



Our Ref: 2219_TPA16

18 March 2026

Economic Development Queensland
c/- Town Planning Alliance

Property Address: 197-201 Teviot Road, Greenbank
Subject: Traffic Statement – Response to Outstanding Issues

Dear Sir/Madam

We refer to the correspondence issued by Economic Development Queensland (EDQ) dated 9 February 2026 outlining outstanding matters associated with the proposed subdivision.

Q Traffic has been working closely with EDQ and the Applicant team in responding to the outstanding matters. Through this process, the Plan of Development has been amended. The reader is referred to the updated Plan of Development (**Attachment 1**).

From a traffic and parking perspective, the plans have been refined such that:

- The POD ensures all lots can accommodate two (2) vehicles parked wholly within the subject site; and
- Driveway locations have been rationalised and, where possible, amalgamated to maximise available kerbside length for on-street parking.

Our responses to the relevant outstanding matters are provided below.

Item 1e – Shared driveway

The POD has been amended to address this matter.

The internal stub road is proposed to have a carriageway width of 5.5 metres, which is sufficient to accommodate two-way vehicle movements in a low-speed residential environment. On this basis, it is noted that the previous arrangement (servicing five lots) would have, in our opinion, been capable of operating in a safe and functional manner from a traffic engineering perspective.

Notwithstanding the above, the design has been refined in response to EDQ's requirements. The number of allotments gaining access from the stub road has been reduced, such that:

- A total of four (4) lots are accessed from the stub road; and
- Of these, two (2) lots share a common driveway, resulting in a total of three (3) crossovers.

This outcome is consistent with EDQ's requirement to limit shared access arrangements and represents an improved and efficient access configuration.

Item 1g – Bin presentation and waste collection

In response to EDQ's comments, the waste management arrangement has been revised.

The reader is referred to **Attachment 2**, which includes:

- Updated bin presentation locations; and
- A swept path assessment demonstrating the manoeuvring of a side-loading refuse collection vehicle.

Key changes include:

- The previously proposed bin pad adjacent Lot 36 (servicing approximately 10 dwellings) has been removed and relocated;
- Lots 1–16 and 40–42 are able to present bins to either Leanne Court or the new road. For the purposes of assessment, bin presentation for these lots has been shown to Leanne Court as this frontage is less constrained, however either frontage is considered acceptable from a servicing perspective;
- The layout has been refined such that the refuse vehicle can service all dwellings with only a single three-point turn required at the stub road.

This revised arrangement improves residential amenity and provides a practical and compliant servicing outcome.

Item 3 - Parking Analysis Plan

Parking provision for the proposed development comprises a combination of:

- On-site parking within each lot; and
- On-street (kerbside) parking.

On-site parking

Reference is made to PDA Guideline No. 07, which seeks an outcome of two (2) parking spaces per dwelling for dwellings containing three (3) or more bedrooms, with at least one space capable of being covered.

The amended POD (**Attachment 1**) ensures compliance with this outcome by:

- Requiring all lots to provide a single garage; and
- Mandating a minimum garage setback of 5.4 metres from the front boundary.

This ensures that:

- One vehicle can be accommodated within the garage; and
- A second vehicle can be accommodated in tandem within the driveway, wholly within the lot.

Accordingly, all lots can accommodate two (2) vehicles on-site, representing a high level of on-site parking provision.

On-street parking

Reference is made to PDA Guideline No. 05, which seeks an outcome of **0.75 on-street parking spaces per dwelling**.

An updated on-street parking plan has been prepared (refer Attachment 3), which demonstrates:

- A total of 26 on-street parking spaces;
- Supporting 42 residential lots; and
- An equivalent provision of approximately **0.62 spaces per lot**.

Note:

The first kerbside parking space on the new north-south road is located approximately 18 metres from its intersection with Leanne Court. This comfortably exceeds the minimum 10 metre no-stopping requirement applicable to an unsignalised intersection under the Queensland Road Rules and provides adequate clearance for vehicle manoeuvring in the vicinity of the intersection.

The guideline also states that:

“A relaxation to the above standards may be approved where the development secures a higher proportion of on-site parking and/or where the development is in proximity to high frequency public transport.”

While this is marginally below the 0.75 spaces per lot target, the extent of variation is considered acceptable having regard to the following:

1. *High on-site parking provision*

The POD ensures all dwellings can accommodate two vehicles on-site, reducing reliance on kerbside parking.

2. *Additional parking opportunity (not relied upon)*

Lots fronting Leanne Court have pedestrian access to this frontage. Whilst not relied upon for compliance, this frontage provides additional kerbside parking for up to 15 vehicles should it be required.

On this basis, the proposed on-street parking provision is considered to satisfy the intent of the guideline and represents an appropriate balance between on-site and on-street parking supply.

Landscape build-outs

The design incorporates two (2) landscape build-outs, as shown in **Figure 1**. These build-outs result in the removal of two (2) kerbside parking spaces; however, this is considered appropriate in the context of the overall parking provision across the site. The build-outs provide traffic calming benefits by narrowing the perceived carriageway width and assisting to moderate vehicle speeds, while also contributing positively to the streetscape through increased landscaping and visual amenity.

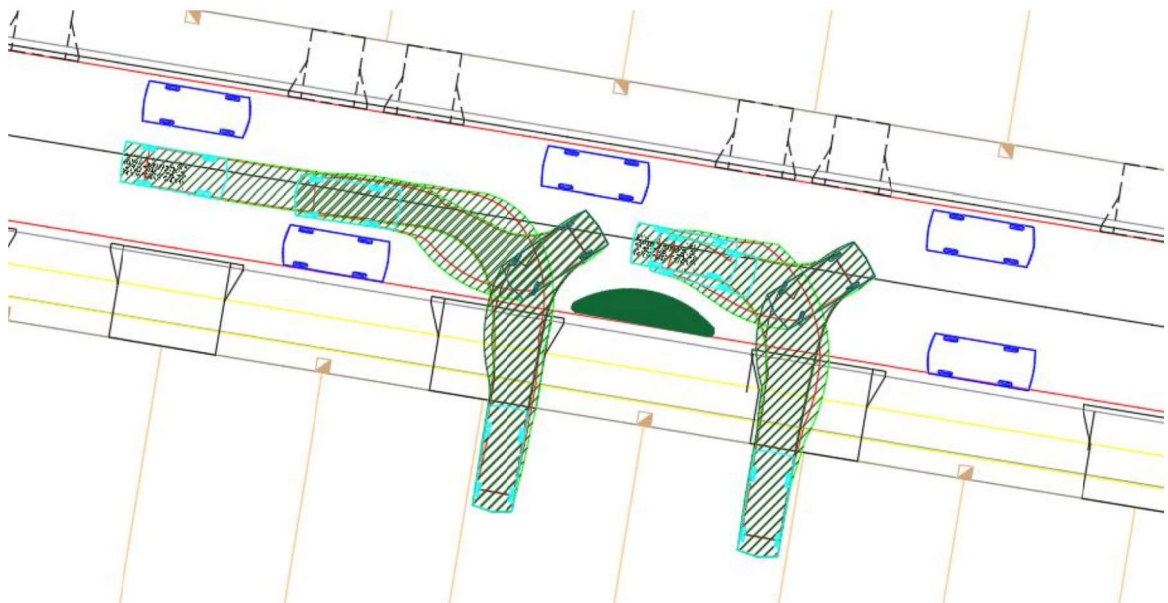


Figure 1: Landscape Islands

Driveway locations

It is acknowledged that AS/NZS 2890.1 nominates a desirable driveway offset of 6 metres from the tangent point of the kerb at an intersection. As shown in **Figure 2** below, the proposed access points achieve an offset of approximately 4.3 to 4.5 metres and therefore represent a minor variation to this criterion. In our opinion, this variation is acceptable. Due to the generous kerb return, the practical separation between the driveways and the intersection is approximately 9.5 metres, which is the more relevant measure from an operational perspective. This clearance is sufficient to ensure that standing or turning vehicles associated with the intersection would not impede driveway access, and vice versa. In addition, the intersection is located within a low-speed residential environment and provides good sight distance for vehicles entering and leaving the subject driveways. Accordingly, the proposed access arrangement is considered to remain safe and functional notwithstanding the minor departure from the nominal AS/NZS 2890.1 dimension

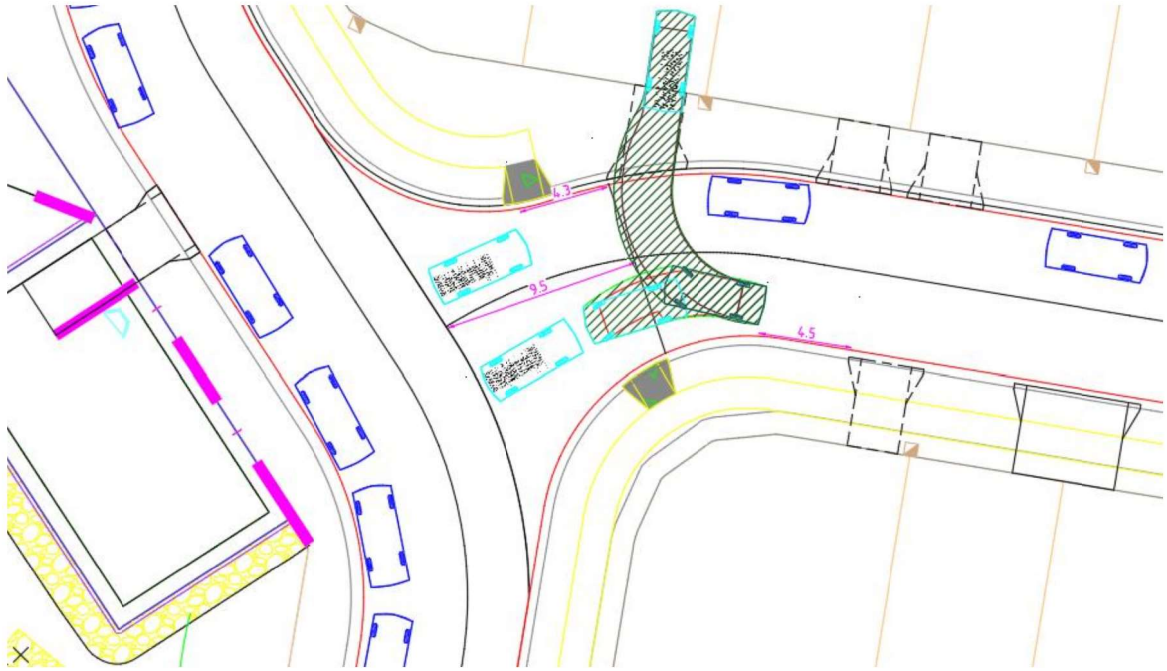


Figure 2: Driveway Locations

Conclusion and Recommendation

The amended POD and supporting documentation demonstrate that the proposed development:

- Provides a robust and enforceable on-site parking outcome;
- Achieves a practical and efficient on-street parking layout; and
- Addresses EDQ's outstanding matters relating to access, waste servicing and parking.

We trust the above satisfactorily addresses the outstanding matters. Should you require any further clarification, please do not hesitate to contact the undersigned.

Regards

Richard Quinn
BECivil, MIEAust, RPEQ (08565)
Director – Q Traffic

ATTACHMENT 1

Updated Plan of Development (POD)

LOGAN CITY COUNCIL

197-201 TEVIOT ROAD, GREENBANK

LOT 16 ON SP343224

PLAN OF DEVELOPMENT



SITE LOCATION
R15

DRAWING INDEX		
DRAWING No.	DRAWING TITLE	REVISION
S0001	COVER SHEET	B
S0002	POD DESIGN PARAMETERS	B
S0003	PLAN OF DEVELOPMENT - SHEET 1	I
S0004	PLAN OF DEVELOPMENT - SHEET 2	I
S0005	TYPICAL DWELLING TYPES - SHEET 1	B
S0006	TYPICAL DWELLING TYPES - SHEET 2	A

SCALE 1:500 A1 (OR SMALLER)

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DEVELOPMENT GUIDANCE

THE BELOW SUMMARISES DEVELOPMENT CONTROLS AS OBTAINED IN THE QUEENSLAND GOVERNMENT PRIORITY DEVELOPMENT AREA (PDA) GUIDELINES. PLEASE REFER TO PLAN OF DEVELOPMENT TABLE FOR SPECIFIC INFORMATION REGARDING LOT TYPES, SETBACKS AND OTHER BUILDING PARAMETERS.

- STREET ADDRESS**
- STREET ADDRESS TO BE IN ACCORDANCE WITH PERFORMANCE CRITERIA OUTLINED IN THE PDA GUIDELINE - LOW RISE BUILDINGS.
 - THE LOT THROUGH FACES SHOULD BE CLEARLY VISIBLE AND IDENTIFIABLE FROM THE STREET.
 - ON CORNER LOTS WITH A SECOND STREET FRONTAGE, TWO OR MORE OF THE FOLLOWING DEVELOPMENT ELEMENTS SHOULD BE APPLIED TO THE BUILDING FACADE:
 - VERANDAH
 - POVERCHES
 - AWNINGS AND SHADE STRUCTURES
 - VARIATION TO ROOF AND BUILDING LINES
 - INCLUSION OF WINDOW OPENINGS
 - USE OF VARYING BUILDING MATERIALS.

- BUILDING ARTICULATION**
- BUILDING ARTICULATION TO BE IN ACCORDANCE WITH PERFORMANCE CRITERIA OUTLINED IN THE PDA GUIDELINE - LOW RISE BUILDINGS.
 - THE ARTICULATION OF BUILDING FACADES SHOULD BE VISIBLE AND RESOLVE THE PERCEIVED BULK OF THE BUILDING STRUCTURES.
 - WALL BUILDINGS - ALL BUILDINGS WITH A HEIGHT OF MORE THAN TEN METRES VISIBLE FROM A STREET SHOULD BE ARTICULATED TO REDUCE THE MASS OF THE BUILDING BY ONE OR MORE OF THE FOLLOWING:
 - WINDOWS RECESSED INTO THE FACADE
 - INCLUSION OF BALCONIES, PORCHES OR VERANDAH
 - INCLUSION OF WINDOW HOODS
 - CAST SHADOW LINES ON THE BUILDING THROUGH MINOR CHANGES (DOWN) IN HEIGHT IN THE BUILDING FACADE
 - INCORPORATE TWO OR MORE OF THE FOLLOWING BUILD FORM ELEMENTS TO ENHANCE BUILDING VARIETY TO ALL BUILDINGS, INCLUDING 2-STORY BUILDINGS:
 - VERANDAH
 - POVERCHES
 - WINDOW HOODS
 - SCREENS
 - AWNINGS AND SHADE STRUCTURES
 - TO REDUCE THE APPEARANCE OF LONG RUNS OF FLAT FACADES FRONTING A STREET, IT IS RECOMMENDED THAT NO MORE THAN FIVE (5) METRES OF A WALL WITH A HEIGHT OF LESS THAN 1.5M IN A ROW UNLESS PROPOSED HOUSE PLANS INCLUDE BUILDING ARTICULATION DETAILING AS DISCUSSED ABOVE.
 - EXTERNAL DRAPERY OR OTHER PERMANENT DEVICES ARE TO BE INTEGRATED WITH BUILDING DESIGN.

- GARAGES**
- THE FULLY HONOR OF ALL GARAGES IS TO COMPLEMENT THE ADJACENT DWELLING IN RELATION TO BUILDING ARTICULATION, SCALE, FORM, MATERIALITY, COLOR, AND DETAILING. GARAGE DOORS ARE TO BE ARTICULATED.

- VENTILATION**
- TO FACILITATE CROSS VENTILATION, ENSURE THE BUILDING DESIGN INCLUDES AT LEAST TWO OPENINGS TO ALL HABITABLE ROOMS INCLUDING INTERNAL AND EXTERNAL WINDOWS.

- PRIVATE OPEN SPACE**
- PRIVATE OPEN SPACE TO BE IN ACCORDANCE WITH THE POS TABLE, REFER TO THE PERFORMANCE CRITERIA OUTLINED IN THE PDA GUIDELINE - LOW RISE BUILDINGS.

- ACOUSTIC REQUIREMENTS**
- ACOUSTIC REQUIREMENTS TO BE IN ACCORDANCE WITH PERFORMANCE CRITERIA OUTLINED IN THE ACOUSTIC REPORT (TRAFFIC NOISE ASSESSMENT BY RENZO TONIN & ASSOCIATES, DATED JAN 2026).
 - THE PRIVATE NOISE ASSESSMENT OUTLINES FUTURE DWELLINGS SHOULD BE DESIGNED TO AVOID BEING AFFECTED AND WOULD REQUIRE VENTILATION OF AIR CONDUCTIONS TO AVOID COMPROMISING WITH THE NATIONAL CONSTRUCTION CODE (NCC).
 - GLAZING, EXTERNAL WALLS, ROOF, FLOORS AND ENTRY DOORS OF THE PROPOSED DWELLINGS WILL ACHIEVE MINIMUM SOUND RATES OF THE RELEVANT NOISE CATEGORIES REFERRED TO IN THE ACOUSTIC REPORT BY RENZO TONIN & ASSOCIATES, DATED JAN 2026.
 - THE FUTURE NOISE MODELS ARE BASED ON THE CURRENT DESIGN CONTAINING NOISE BY THE PROPOSED LEVELS OF LOTS CHANGES SUBSTANTIALLY GREATER THAN 300M DISTANCE, A REFINED NOISE ASSESSMENT IS REQUIRED.

- FENCING**
- FENCING TO BE IN ACCORDANCE WITH PERFORMANCE CRITERIA OUTLINED IN THE PDA GUIDELINE - LOW RISE BUILDINGS AND OUTLINED IN THE TRAFFIC NOISE ASSESSMENT REPORT TO ACOUSTIC REQUIREMENTS.
 - FENCING SCREENING OR LIVES CHANGES TO PROTECT PRIVACY BUT STILL ALLOW FOR OVERLOOKING OF THE STREET TO PROMOTE CASUAL SURVEILLANCE.
 - FRONT FENCES AND WALLS ALONG PRIMARY STREET FRONTAGES TO BE ONE OF THE FOLLOWING:
 - THE SOLID PORTION OF THE FRONT FENCE TO BE NO MORE THAN 1.5M IN HEIGHT
 - IF FENCE HAS SOLID TRANSPARENT (GLASS) CONTAINS OPENINGS THAT ARE NOT MORE THAN 50% TRANSPARENT, THE MAXIMUM FENCE HEIGHT CAN BE 1.5M HIGH
 - FENCING TO SIDE BOUNDARIES MAY BE SCREEN FENCING OF 1.5M HIGH (MAXIMUM) AND EXTEND UP TO THE FRONT BUILDING LINE (BANKING).
 - FENCING TO SECONDARY STREET FRONTAGE (LEANNE COURT) TO HAVE SOLID TRANSPARENT TO ALLOW PASSIVE SURVEILLANCE FROM THE REAR OF THE PROPERTIES. DEVELOPER TO CONSTRUCT FENCING ALONG LEANNE COURT.

- SERVICE AREAS**
- ALL SERVICE (REAR BATHROOMS AND DRYING) CLOTHES LINES ARE TO BE SCREEN BEHIND THE REAR FACE OF THE DWELLING.

TABLE 1 - PLAN OF DEVELOPMENT TABLE						
PARAMETER	COURTYARD LOTS		VILLA LOTS		TERRACE LOTS	
	12m + LOT WIDTH 370m² - 480m²		9m + 11m LOT WIDTH		7.5 - 8.5m LOT WIDTH	
FRONT (ROAD) SETBACK				2.4m		
GARAGE SETBACK (MINIMUM)				5.4m		
REAR SETBACK	0.9m	1.2m	0.9m	1.2m	0.9m	1.2m
SECONDARY FRONTAGE				1.5m		
SIDE SETBACK						
BUILT-TO BOUNDARY*	0m	0m	0m	0m	0m	0m
NON-BUILT TO BOUNDARY	0.9m	1.2m	0.9m	0.9m	0.9m	1.2m
SITE COVER						
SITE COVER (MINIMUM)**		60%		60%		70%
BUILDING HEIGHT				2 STOREY (3m)		
ON SITE PARKING REQUIREMENTS						
GARAGE TYPE	SINGLE, TANDUM OR DOUBLE GARAGES ACCEPTABLE		SINGLE, TANDUM OR DOUBLE GARAGES ACCEPTABLE FOR LOTS GREATER THAN 10m WIDE		SINGLE GARAGES UP TO 1.5m WIDE OR TANDUM GARAGES ARE ACCEPTABLE	
GARAGE WIDTH (MINIMUM)	NO MORE THAN 40% OF THE LOT FRONTAGE (WHERE THE FRONTAGE IS LESS THAN 12.5m)					
DRIVEWAY WIDTH (MINIMUM)	DOUBLE GARAGES 1.5m AT THE LOT BOUNDARY		1.5m AT THE LOT BOUNDARY		1.5m AT THE LOT BOUNDARY	
PRIVATE OPEN SPACE (POS)						
AREA AND DIMENSIONS (GROUND OR UPPER LEVELS)	ONE ROOM OR ONE BEDROOM HOUSE (DWELLING UNIT)		TWO BEDROOM HOUSE (DWELLING UNIT)		3 OR MORE BEDROOM HOUSE (DWELLING UNIT)	
OTHER DESIGN ELEMENTS	MINIMUM 5m² WITH A MINIMUM DIMENSION OF 1.5m		MINIMUM 8m² WITH A MINIMUM DIMENSION OF 1.5m		MINIMUM 10m² WITH A MINIMUM DIMENSION OF 2.0m	
	MAXIMUM GRADE OF 1:10 ACCESSIBLE FROM A LIVING ROOM WITH ADJACENT SPACE TO ACCOMMODATE A TABLE, CHAIR, PLANTING, BBQ AND SHADE					

* REFER TO PLAN OF DEVELOPMENT FOR LOCATIONS. MAXIMUM DELT TO BOUNDARY WALL LENGTHS 15m (OR 50% OF THE LOT DEPTH) AND A MAXIMUM HEIGHT OF 1.5m
** SITE COVERS THE PROPORTION OF THE SITE COVERED BY BUILDINGS AND STRUCTURES, INCLUDING ROOF OVERHANGS WITH IMPERVIOUS MATERIALS (AS SPECIFIED IN THE GREATER FLAGE (ONE PROPOSED DEVELOPMENT SCHEME).

TABLE 2 - MINIMUM Rw RATING FOR EACH RELEVANT NOISE CATEGORY (EXCERPT FROM TRAFFIC NOISE ASSESSMENT BY RENZO TONIN & ASSOCIATES, TABLE 6, DATED JAN 2026)			
NOISE CATEGORY	MINIMUM TRANSPORT NOISE REDUCTION (Rw) REQUIRED FOR HABITABLE ROOMS	COMPONENT OF BUILDING'S EXTERNAL ENVELOPE	MINIMUM Rw REQUIRED
4	40	GLAZING	43
		EXTERNAL WALLS	52
		ROOF	45
		FLOORS	51
		ENTRY DOORS	35
3	30	GLAZING	38 (TOTAL AREA OF GLAZING FOR A HABITABLE ROOM B GREATER THAN 1.5m²) 35 (TOTAL AREA OF GLAZING FOR A HABITABLE ROOM B LESS THAN OR EQUAL TO 1.5m²)
		EXTERNAL WALLS	47
		ROOF	41
		FLOORS	46
		ENTRY DOORS	33
2	20	GLAZING	25 (TOTAL AREA OF GLAZING FOR A HABITABLE ROOM B GREATER THAN 1.5m²) 22 (TOTAL AREA OF GLAZING FOR A HABITABLE ROOM B LESS THAN OR EQUAL TO 1.5m²)
		EXTERNAL WALLS	41
		ROOF	36
		FLOORS	40
		ENTRY DOORS	33
1	10	GLAZING	21 (TOTAL AREA OF GLAZING FOR A HABITABLE ROOM B GREATER THAN 1.5m²) 18 (TOTAL AREA OF GLAZING FOR A HABITABLE ROOM B LESS THAN OR EQUAL TO 1.5m²)
		EXTERNAL WALLS	35
		ROOF	30
		FLOORS	35
		ENTRY DOORS	29

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approved, ... E. NICHOLAOU, SK002



- LEGEND**
- LOT 16 ON SP44224 (TOTAL AREA = 15.86m²)
 - STAGE 1 BOUNDARY
 - STAGE 2 BOUNDARY
 - COURTYARD LOTS - LOT NUMBER AND LOT AREA
 - INDICATIVE BUILDING ENVELOPE BASED ON REQUIRED SETBACKS
 - VILLA LOTS - LOT NUMBER AND LOT AREA
 - INDICATIVE BUILDING ENVELOPE BASED ON REQUIRED SETBACKS
 - TERRACE LOTS - LOT NUMBER AND LOT AREA
 - INDICATIVE BUILDING ENVELOPE BASED ON REQUIRED SETBACKS
 - NOMINATED BUILT TO BOUNDARY WALL (PENDING CIVIL ENGINEERING INPUT)
 - INDICATIVE LANDSCAPE ISLAND (REFER TO LANDSCAPE CONCEPT PLAN)
 - ACQUISITION FENCE (IN ACCORDANCE WITH ACQUISITION ENGINEERS REPORT)
 - 1.5m WIDE DRAINAGE EASEMENT
 - ON STREET PARALLEL PARKING (INDICATIVE LOCATION)
 - INDICATIVE LOCATION OF GARAGE
 - INDICATIVE LOCATION OF DRIVEWAY

NOTES

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PLAN OF DEVELOPMENT - SHEET 1

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approved, ... E. NICHOLAOU, SK003



LEGEND

- LOT 10 ON RP210430 (TOTAL AREA = 15.86ha)
- STAGE 1 BOUNDARY
- STAGE 2 BOUNDARY
- COURTYARD LOTS - LOT NUMBER AND LOT AREA
- INDICATIVE BUILDING ENVELOPE BASED ON REQUIRED SETBACKS
- VILLA LOTS - LOT NUMBER AND LOT AREA
- INDICATIVE BUILDING ENVELOPE BASED ON REQUIRED SETBACKS
- TERRACE LOTS - LOT NUMBER AND LOT AREA
- INDICATIVE BUILDING ENVELOPE BASED ON REQUIRED SETBACKS
- NOMINATED BUILT TO BOUNDARY WALL (PENDING CIVIL ENGINEERING INPUT)
- INDICATIVE LANDSCAPE ISLAND (REFER TO LANDSCAPE CONCEPT PLAN)
- ACQUISITION FENCE (IN ACCORDANCE WITH ACQUISITION REPORT)
- 1.5m WIDE DRAINAGE EASEMENT
- ON STREET PARALLEL PARKING (INDICATIVE LOCATION)
- INDICATIVE LOCATION OF GARAGE
- INDICATIVE LOCATION OF DRIVEWAY

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PLAN OF DEVELOPMENT - SHEET 2

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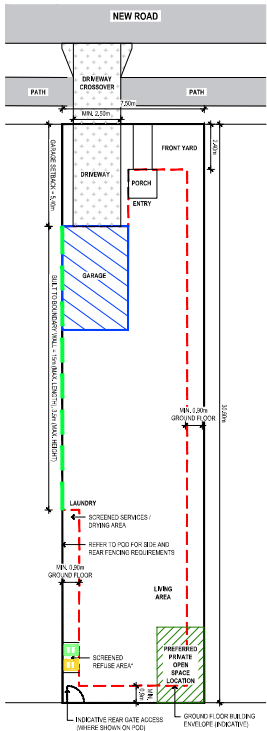
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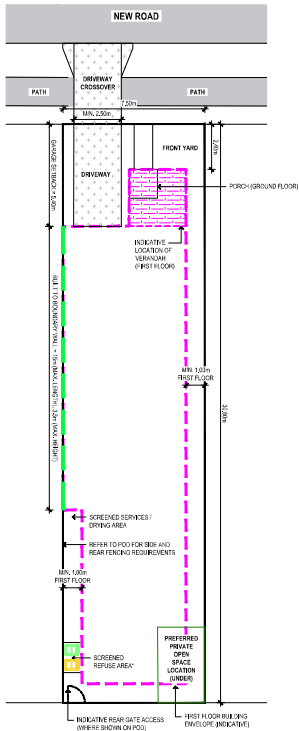
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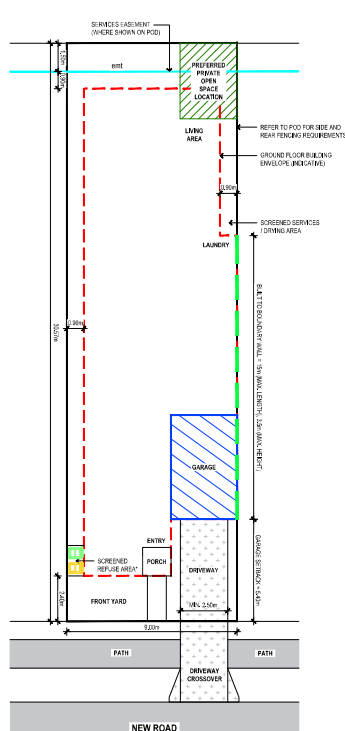
PLAN OF DEVELOPMENT
SCALE 1:250



EXAMPLE OF TYPICAL TERRACE LOT (SINGLE DRIVEWAY)
WITH NORTH FACING FRONTAGE < THAN 250sqm
GROUND FLOOR
SCALE 1:100



EXAMPLE OF TYPICAL TERRACE LOT (SINGLE DRIVEWAY)
WITH NORTH FACING FRONTAGE < THAN 250sqm
FIRST FLOOR
SCALE 1:100



EXAMPLE OF TYPICAL VILLA LOT WITH
SOUTH FACING FRONTAGE < THAN 300sqm
GROUND FLOOR
SCALE 1:100

LEGEND

- LOT BOUNDARY, REFER TO POD FOR LOT NUMBER AND AREA
- MAXIMUM BUILDING ENVELOPE (GROUND FLOOR)
- MAXIMUM BUILDING ENVELOPE (FIRST FLOOR)
- BUILDING SETBACK DIMENSIONING
- GROUND FLOOR BUILDING ENVELOPE (INDICATIVE)
- 5m WIDE DRAINAGE EASEMENT
- INDICATIVE LOCATION OF GARAGE
- INDICATIVE LOCATION OF POS
- INDICATIVE LOCATION OF VERANDAH
- INDICATIVE LOCATION OF DRIVEWAY
- SCREENED REFUSE AREA *POD REFUSE COLLECTION POINTS REFER TO TYPING ENGINEERS DRAWINGS FOR DETAILS
- REAR ACCESS GATE, REFER TO POD FOR SPECIFIC LOCATIONS

0 1 2 3 4 5m
SCALE 1:100 (NOT TO SCALE)

PRELIMINARY

rev	description	drawn	date
B	REVISED ISSUE	JS	18.03.26
A	INITIAL ISSUE	JS	09.03.26

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197 - 201 TEVIOT ROAD, GREENBANK QLD 4124
RESIDENTIAL SUBDIVISION DESIGN
TYPICAL DWELLING TYPES - SHEET 1

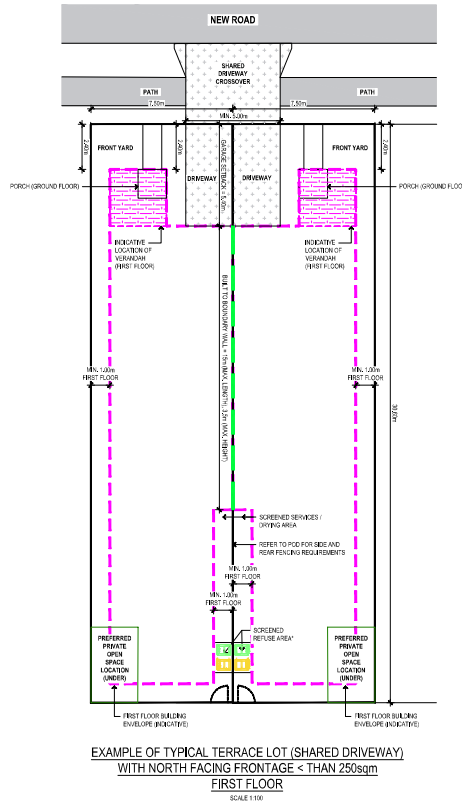
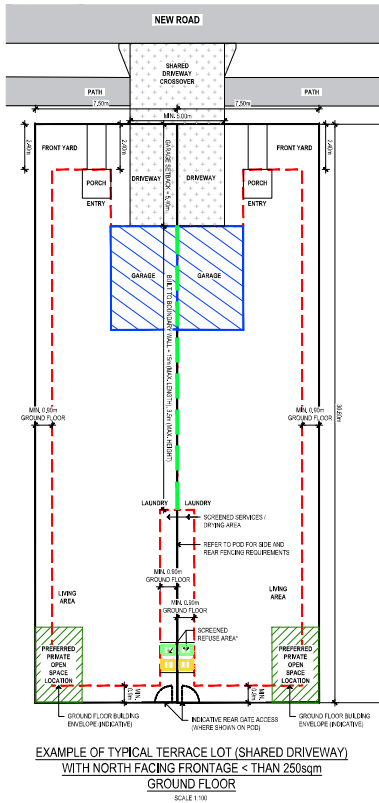
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scale | 1:100 for A1 job no. | 2510-005
date | MAR 2026 rev no. | B

approved, ... E. NICHOLAOU, SK005



- LEGEND**
- LOT BOUNDARY, REFER TO POD FOR LOT NUMBER AND AREA
 - WALL/RAIN BUILDING ENVELOPE (GROUND FLOOR)
 - WALL/RAIN BUILDING ENVELOPE (FIRST FLOOR)
 - BUILDING SETBACK DIMENSIONS
 - WALL/RAIN BUILT TO BOUNDARY WALL (PENDING CIVIL ENGINEERING INPUT)
 - 5.0m WIDE DRAINAGE GUTTER
 - INDICATIVE LOCATION OF GARAGE
 - INDICATIVE LOCATION OF POOL
 - INDICATIVE LOCATION OF VERANDAH
 - INDICATIVE LOCATION OF DRIVEWAY
 - SCREENED REFUSE AREA *FOR REFUSE COLLECTION POINTS, REFER TO TRADING ENGINEERS DRAWINGS FOR DETAILS
 - REAR ACCESS GATE, REFER TO POD FOR SPECIFIC LOCATIONS

0 1 2 3 4 5m
SCALE - 1:100 (GRAPHIC SCALE)

PRELIMINARY

rev	description	drawn	date
A	INITIAL ISSUE	JS	18.03.26
HATIA DALBY PTY LTD 197 - 201 TEVIOT ROAD, GREENBANK QLD 4124 RESIDENTIAL SUBDIVISION DESIGN TYPICAL DWELLING TYPES - SHEET 2			

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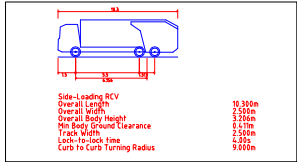
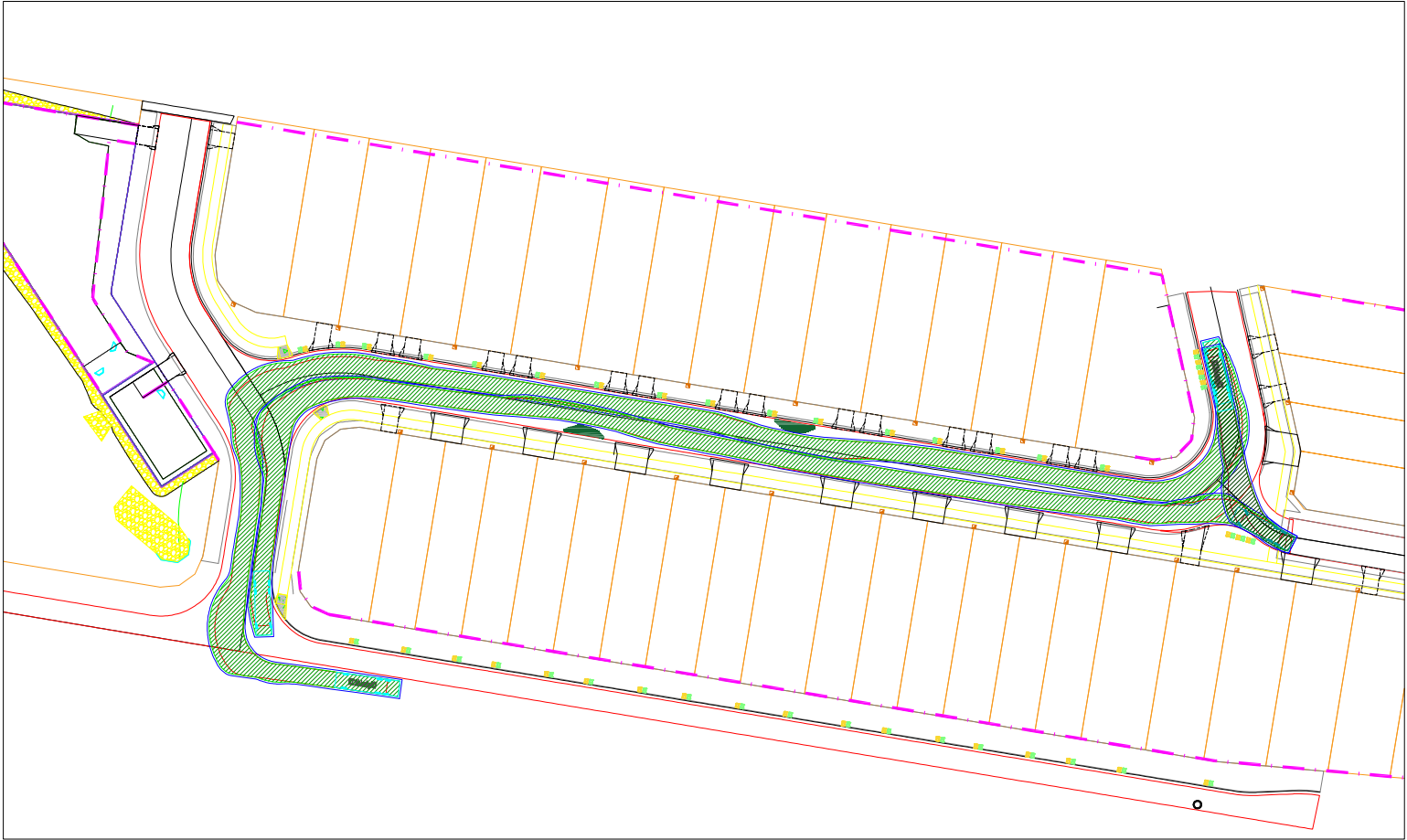
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scale | 1:100 for A1 job no. | 2510-005
date | MAR 2026 rev no. | A

approved, ... E. NICHOLAOU, SK006

ATTACHMENT 2

Bin Presentation Plan and Refuse Vehicle Swept Path Assessment



drawing title

197-201 Teviot Road Greenbank
Refuse

drawn RQ

Scale: 1:500 @ A3

date: 18/03/28

2219_TPA16

SK.05

B

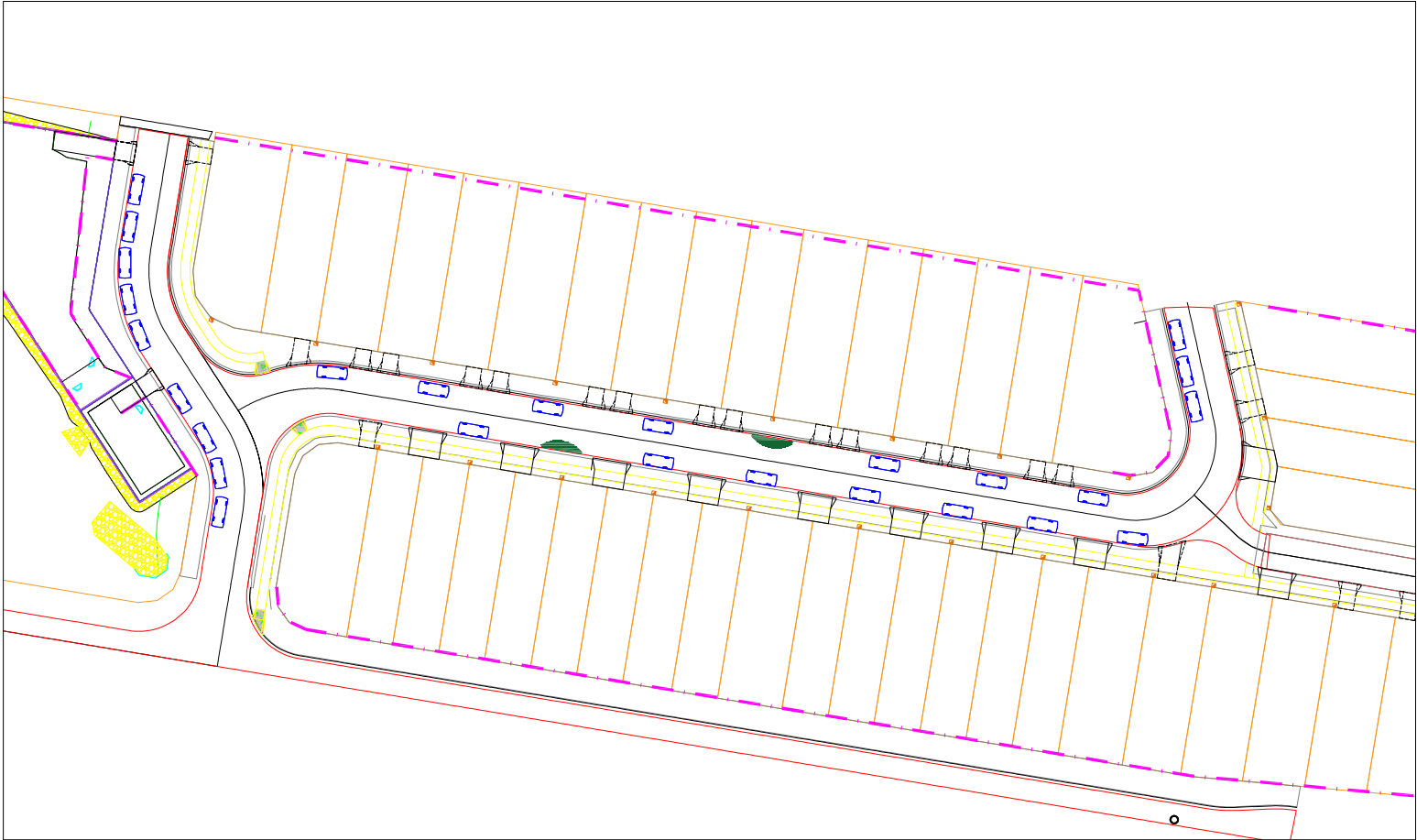
project no.

drawing no.

rev

ATTACHMENT 3

On-Street Parking Plan



Lots	42
On-Street Spaces	26
rate	0.62 spaces / lot
surplus Leanne Ct	15 spaces
max rate	0.97 spaces / lot

drawing title			
197-201 Teviot Road Greenbank Parking Plan			
drawn: RQ	Scale: 1:500 @ A3	date: 16/03/26	
This drawing is provided to Q Traffic by ePlan. This drawing is not for decision purposes or for			
2219_TPA16	SK.04	B	
project no.	drawing no.	rev	