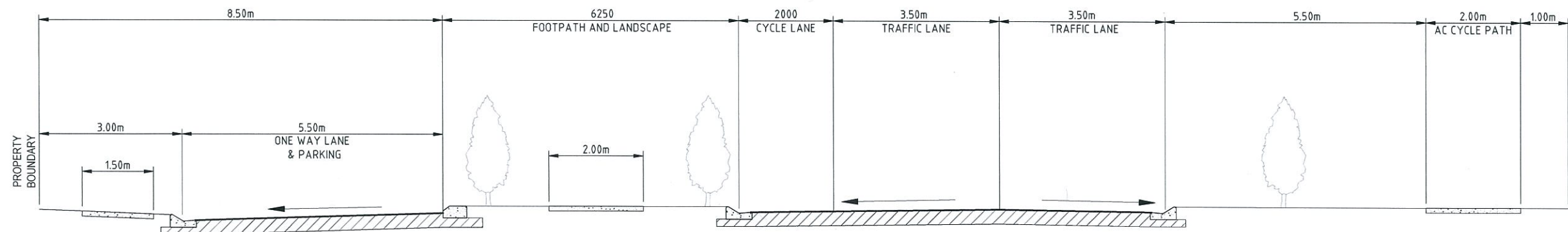
05 FEB 2014

MEDQ

SECTION 11
1:50
111



SECTION 13
1:50
112



SECTION 14
1:50
112

0 1 2 3 4 5m
SCALE 1:50 @A1

Rev	Date	Description	Drawn	Appr.
F	20/01/2014	SECTION 11 AND 14 VERGE PROFILE AMENDED	TCW	LG
E	18/09/2013	DIMENSIONS AMENDED	SBH	PJO
D	18/03/2013	NO PARKING LINE MARKING ADDED TO 9.0m LANEWAY SECTION	SJT	PJO
C	15/03/2013	SERVICE ROAD SECTION ADDED	SJT	DR
B	30/01/2013	FOOTPATH WIDTH AMENDED TO 2.0m ON SECTIONS	SJT	PJO
A	23/01/2013	SECTION 13 AMENDED TO INCREASED BIKE LANE WIDTHS	SJT	PJO



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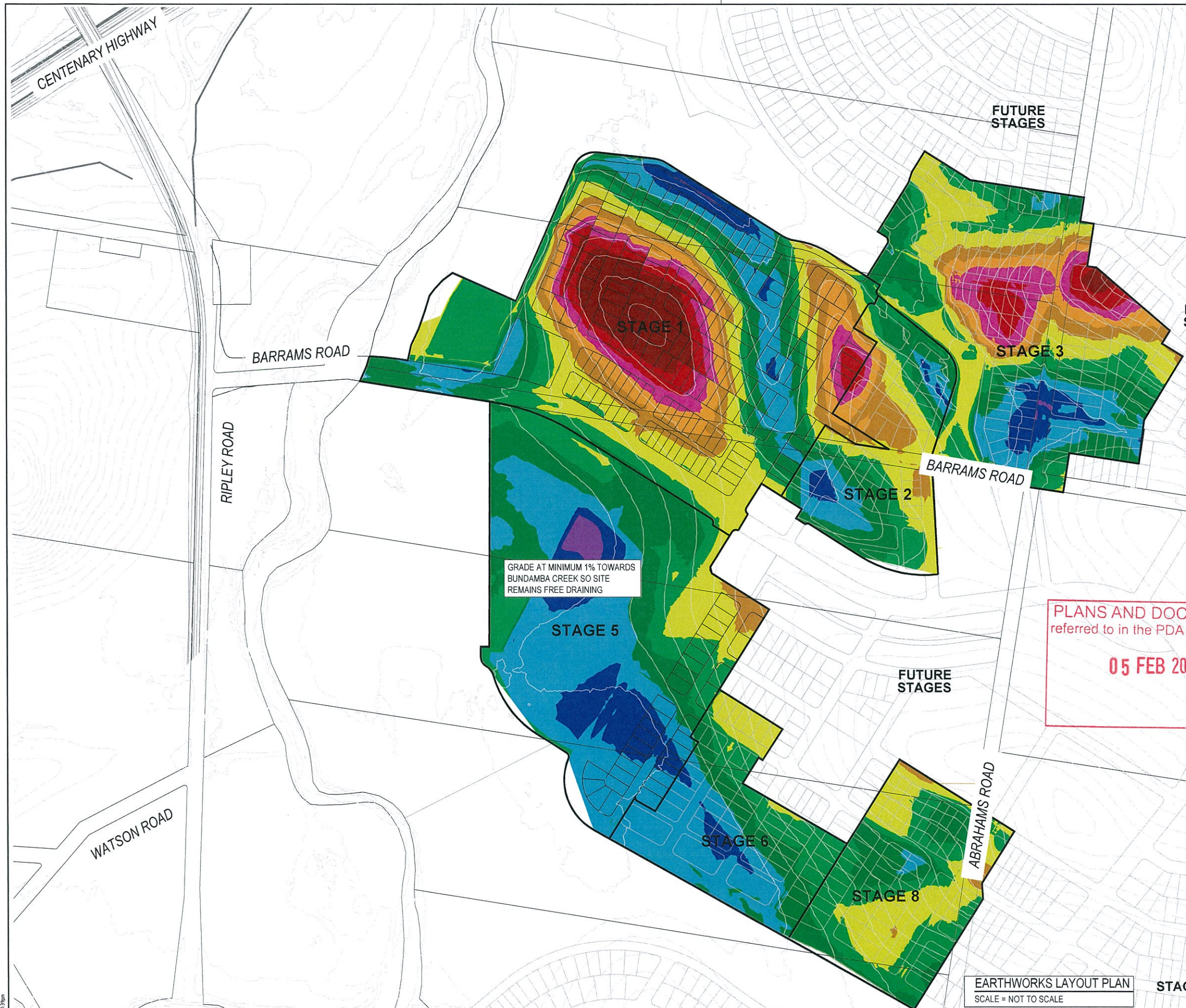
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Checked	D.M	Date	24/02/2011
Designed	C.P	Date	24/02/2011
Verified	DT	Date	24/02/2011
Approved	D.T	Date	24/02/2011

Client **AMEX PTY. LTD.**
RIPLEY VALLEY - SUCE
STAGE 1 - TRUNK ROAD WORKS
INTERIM CONSTRUCTION
ROAD CROSS SECTIONS
SHEET 1 OF 2

Status	NOT FOR CONSTRUCTION			
Date	22/02/2011	Datum	A.H.D.	Scale
Drawing Number	7901.47.012-142	Revision	F	Size
				A1



- NOTES
1. Earthwork volumes detailed on this sheet are indicative only and are subject to change upon completion of detail design and the inclusion of road boxing, trench spoil and bulking factor.
 2. A detailed Erosion and Sediment Control Management Plan is to be developed and implemented for stages 1, 2, 3, 5, 6 & 8 prior to any earthworks being undertaken in these areas.
 3. The Erosion and Sediment Control Management Plan will ensure Bundamba Creek and surrounding waterways are not adversely affected during, or as a result of, earthwork operations.
 4. Areas filled to final level will be stabilised and vegetated as per the Erosion and Sediment Control Management Plan to ensure no bare areas are left exposed.
 5. Any filling outside of Stage 1 will be graded to ensure no trapped low points and that the area remains free draining.

EARTHWORK VOLUMES (m³)			
STAGE	Cut	Fill	Balance
1	260,000	112,000	148,000
2	20,600	42,000	-21,400
3	75,000	65,000	10,000
5	6,000	242,000	-236,000
6	1,000	77,000	-76,000
8	11,000	59,000	-48,000
TOTAL	373,600	597,000	-223,400

EXCESS CUT FROM STAGE 1 TO BE PLACED IN STAGES 2, 3, 5, 6 AND/OR 8 TO LEVEL 1 COMPACTION

LEGEND	
Depth +5m	
Depth 4m to 5m	
Depth 3m to 4m	
Depth 2m to 3m	
Depth 1m to 2m	
Depth 0m to 1m	
Depth -1m to 0m	
Depth -2m to -1m	
Depth -3m to -2m	
Depth -4m to -3m	
Depth -5m to -4m	
Depth -5m	

PLANS AND DOCUMENTS referred to in the PDA APPROVAL

05 FEB 2014

MEDQ

EXISTING CONTOURS SHOWN @ 2m INTERVALS

CONCEPT PLAN FOR REVIEW ONLY

EARTHWORKS LAYOUT PLAN

SCALE = NOT TO SCALE

DATE PLOTTED: 14 June 2011 - 1:30pm

1	14.06.11	UPDATED STAGE DETAILS	GH	CP
0	08.06.11	ISSUED FOR REVIEW	GH	PO
Rv.	DATE	REVISIONS	REC.	APPR.

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DESIGNED: CP
DRAWN: DM
CHECKED: AS
RECOMMENDED: PROJ. MAN.

A1
DATUM: A.H.D.

APPROVED: PROJ. DIR.

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RIPLEY VALLEY
RIPLEY VALLEY PRELIMINARY EARTHWORKS MODELLING
AMEX SUCE
PROPOSED INDICATIVE DISPOSAL AREAS FOR EXCESS CUT FROM STAGE 1

ORIG. ISSUE DATE: MAY 2011
DRAWING No:
7901-47-001-SK001

Rv. 1