



New Beith, Context Area 2 – Lot 4 Precinct B ROL

Traffic Impact Statement

Fraser Property Australia

Greenhouse West Village, Level 8,
97 Boundary Street, West End QLD 4101

Prepared by:

SLR Consulting Australia Pty Ltd

SLR Project No.: 620.31041.00001

31 October 2025

Revision: 3.0

**PLANS AND DOCUMENTS
referred to in the PDA
DEVELOPMENT APPROVAL**

Approval no: DEV2025/1681

Date: 15 December 2025



Revision Record

Revision	Date	Prepared By	Checked By	Authorised By
3.0	31 October 2025	Naristi Neale	Kris Stone	Kris Stone
2.0	15 August 2025	Naristi Neale	Kris Stone	Kris Stone
1.0	10 July 2025	Naristi Neale	Kris Stone	Kris Stone

Basis of Report

This report has been prepared by SLR Consulting Australia Pty Ltd (SLR) with all reasonable skill, care and diligence, and taking account of the timescale and resources allocated to it by agreement with Fraser Property Australia (the Client). Information reported herein is based on the interpretation of data collected, which has been accepted in good faith as being accurate and valid.

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SLR disclaims any responsibility to the Client and others in respect of any matters outside the agreed scope of the work.



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1.0 Introduction

1.1 Overview

SLR Consulting Australia (SLR) has been engaged by Frasers Property Australia (**Frasers**) to provide traffic engineering and transport planning advice in relation to the New Beith development, located within Context Area 2 - Lot 4 of the Greater Flagstone Priority Development Area (PDA).

This advice relates specifically to the proposed Reconfiguring of Lot (ROL) application for Precinct B. Plans illustrating the proposed development prepared by RPS and Colliers are included in **Appendix A**.

1.2 Purpose and Scope

This Traffic Impact Statement (TIS) has been prepared to inform the Economic Development Queensland (EDQ) assessment of the Precinct B ROL by identifying and addressing the relevant traffic and transport matters.

This report addresses internal design matters, and consistency with prior approvals including the *Movement Network Infrastructure Master Plan* (IMP) endorsed on 2 October 2025.



2.0 Existing Situation

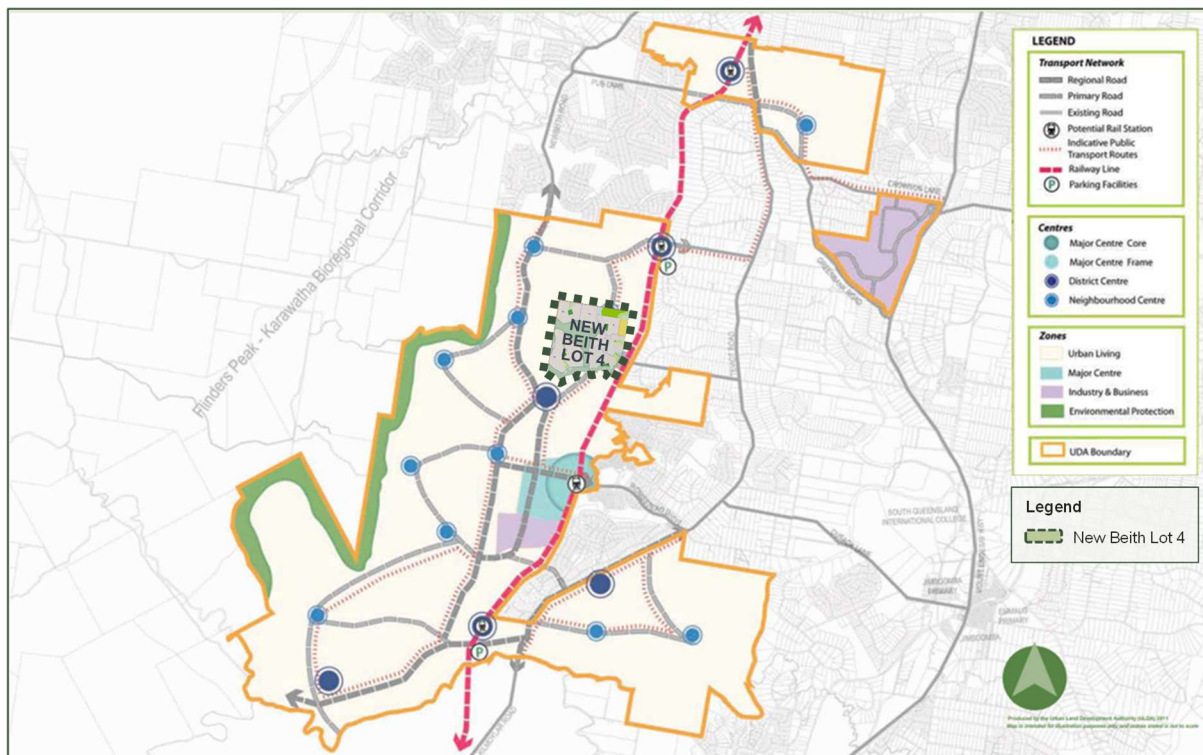
2.1 Site Location

The Greater Flagstone Priority Development Area (PDA) is defined as a priority growth zone in the State Government's *South-East Queensland (SEQ) Regional Plan 2009-2031*. The Greater Flagstone PDA is a 7,188-hectare area of development land located in South Logan, about 40km southwest of Brisbane CBD. The PDA is forecast to house approximately 145,000 people, providing 55,000 dwellings.

The subject New Beith site forms part of Context Area 2 – Lot 4, see **Figure 1**.

Preliminary planning for the New Beith site indicates an approximate residential development yield of 2,150 dwellings.

Figure 1 New Beith (Context Area 2 – Lot 4) Location within PDA



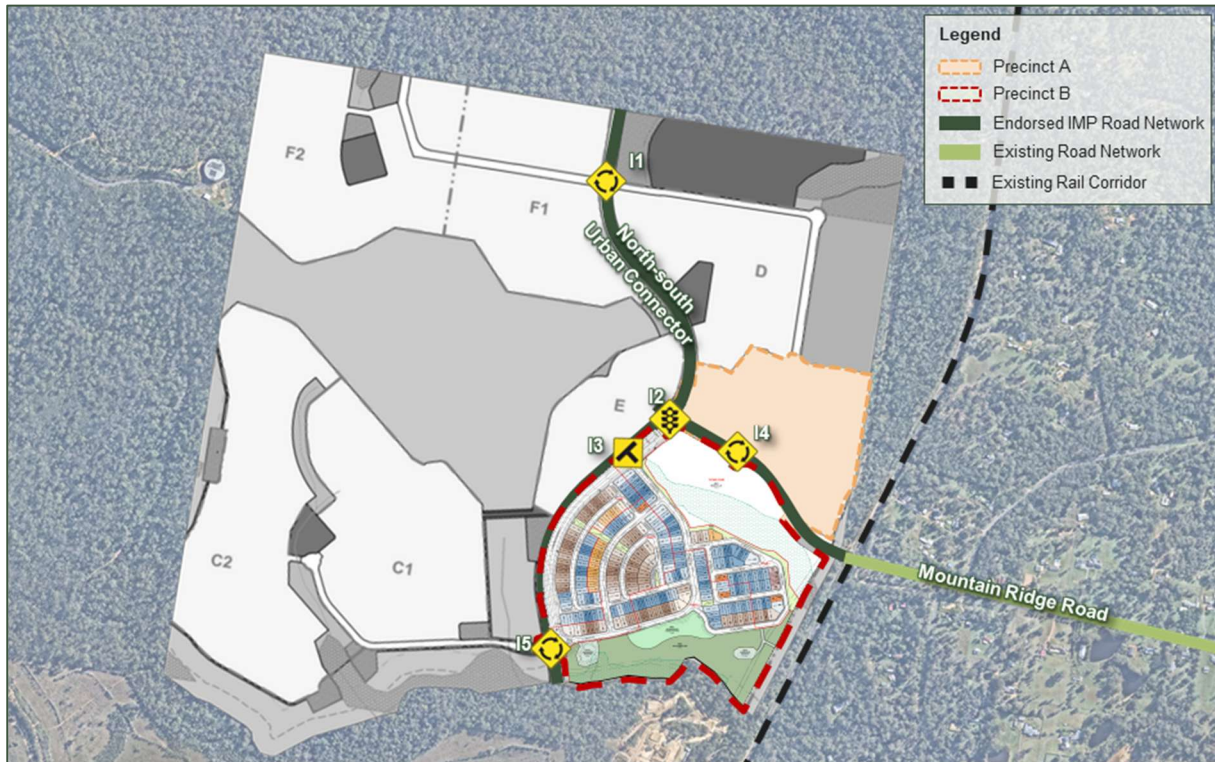
Source: Greater Flagstone Development Scheme, October 2011 (Modified by SLR)

The Precinct B development is illustrated at **Figure 2** in the context of the broader planned New Beith Context Area 2 – Lot 4 site.

A ROL development application has been Precinct A approved in October 2025 (the location of Precinct A can be seen in **Figure 2**).



Figure 2 Precinct B Development within Broader New Beith Context Area 2 – Lot 4



2.2 Road Network

The majority of the road network ultimately planned within the Flagstone PDA has not yet been constructed. At present, only Flagstone City (Context Area 1) south of New Beith has commenced the staged construction of some higher order roads to enable initial development stages.

Neither the Precinct A or B sites are currently accessible via existing roads. An Operational Works application (OPW) has been lodged with Logan City Council (Council) regarding the upgrading of Mountain Ridge Road west of Pebble Creek Way and a new road bridge over the rail corridor.

This proposed infrastructure would enable vehicle access to Precinct A via Mountain Ridge Road consistent with the Flagstone PDA planning which will then in turn provide access to Precinct B.

Precinct B will also enable the road connection to the adjoining southern development land.

2.3 Public Transport

At present, the Flagstone PDA has limited public transport and is only serviced by a single Translink bus route (#535) with an approximate 30-minute service frequency in the peak hour direction.

The New Beith site is located adjacent to the Sydney to Brisbane rail transport corridor; however, there are no existing or planned passenger stations.



2.4 Active Transport

At present, there is limited active travel infrastructure that connects the Flagstone PDA to the broader movement network. Council's *Active Travel Infrastructure Planning Scheme Policy* (PSP5) mapping denotes Mountain Ridge Road as forming part of the Major Future Cycle Network (**Figure 3**).

Part of this infrastructure east of Pebble Creek Way has already been constructed.

The OPW application discussed in **Section 2.2** incorporates new off-road paths in both verges (and across the bridge) that could connect to the existing infrastructure east of Pebble Creek Way and deliver the planned PSP network.

Figure 3 Council Infrastructure PSP Cycle Network



Source: Logan City's Infrastructure Planning Scheme Policy



3.0 Background Planning and Approvals

3.1 Overview

The traffic and transport background relevant to the broader New Beith site and the Precinct B development is summarised below:

- i. Mesoscopic modelling was prepared by Veitch Lister Consulting (VLC) in 2019 for the broader Flagstone Priority Development Area (PDA).
- ii. Meso and microsimulation modelling was prepared by Bitzios Consulting in 2021 for the Flagstone PDA.
- iii. The *Greater Flagstone PDA Infrastructure Charging Offset Plan (ICOP)* was prepared by Economic Development Queensland (EDQ) in 2020. The plans were informed by the prior traffic modelling.
- iv. The *Greater Flagstone PDA Infrastructure Plan Background Report* was prepared by EDQ in 2022. This supported the *Greater Flagstone PDA Development Charges and Offset Plan (DCOP)* which became effective in July 2022 and superseded the ICOP.
- v. In 2023, the 2021 Bitzios Aimsun modelling was revised to reflect updated development and road network planning. The modelling was submitted as part of the New Beith IMP.
- vi. In January 2024, the *New Beith Context Plan* including the *Movement Network IMP (Version 5.1 dated 8 December 2023)*, was endorsed by DILGP.
- vii. In October 2025, an updated *Movement Network IMP (Version 5.2 dated 12 August 2025)*, was endorsed by DILGP.
- viii. An ROL application for Precinct A was submitted in 2024 and has since been approved with draft conditions as of October 2025.

3.2 Bitzios Traffic Modelling

The 2021 Bitzios modelling [i] informed the strategic road network planning including the now superseded ICOP [iii] and the current DCOP [iv]. The modelling was updated by Bitzios in 2023 [v] so that it was reflective of the updated Council, Flagstone PDA, and New Beith development and road network planning.

The updated 2023 modelling forms part of the endorsed New Beith IMP [vii].

The 2023 Bitzios modelling report concluded that the assessed road network would satisfactorily accommodate the forecast traffic inclusive of the Flagstone PDA and New Beith site (and by extension Precinct B).

3.3 New Beith Context Plan and Movement IMP

The New Beith Context Plan and Movement IMP were endorsed in 2023 and 2025 respectively and supersede the DCOP mapping. These documents outline the latest planning for New Beith, including:

- Road and active travel hierarchy.
- Road cross-sections and verges.
- Road staging and development/traffic thresholds.
- Intersection location and control types.



4.0 Proposed Development

4.1 Development Yield

The Precinct B plans prepared by RPS and Colliers are included **Appendix A**. The plans indicate 261 residential dwelling lots that are comprised of a mixture of types with road frontages that vary between 13.5-21.5m. The ROL application also proposes the formation of a Neighbourhood Recreation Park.

4.2 Vehicular Access and Road Network

Vehicular access to Precinct B is proposed via the North-South Trunk Connector that is established by the IMP.

The Precinct B plans include parts of the road network ultimately envisaged as part of the endorsed New Beith IMP. **Figure 4** depicts the following:

- The North-South Urban Connector (Road 34B).
- North-South Urban Connector (Road 34B)/Esplanade Neighbourhood Connector roundabout intersection. IMP = RI006A/B.

Further to the above, a second access via the North-South Urban Connector, located at the northern edge of Precinct B, was modelled by Bitzios as part of the Movement IMP. This intersection is ultimately planned as a left-in/left-out intersection as shown in **Figure 4**.

Figure 4 Precinct B Road Network



The remainder of the lower-order streets within Precinct B are classified as Neighbourhood Collector Streets, Neighbourhood Access Streets or similar consistent with the IMP and EDQ's *Street and Movement Network Guidance* (PDA guideline no.06).

4.3 Public Transport

The Precinct B plans include parts of the bus stop network ultimately envisaged by the endorsed New Beith IMP. **Figure 5** depicts the following:

- Indented bus stop (northbound) located on North-South-Urban Connector Road (34A) south of Mountain Ridge Road.
- Indented bus stop (north and southbound) located on North-South-Urban Connector Road (34B) south of Mountain Ridge Road.

Figure 5 Precinct B – Bus Stops Locations



These stops are consistent with the indicative arrangement shown on the IMP and will facilitate bus services running north-south through the broader Flagstone PDA. Consistent with IMP Item 5.26, the stops:

- Are designed in accordance with the Department of Transport and Main Roads (DTMR) *Public Transport Infrastructure Manual* (PTIM, 2020).
- Are indented on major Trunk Connector roads.



4.4 Active Transport

The Precinct B plans include parts of the active travel network ultimately envisaged by the endorsed IMP. Drawing No. STG-01 in Error! Reference source not found. depicts the following:

- Off-road cycle track and footpath along the North-South Trunk Road.
- Off-road paths in all lower-order Neighbourhood Connector and Access Streets.

These facilities are consistent with the indicative arrangements described in the IMP (Context Area Plan and IMP Item 5.28).



5.0 Road Network Assessment

5.1 Overview

The road and intersection arrangements were assessed as part of the *Flagstone City – Traffic Modelling Report* (Bitzios, December 2023). This modelling forms part of the endorsed IMP (Annexure 3).

The following sections summarise a review of the 2023 Bitzios modelling to establish that it is still representative with respect to the of the proposed Precinct B development.

5.2 Development Yield

The 2023 Bitzios modelling (Figure 5.4 and Table 5.1) assessed the Precinct B development as zone 1130 comprising 287 dwellings. The proposed development yield is 26 dwellings less than the modelling assumption. Accordingly, the 2023 Bitzios modelling that forms part of the endorsed IMP is conservative and valid.

5.3 Road Network

The 2023 Bitzios modelling assessed a road network (Figure 5.2) and intersection arrangements (Figure 5.2 and Figure 6.1) that are consistent with that proposed as part of Precinct B application. The following intersections was identified to provide access to Precinct B:

- Northern Access: Left-in/left-out
- Southern Access: Roundabout

Accordingly, the 2023 Bitzios modelling that forms part of the endorsed IMP is still valid.

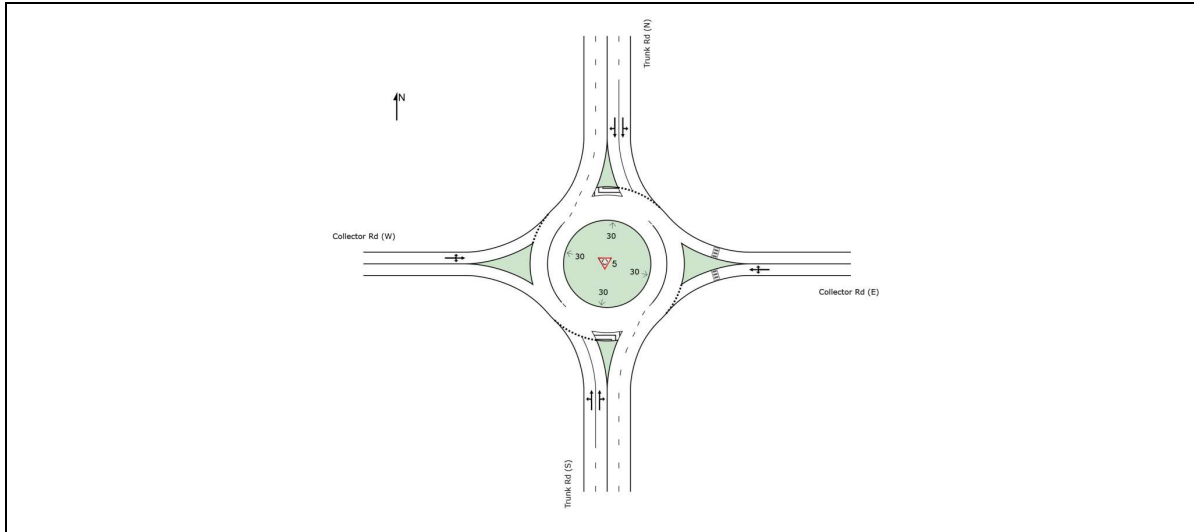
5.4 Modelling Findings

The 2023 Bitzios modelling assessed the road and intersection operational capacity at the 2066 design horizon.

The road capacity was evaluated using the microsimulation flow outputs (Section 5.1). The report concluded that link delays and traffic flow speeds were satisfactory. The intersection capacity was evaluated with SIDRA Intersection and the microsimulation output turning movements (Section 6). The assessed intersection arrangement and results for the trunk intersections delivered (in part or full) by Precinct B are reproduced **Figure 6** and **Table 1**.



Figure 6 Assessed Intersection Arrangement



Source: Flagstone City: Traffic a Traffic Modelling Assessment by Bitzios Consulting, December 2023

Table 1 Summarised SIDRA Intersection Results (Year 2066)

ID	Peak Period	Degree of Saturation (v/c)	Avg. Delay (s)	Level of Service
4	AM	0.59	6	A
	PM	0.64	5	A

Source: Flagstone City: Traffic a Traffic Modelling Assessment by Bitzios Consulting, December 2023

The 2023 Bitzios findings concluded that the intersection arrangement provided sufficient capacity at the 2066 design horizon and could accommodate the broader Flagstone PDA, inclusive of the New Beith site (and Precinct B).



6.0 Movement Network Design

6.1 Overview

The Precinct B movement network has been designed based on the endorsed IMP (Version 5.2) and EDQ *Street and Movement Network Guidance (PDA guideline no.06)*.

6.2 Road/Street Cross-Sections

The adopted road hierarchy (**Figure 7**) and cross-sections (**Table 2**) are consistent with those referenced as part of the endorsed IMP.

Figure 7 Precinct B – Road Hierarchy



It should be noted that the 17.5m Esplanade Neighbourhood Connector cross-section established in the IMP allows for indented parking on both sides of the road. However, for these road types in Precinct B, indented parking is only included on one side. This is proposed given the limited parking that can be achieved between the closely spaced residential driveways, and the parking supply that can be achieved on the opposite side of the road. This is deemed acceptable given the parking discussion in **Section 6.4**.

The 13.5 m Esplanade Neighbourhood Access Street established in the IMP allows for one pedestrian path located in the verge fronting the residential lots. However, for these road types in Precinct B, the path is located in the Open Space in lieu of a path in the dwelling side verge. This is considered acceptable as it delivers a uninterrupted path along the entirety of this street and provides a direct connection to the Neighbourhood Recreation Park and Open Space off-street paths that connect to other development precincts.



Table 2 Road Hierarchy and Cross-Sections (Endorsed IMP v5.2)

Classification (Road Reserve)	Road Cross-Section
<p>Four Lane Trunk Connector (33.0m)</p>	
<p>Green Neighbourhood Connector (21.5m)</p>	
<p>Esplanade Neighbourhood Connector (17.5m)</p>	
<p>Green Neighbourhood Access Road (19.5m)</p>	
<p>Neighbourhood Access Road (15.5m)</p>	
<p>Esplanade Neighbourhood Access Street (13.5m)</p>	



6.3 Active Travel Design

Active travel infrastructure has been designed in accordance with the following guidance:

- *Street and Movement Network Guidance* (EDQ, 2019).
- *Selection and Design of Cycle Tracks* (DTMR, 2019).
- *Raised Priority Crossings for Pedestrians and Cycle Paths* (DTMR, 2019).

The separate cycle track and footpath is located in the eastern verge of the North-South Trunk Road. The cycle track is 3.0m wide and the footpath is 1.5m wide. These paths are appropriate for the type, volume and directional composition of users in this segment. The shared path crosses the two access intersections and will be:

- **Northern Access to Precinct B** (left-in-left-out) – A raised priority crossing is proposed at this location consistent with 2.6.1(a) of the DTMR guidance.
- **Southern Access to Precinct B, eastern approach of the roundabout** – A raised priority crossing is proposed at this location consistent with 2.6.4 of the DTMR guidance.

6.4 Car Parking

On-site car parking should be provided consistent with the Greater Flagstone Development Scheme.

On-street car parking is provided within the road and street pavement consistent with EDQ's *Neighbourhood Planning and Design Guidance* (PDA guideline no.05). RPS has prepared a Car Parking Analysis plan (**Appendix A**) that indicates a conservative 196 on-street parking spaces can be readily achieved.

Based on the proposed 261 residential dwellings within Precinct B, this supply would equate to approx. 0.75 spaces per dwelling which is consistent with the range outlined in the EDQ guidance.

6.5 Development Servicing

The road design is consistent with the endorsed IMP cross-sections so should therefore accommodate typical design vehicles.



7.0 Summary & Conclusions

SLR Consulting Australia (SLR) has been engaged by Frasers Property Australia (**Frasers**) to provide traffic engineering and transport planning advice in relation to the New Beith development, located within Context Area 2 - Lot 4 of the Greater Flagstone Priority Development Area (PDA).

This advice relates specifically to the proposed Reconfiguring of Lot (ROL) application for Precinct B. Plans illustrating the proposed development prepared by Urbis are included in **Appendix A**.

Based on the analysis and discussion documented herein, the following is concluded:

- The broader New Beith site and Precinct B is not currently accessible by vehicle.
- Access is reliant on an upgraded Mountain Ridge Road and a new road bridge over the rail corridor, both of which are the subject of current Operational Works applications being assessed by Logan City Council.
- The proposed Precinct B movement network is consistent with the New Beith *Infrastructure Master Plan* (Version 5.2) endorsed in October 2025, including:
 - The road hierarchy and number of lanes
 - The cross-sectional design of roads
 - The location and form of road intersections
 - The provision for indented bus stop infrastructure on higher order routes
 - The provision for active travel infrastructure
 - The provision for on-street car parking.
- The *Flagstone City – Traffic Modelling Report* (Bitzios, 2023) incorporated the subject Precinct B development. The modelling assumptions made with respect to development yield and road network are consistent with what is proposed. Accordingly, the prior findings remain valid. No further modelling or capacity assessment is warranted.





Appendix A Development Plans

New Beith, Context Area 2 – Lot 4 Precinct B ROL

Traffic Impact Statement

Fraser Property Australia

SLR Project No.: 620.31041.00001

31 October 2025



Prepared by Urbis for Frasers Property October 2025



ROUND MOUNTAIN

Precinct B - Application Plans





- LEGEND**
- SITE BOUNDARY
 - PRECINCT BOUNDARY
 - STAGE BOUNDARY
 - CONTOURS (1m INTERVALS)
 - EASEMENT BOUNDARY
 - RIPARIAN CORRIDOR BOUNDARY
 - NEIGHBOURHOOD PARK
 - LINEAR PARK
 - PEDESTRIAN LINKAGE (ROAD RESERVE)
 - MAJOR LINEAR PARK
 - STORMWATER DETENTION
 - BALANCE LOT
 - FUTURE RAIL CORRIDOR
 - PROPOSED ROAD DEDICATION
 - ENTRY STATEMENT
 - BIO RETENTION BASINS
 - BIN PAD LOCATION

DEVELOPMENT STATISTICS

Lot Width	Typical Lot Size	No. of Lots	%
15m Depth Allotments - Urban Twin			
15m	225m ²	8	3.1%
Subtotal		8	3.1%
26m Depth Allotments - Terrace Twin			
9m	234m ²	4	1.5%
10m+	260m ²	2	0.8%
Subtotal		6	2.3%
28m Depth Allotments			
10m	280m ²	4	1.5%
12.5m	350m ²	8	3.1%
14m	392m ²	4	1.5%
15m	420m ²	0	0.0%
17m	476m ²	4	1.5%
Subtotal		20	7.6%
30m Depth Allotments			
10m	300m ²	26	9.9%
12.5m	375m ²	44	16.8%
14m	420m ²	43	16.4%
15m	450m ²	19	7.3%
17m	510m ²	10	3.8%
Subtotal		142	54.2%
32m Depth Allotments			
10m	320m ²	20	7.6%
12.5m	400m ²	31	11.8%
14m	448m ²	19	7.3%
15m	480m ²	12	4.6%
17m	544m ²	4	1.5%
Subtotal		86	32.8%
TOTAL LOTS		262	100.0%
AVERAGE LOT SIZE			420m²

KEY PLAN - Scale 1:40,000 @ A3



DENSITY (NET RESIDENTIAL DENSITY) 15.1 dw/ha

LAND USE BUDGET		
Land Use	Area	%
Residential Allotments	11.18 ha	33%
Trunk Road Reserve	2.52 ha	7%
Road Reserve	5.84 ha	17%
Linear Park	0.64 ha	2%
Major Linear Park	5.07 ha	15%
Neighbourhood Park	1.00 ha	3%
Pedestrian Links	0.28 ha	1%
Stormwater	0.55 ha	2%
Balance Lot	7.15 ha	21%
TOTAL SITE AREA	34.24 ha	100%



ROUND MOUNTAIN, NEW BEITH - PRECINCT B
RECONFIGURING OF A LOT - OVERALL



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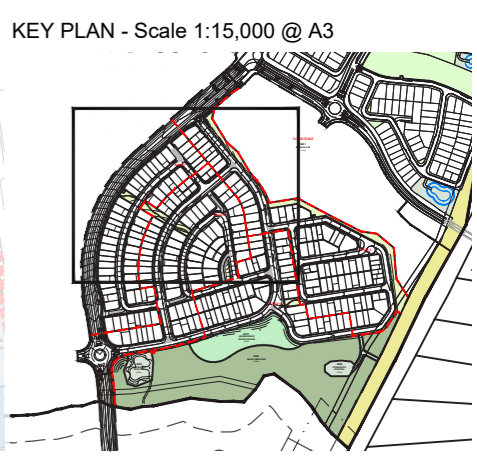


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JOB NO: P0055018
DWG NO: ROL-00



- LEGEND**
- SITE BOUNDARY
 - - - PRECINCT BOUNDARY
 - - - STAGE BOUNDARY
 - CONTOURS (1m INTERVALS)
 - ▨ RIPARIAN CORRIDOR BOUNDARY
 - ▨ NEIGHBOURHOOD PARK
 - ▨ LINEAR PARK
 - ▨ PEDESTRIAN LINKAGE (ROAD RESERVE)
 - BALANCE LOT
 - BIN PAD LOCATION
- RESIDENTIAL**
- TERRACE**
- ▨ 15.0m URBAN TWIN
- 28m DEEP**
- ▨ 10m x 28m VILLA LOT
 - ▨ 12.5m x 28m PREMIUM VILLA LOT
 - ▨ 14m x 28m COURTYARD LOT
 - ▨ 15m x 28m PREMIUM COURTYARD LOT
 - ▨ 17m x 28m TRADITIONAL LOT
- 30m DEEP**
- ▨ 10m x 30m VILLA LOT
 - ▨ 12.5m x 30m PREMIUM VILLA LOT
 - ▨ 14m x 30m COURTYARD LOT
 - ▨ 15m x 30m PREMIUM COURTYARD LOT
 - ▨ 17m x 30m TRADITIONAL LOT
- 32m DEEP**
- ▨ 10m x 32m VILLA LOT
 - ▨ 12.5m x 32m PREMIUM VILLA LOT
 - ▨ 14m x 32m COURTYARD LOT
 - ▨ 15m x 32m PREMIUM COURTYARD LOT
 - ▨ 17m x 32m TRADITIONAL LOT

JOINS DRAWING ROL-03



JOINS DRAWING ROL-02



ROUND MOUNTAIN, NEW BEITH - PRECINCT B
 RECONFIGURING OF A LOT - ROL 01



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 DWG NO: ROL-01



- LEGEND**
- SITE BOUNDARY
 - - - PRECINCT BOUNDARY
 - - - STAGE BOUNDARY
 - CONTOURS (1m INTERVALS)
 - EASEMENT BOUNDARY
 - ▭ RIPARIAN CORRIDOR BOUNDARY
 - ▭ NEIGHBOURHOOD PARK
 - ▭ PEDESTRIAN LINKAGE (ROAD RESERVE)
 - ▭ MAJOR LINEAR PARK
 - ▭ STORMWATER DETENTION
 - ▭ PROPOSED ROAD DEDICATION
 - ENTRY STATEMENT
 - BIO RETENTION BASINS
 - BIN PAD LOCATION
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 - ▭ 14m x 32m COURTYARD LOT
 - ▭ 15m x 32m PREMIUM COURTYARD LOT
 - ▭ 17m x 32m TRADITIONAL LOT

KEY PLAN - Scale 1:15,000 @ A3



ROUND MOUNTAIN, NEW BEITH - PRECINCT B
RECONFIGURING OF A LOT - ROL 02



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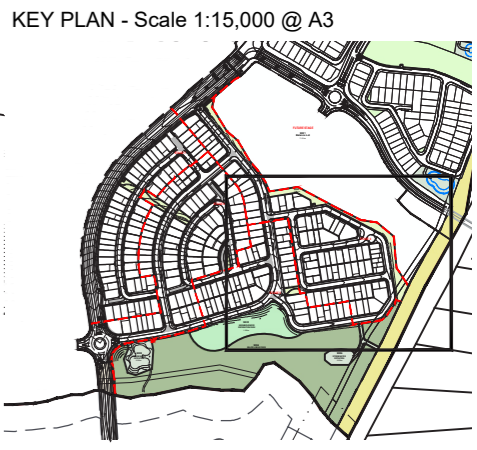
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 DWG NO: ROL-02



- LEGEND**
- SITE BOUNDARY
 - - - - - PRECINCT BOUNDARY
 - · - · - · STAGE BOUNDARY
 - CONTOURS (1m INTERVALS)
 - EASEMENT BOUNDARY
 - ▭ RIPARIAN CORRIDOR BOUNDARY
 - ▭ NEIGHBOURHOOD PARK
 - ▭ LINEAR PARK
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 - ▭ MAJOR LINEAR PARK
 - ▭ STORMWATER DETENTION
 - ▭ BALANCE LOT
 - ▭ FUTURE RAIL CORRIDOR
- RESIDENTIAL**
- TERRACE**
- ▭ 15.0m URBAN TWIN
 - ▭ 9m FRONT LOADED TERRACE
 - ▭ 10m+ FRONT LOADED TERRACE
- DEEP**
- ▭ 12.5m x 28m PREMIUM VILLA LOT
 - ▭ 17m x 28m TRADITIONAL LOT
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 - ▭ 17m x 32m TRADITIONAL LOT



ROUND MOUNTAIN, NEW BEITH - PRECINCT B
RECONFIGURING OF A LOT - ROL 03



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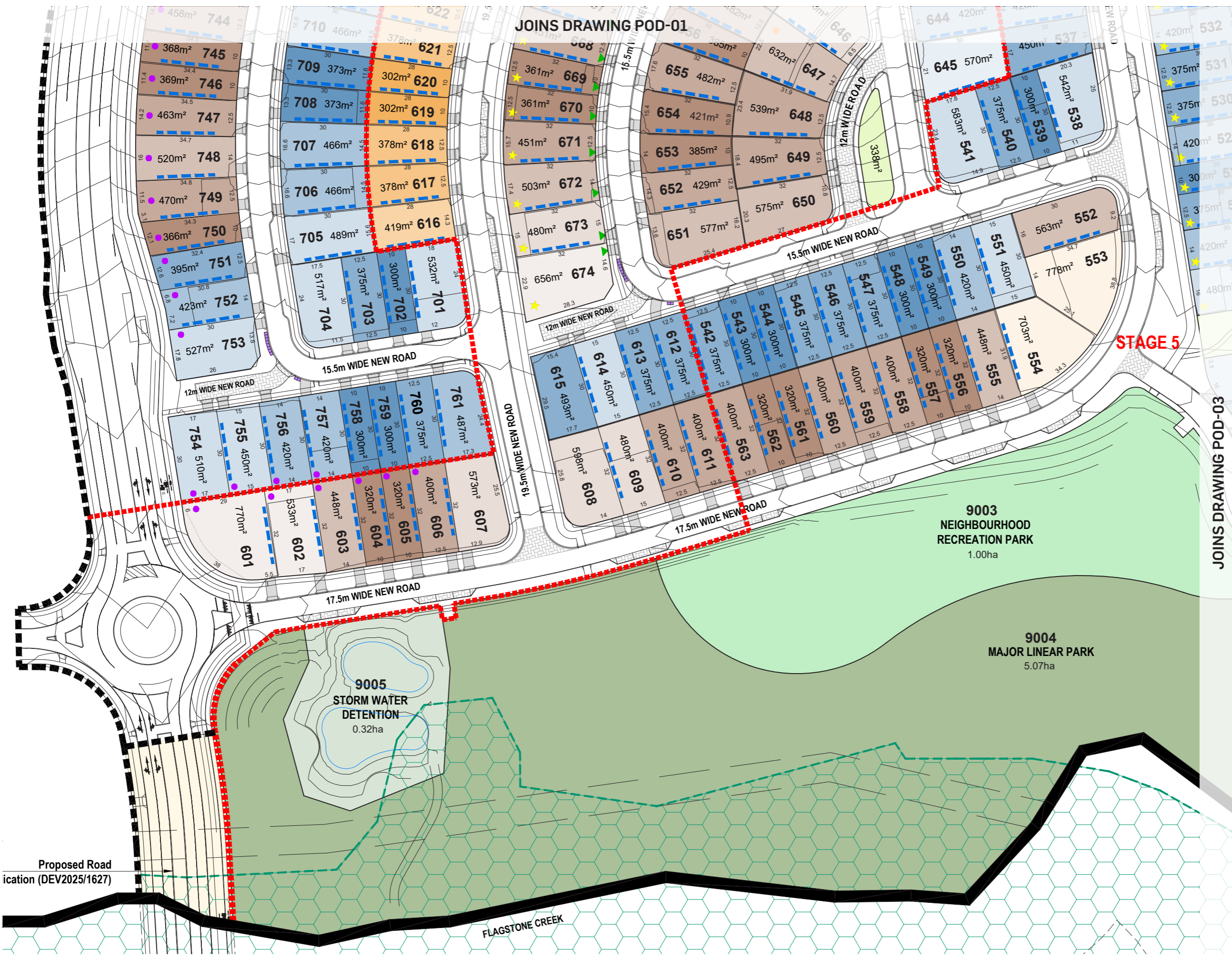


- LEGEND**
- SITE BOUNDARY
 - - - - - PRECINCT BOUNDARY
 - - - - - STAGE BOUNDARY
 - CONTOURS (1m INTERVALS)
 - EASEMENT BOUNDARY
 - ▭ RIPARIAN CORRIDOR BOUNDARY
 - ▭ NEIGHBOURHOOD PARK
 - ▭ LINEAR PARK
 - ▭ PEDESTRIAN LINKAGE (ROAD RESERVE)
 - OPTIONAL BUILT TO BOUNDARY WALL
 - MANDATORY BUILT TO BOUNDARY WALL
 - DRIVEWAY LOCATION
 - SPLIT LOT LOCATIONS
 - ACOUSTIC IMPACTED LOTS (FIRST FLOOR)
 - ★ GREEN STREET LOT LOCATIONS
 - ▶ PRIMARY FRONTAGE (GREEN STREET LOTS)
 - BIN PAD LOCATION

- RESIDENTIAL**
- TERRACE**
- 15.0m URBAN TWIN
- 28m DEEP**
- 10m x 28m VILLA LOT
 - 12.5m x 28m PREMIUM VILLA LOT
 - 14m x 28m COURTYARD LOT
 - 15m x 28m PREMIUM COURTYARD LOT
 - 17m x 28m TRADITIONAL LOT
- 30m DEEP**
- 10m x 30m VILLA LOT
 - 12.5m x 30m PREMIUM VILLA LOT
 - 14m x 30m COURTYARD LOT
 - 15m x 30m PREMIUM COURTYARD LOT
 - 17m x 30m TRADITIONAL LOT
- 32m DEEP**
- 10m x 32m VILLA LOT
 - 12.5m x 32m PREMIUM VILLA LOT
 - 14m x 32m COURTYARD LOT
 - 15m x 32m PREMIUM COURTYARD LOT
 - 17m x 32m TRADITIONAL LOT

KEY PLAN - Scale 1:15,000 @ A3



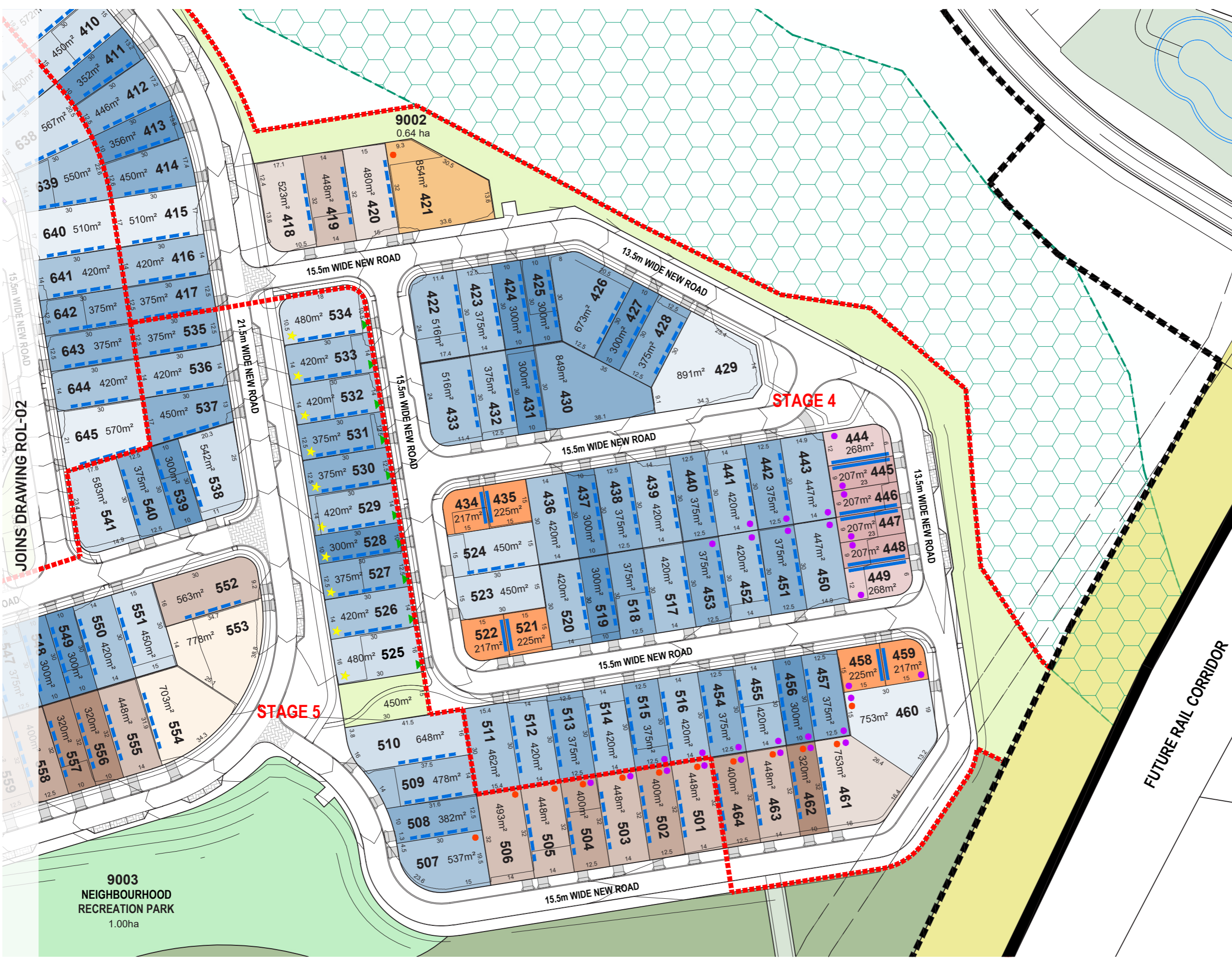


- LEGEND**
- SITE BOUNDARY
 - PRECINCT BOUNDARY
 - - - STAGE BOUNDARY
 - CONTOURS (1m INTERVALS)
 - EASEMENT BOUNDARY
 - ▭ RIPARIAN CORRIDOR BOUNDARY
 - ▭ NEIGHBOURHOOD PARK
 - ▭ PEDESTRIAN LINKAGE (ROAD RESERVE)
 - ▭ MAJOR LINEAR PARK
 - ▭ STORMWATER DETENTION
 - ▭ PROPOSED ROAD DEDICATION
 - ▭ OPTIONAL BUILT TO BOUNDARY WALL
 - ▭ MANDATORY BUILT TO BOUNDARY WALL
 - ▭ DRIVEWAY LOCATION
 - ENTRY STATEMENT
 - BIO RETENTION BASINS
 - SPLIT LOT LOCATIONS
 - ACOUSTIC IMPACTED LOTS (FIRST FLOOR)
 - ★ GREEN STREET LOT LOCATIONS
 - ▲ PRIMARY FRONTAGE (GREEN STREET LOTS)
 - BIN PAD LOCATION

- RESIDENTIAL**
- 28m DEEP
 - ▭ 10m x 28m VILLA LOT
 - ▭ 12.5m x 28m PREMIUM VILLA LOT
 - ▭ 14m x 28m COURTYARD LOT
 - 30m DEEP
 - ▭ 10m x 30m VILLA LOT
 - ▭ 12.5m x 30m PREMIUM VILLA LOT
 - ▭ 14m x 30m COURTYARD LOT
 - ▭ 15m x 30m PREMIUM COURTYARD LOT
 - ▭ 17m x 30m TRADITIONAL LOT
 - 32m DEEP
 - ▭ 10m x 32m VILLA LOT
 - ▭ 12.5m x 32m PREMIUM VILLA LOT
 - ▭ 14m x 32m COURTYARD LOT
 - ▭ 15m x 32m PREMIUM COURTYARD LOT
 - ▭ 17m x 32m TRADITIONAL LOT

KEY PLAN - Scale 1:15,000 @ A3





- LEGEND**
- SITE BOUNDARY
 - PRECINCT BOUNDARY
 - STAGE BOUNDARY
 - CONTOURS (1m INTERVALS)
 - EASEMENT BOUNDARY
 - RIPARIAN CORRIDOR BOUNDARY
 - NEIGHBOURHOOD PARK
 - LINEAR PARK
 - PEDESTRIAN LINKAGE (ROAD RESERVE)
 - MAJOR LINEAR PARK
 - STORMWATER DETENTION
 - BALANCE LOT
 - FUTURE RAIL CORRIDOR
 - OPTIONAL BUILT TO BOUNDARY WALL
 - MANDATORY BUILT TO BOUNDARY WALL
 - DRIVEWAY LOCATION
 - ACOUSTIC IMPACTED LOTS (FIRST FLOOR)
 - BUSHFIRE IMPACTED LOTS (BAL29)
 - GREEN STREET LOT LOCATIONS
 - PRIMARY FRONTAGE (GREEN STREET LOTS)
- RESIDENTIAL**
- 15.0m URBAN TWIN
 - 9m FRONT LOADED TERRACE
 - 10m+ FRONT LOADED TERRACE
 - 12.5m x 28m PREMIUM VILLA LOT
 - 17m x 28m TRADITIONAL LOT
 - 10m x 30m VILLA LOT
 - 12.5m x 30m PREMIUM VILLA LOT
 - 14m x 30m COURTYARD LOT
 - 15m x 30m PREMIUM COURTYARD LOT
 - 17m x 30m TRADITIONAL LOT
 - 10m x 32m VILLA LOT
 - 12.5m x 32m PREMIUM VILLA LOT
 - 14m x 32m COURTYARD LOT
 - 15m x 32m PREMIUM COURTYARD LOT
- 28m DEEP
- 30m DEEP
- 32m DEEP

KEY PLAN - Scale 1:15,000 @ A3



ROUND MOUNTAIN, NEW BEITH - PRECINCT B
PLAN OF DEVELOPMENT - POD 03



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1:1250 @ A3



DATE: 31.10.2025
 JOB NO: P0055018
 DWG NO: POD-03

DEVELOPMENT CONTROLS:

General

- All development is to be undertaken in accordance with the Development Approval and in compliance with the Queensland Development Code except where varied by this Plan of Development.
- Building locations are subject to future proposed easements and/or other underground services.
- All buildings are to comply with the approved Transport Noise Impact Assessment which specifies the level of acoustic treatments. Further site specific acoustic assessment may be undertaken to specify alternative acoustic treatments.
- All buildings are to comply with the approved Bushfire Mitigation Report which specifies the level of bushfire treatments. Further site-specific bushfire assessment may be undertaken to specify alternative bushfire treatments.
- Approved development are the uses of:
 - House, Home Based Business, Display Home and Sales Office; and
 - Advertising Devices associated with Display Home and Sales Office where not exceeding a cumulative total of 15m² per Display Home and 30m² per Sales Office with no individual sign to exceed a face area of 5m² per side, or a height of 5m. Advertising devices must be removed when the associated Display Home and Sales Office use has ceased.

Building Height

- The maximum height of buildings shall not exceed 9m.
- Buildings must be no more than two (2) storeys.

Setbacks

- Setbacks are as per the Plan of Development Table unless otherwise specified.
- Boundary setbacks are measured to the outermost projection of the building or structure. Overhangs, awnings and sunhoods may extend into this building setback.
- All dwellings must have eaves of a minimum width of 450mm where attached to habitable rooms. Gutters may not extend beyond the boundaries of the lot including those attached to built to boundary walls. Overhangs, awnings and sunhoods to habitable room windows may be provided as an alternative to eaves.
- A corner lot, for the purposes of determining setbacks, is a lot that adjoins the intersection of two streets. This excludes those lots that abut a shared access driveway, laneway, or a pedestrian link/ landscape buffer and therefore in these cases a secondary frontage setback does not apply.
- Setbacks to corner truncations are to be in accordance with 'Typical Corner Allotments' diagram (located on DWG NO: CTR-02). The setback applies to any building or structure greater than 2m in height and overrides the QDC provision for a 9m by 9m corner setback.
- Entry porticos may be located closer to the property boundary than stated in Plan of Development Table provided that the portico:
 - Is located no less than 1.5m from the front property boundary;
 - Does not exceed a maximum height of 4.5m;
 - Does not exceed a width of 3m; and
 - Portico remains open and not enclosed.

Built to Boundary Walls

- Nominated Built to Boundary Walls are indicated on the Plan of Development
- Built to Boundary walls are optional except for Terrace, Urban Twin Lots and lots identified on the Plan of Development (Mandatory BTB). Where optional built to boundary walls are not adopted side setbacks shall be in accordance with the Plan of Development Table.
- For all residential lots except Terrace and Urban Twin lots, built to boundary walls can have a maximum length of 15m or 50% of the property boundary, whichever is the greater, and a maximum height of 3.5 metres.
- For Terrace and Urban Twin lots, built to boundary walls have a maximum length of 85% of the length of the property boundary and may be higher than 3.5m where in accordance with approved house plans and where appropriate building staging and construction techniques are demonstrated.
- Built to Boundary walls are to contain no windows or openings to the side boundary.

On-site car parking and driveways

- A minimum of two on-site car parking spaces must be provided for each dwelling, one of which must be in a garage.
- Double car garages or greater must be provided for a dwelling on a Lot of 12.5m or greater in width (except for Terrace and Urban Twin allotments).

- Double car garages for any single storey dwelling on a Lot between 10.0m and 12.49m in width must adhere to the following design criteria:
 - The front facing building wall, which includes the garage door, must not exceed an external width of 5.8m.
 - The garage door:
 - Width does not exceed 4.8m
 - Must have a minimum 450mm eave above it
 - Must be setback a minimum of 240mm behind the pillar of the garage door and
 - Must have a sectional, tilt or integrated door.
 - The front façade of the dwelling must be forward of the alignment of the garage wall, and must include the following:
 - A front entrance door with glass inserts or a solid door with a sidelight or a habitable room with window.
 - A front verandah, portico or porch located over the front entrance, which extends a minimum of 1500mm forward of the entrance door
 - The verandah, portico or porch is to include front piers, posts or columns with distinct materials and/ or colours.
- Driveways are to have a minimum setback of 0.5m from the side boundary
- Driveway finish must not be plain concrete and must have a minimum standard finish of exposed aggregate.
- Driveways are to accord with Logan City Council's (LCC) standards. Prior to construction, approval from LCC for Vehicular Access to Residential Premises is required.
- Maximum of one driveway per dwelling.
- The maximum width of a driveway at the boundary shall be:
 - 3.0m for single garages; and
 - 4.8m for double garages
- Minimum distance of a driveway from an intersection of one street with another street is 6.0 metres from the tangent point unless otherwise approved.
- All garages should be setback behind the main facade of the dwelling.
- Garages are to be located along the Built to Boundary wall. Where no Built to Boundary wall is proposed, garages are to be located where the associated driveway will not compromise service placements, street trees or on-street parking.

Private Open Space

- Private open space must measure a minimum of 12m² with a minimum dimension in any direction of 2.4 metres.
- Private open space must be directly accessible from a living space.
- Part of the open space may be roofed.

Fencing

- Fencing erected by the developer must not be altered, modified or removed without prior written approval from the developer.
- Fencing on all park, open space, pedestrian links or secondary street frontages is to have a maximum height of 1.2m where solid or have a maximum height of 1.8m where containing consistent openings across the full area of the fence that make the fence more than 50% transparent.
- Fencing to the side boundary is to return a minimum distance of 1.0m behind the front façade.
- Fencing to the front boundary and to side boundaries forward of the building line is to have a maximum height of 1.2m and contain openings that make the fence at least 75% transparent.

Retaining Walls Not Constructed by the Developer

- Any retaining walls adjoining a public street or public space must be constructed as follows:
 - Limited to a maximum height of 1.0m from finished ground level
 - Wholly located within the subject lot and be maintained by the lot owner
- Retaining walls visible from public space must be built from materials consisting of stone or rendered/ bagged masonry block. Must not be constructed from concrete "I-beam", timber sleepers, unfinished masonry block or commons brick.
- Retaining walls consisting of timber sleepers are only permitted to side and rear boundaries which are behind the front wall of the building and do not adjoin a public space.
- Walls over 1.0m require RPEQ certification.

Building Articulation

- All buildings with a width of more than 8 metres that are visible from a street or park are to include articulation

to reduce the mass of the building by one or more of the following:

- Windows recessed into the façade or bay windows;
- Balconies, porches or verandahs;
- Articulation of roof lines
- Window hoods; and/or
- Use of multiple cladding materials

- Where adjoining an area of open space, housing design must facilitate passive surveillance of the public open space, which can be achieved through the incorporation of at least one (1) habitable room orientated towards the open space.
- All building materials must be suitably coloured, stained or painted, including retaining, fences, walls and roofs. Untreated materials, such as zinc coated steel, bare metal, concrete block or masonry panels are not permitted.
- Homes must include a clearly identifiable and addressed front door and undercover point of entry.
- At least two openings to all habitable rooms to facilitate cross flow ventilation are required.

Slope and Building Footings

- If buildings on sloping sites are built to the boundary on the low side of the lot, the footing must be projected deep enough to be below the adjoining property building pad level.
- If the nominated pad level is not provided, the pad level is to be assumed as the average of the four corners of the adjacent block, using as constructed levels.
- Building footings are to be designed in accordance with the appropriate Australian Standard. Building footings are to be designed to ensure that there are no adverse impacts (functional, financial or construction limitations) on adjoining allotments, particularly in relation to retaining walls.

Privacy and Screening

- Air-conditioners, gas bottles, hot water systems, clothes lines and other household services must be screened and/or located to minimise visual impact to public streets or parks.
- Screened drying and rubbish bins area must be behind the main face of the dwelling.

Multigenerational Home / Secondary Dwelling

- A secondary dwelling is only permitted where:
 - The lot is 400m² or more
 - The lot frontage is greater than 12.5m
- There is to be no more than one secondary dwelling on a lot.
- Any secondary dwelling is unable to be separately titled to the primary dwelling
- No separate infrastructure servicing to the secondary dwelling is permitted (water, electricity etc).
- Secondary dwelling floor area is a minimum of 45m² and a maximum of 75m² GFA
- Design and siting of buildings and structures to be in accordance with this Plan of Development.
- Materials including colours and roof form must be consistent with the primary dwelling.
- Outdoor living space is a minimum of 9m² with a minimum dimension of 2.4m and directly accessible from main living area.
- In addition to the carparking required for the primary dwelling a minimum one carpark or garage space is to be provided for the secondary dwelling.
- Where not on a corner lot the secondary dwelling entry must not be directly visible from the street to give the impression of one single dwelling.
- Where on a corner lot frontage the secondary dwelling may have dedicated pedestrian entry and front door addressing the secondary street.
- Secondary dwellings are not permitted to have their own letterbox.

Green Street Lots

- Green street lots are permitted to have pedestrian access from both roads/streets it abuts - with the lower order road (15.5m access road) being the primary frontage for driveway and front door access as well as letterbox and bin pad location.
- Green street lots must have 50% transparency fencing along the green street edge to provide additional visibility for passive surveillance
- Green street lots must be level with both of the roads they interface with to assist in passive surveillance - this includes any required to be a split level lot due to steep grading, in this case both the front and rear pads of the lot must be level with their respective roads.

Electric Vehicle Readiness

- Each dwelling is to include a separate dedicated electric circuit (conduit and wiring) into the garage or carport to enable the installation of Electric Vehicle Supply Equipment. Where not used for Electric Vehicle Supply Equipment, the circuit is to be fitted with a minimum 20amp GPO outlet.

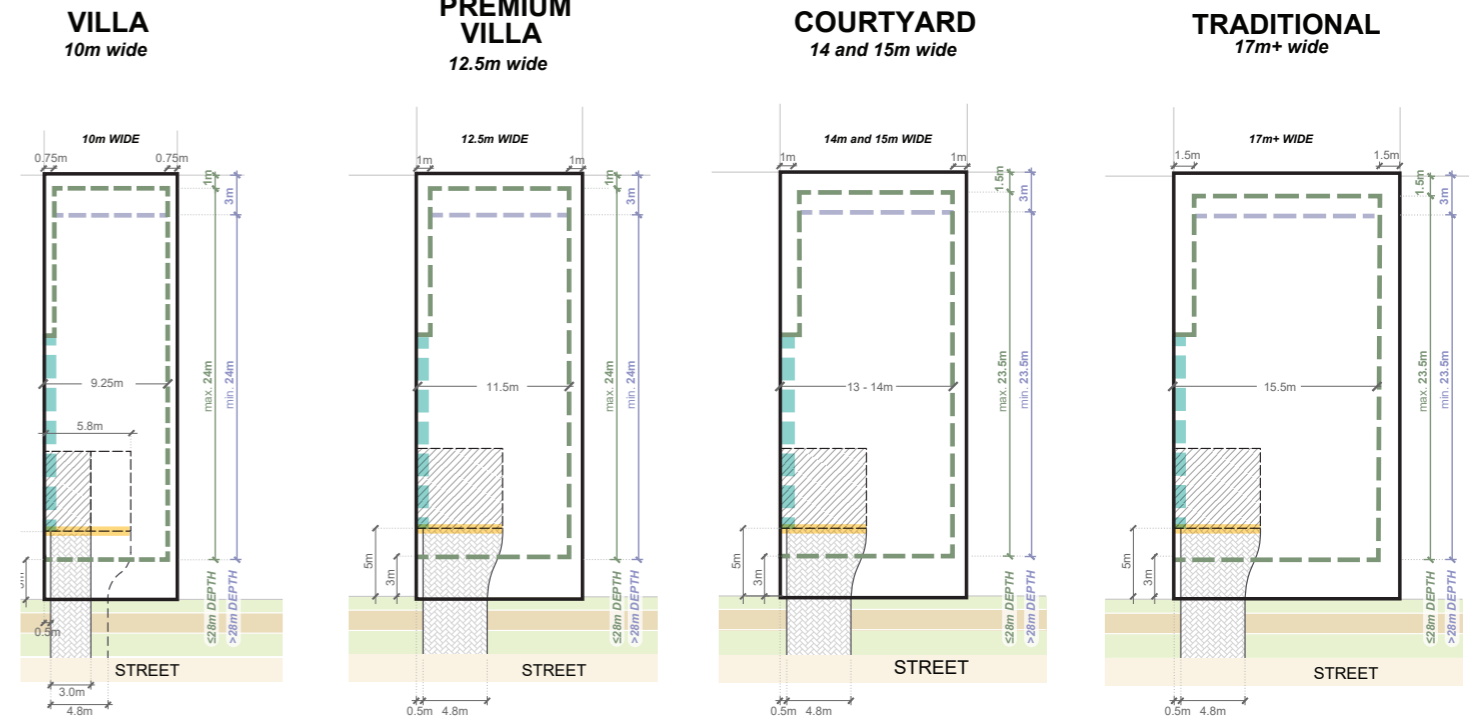
PLAN OF DEVELOPMENT TABLE:

Lot Typology	URBAN TWIN		TERRACE ALLOTMENTS		VILLA ALLOTMENTS		PREMIUM VILLA ALLOTMENTS		COURTYARD/ PREMIUM COURTYARD ALLOTMENTS		TRADITIONAL ALLOTMENTS	
	15m		9m Wide		10m Wide		12.5m Wide		14m & 15m Wide		17m+ Wide	
Elevation	Ground Floor	First Floor	Ground Floor	First Floor	Ground Floor	First Floor	Ground Floor	First Floor	Ground Floor	First Floor	Ground Floor	First Floor
Front Setback (metres)												
Front Wall	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Garage	5.0	n/a	5.0	n/a	5.0	n/a	5.0	n/a	5.0	n/a	5.0	n/a
Corner Allotments (metres)												
Secondary Road Frontage	1.0	1.0	1.0	1.5	1.5	2.0	1.5	2.0	1.5	2.0	1.5	2.0
Rear Setback (metres)												
Lot Depth ≤ 28m	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.5	2.0	1.5	2.0
Lot Depth > 28m	n/a	n/a	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0	3.0
Park / Open Space Setback (metres)												
Side	1.0	1.0	1.0	1.5	1.5	2.0	1.5	2.0	1.5	2.0	1.5	2.0
Side - General Lots (metres)												
Built to Boundary	0.0-0.2	0.0-0.2	0.0-0.2	0.0-0.2	0.0-0.2	1.0	0.0-0.2	1.0	0.0-0.2	1.0	0.0-0.2	1.0
Non Built to Boundary	1.0	1.0	1.0	1.0	0.75	0.75	1.0	1.0	1.0	1.0	1.5	1.5
Other Requirements												
Maximum Site Cover	75%		75%		65%		65%		60%		60%	

* Lot coding is generally based on typical frontage width, except irregular and corner allotments where lot typologies are nominated based on the largest regular allotment BLE that can be accommodated.

#Refer to Note 13 for Porticos setbacks.

TYPICAL HOUSE & MULTIPLE RESIDENTIAL ALLOTMENTS:



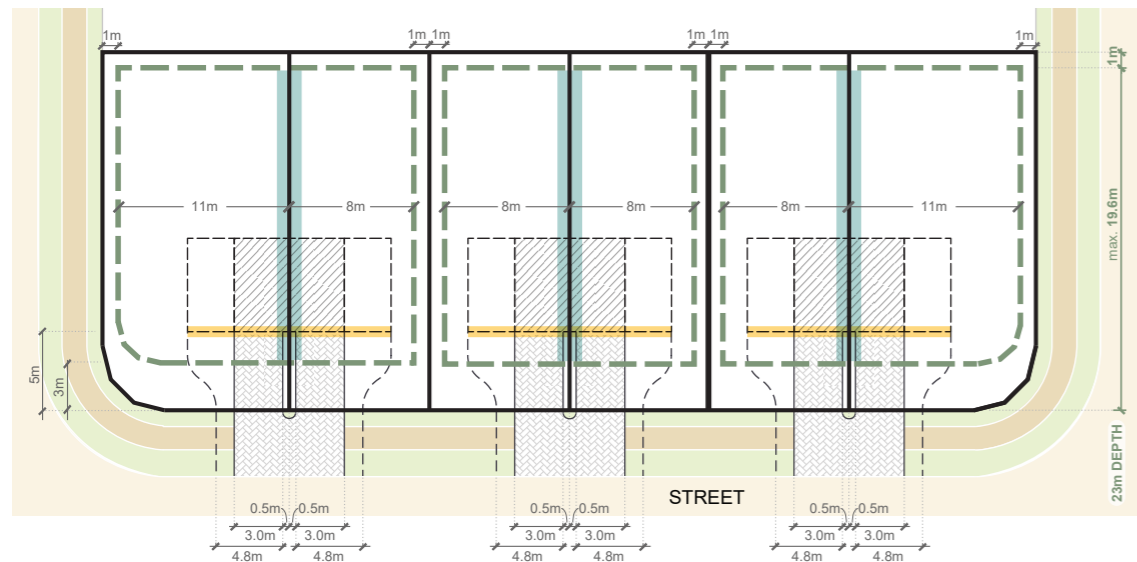
TYPICAL TERRACE ALLOTMENTS:

TYPICAL URBAN TWIN ALLOTMENTS:

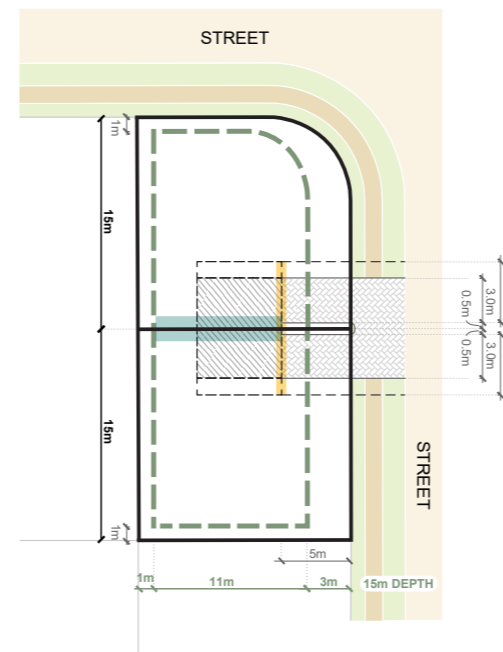
TYPICAL CORNER ALLOTMENTS:

TYPICAL GREEN STREET ALLOTMENTS:

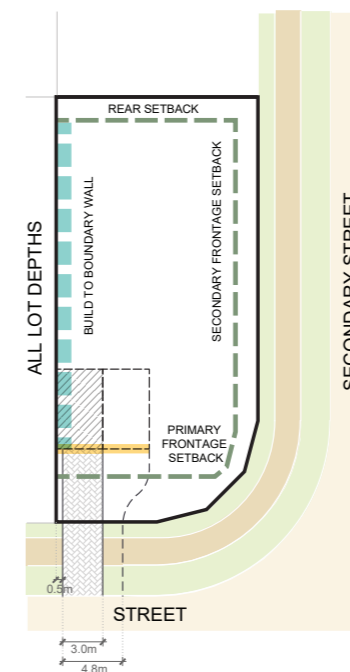
TERRACE
9m wide, 23 deep



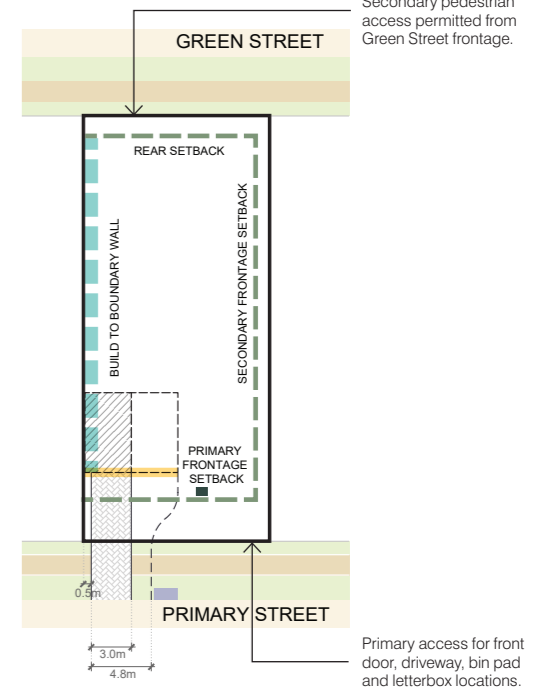
URBAN TWIN
15m wide, 15 deep



CORNER



GREEN STREET LOT



LEGEND:

- Lot boundary
- - - Maximum building envelope (≤28m deep lot)
- - - Maximum building envelope (>28m deep lot)
- 1.0m / - Minimum building setback dimensions
- Mandatory built to boundary wall
- - - Optional built to boundary
- Garage setback from front boundary
- Indicative garage location
- Single garage for single storey dwelling, double garage for double storey dwelling
- Indicative driveway location
- Approximate bin pad location
- Approximate letterbox location



- LEGEND**
- SITE BOUNDARY
 - PRECINCT BOUNDARY
 - STAGE BOUNDARY
 - CONTOURS (1m INTERVALS)
 - RIPARIAN CORRIDOR BOUNDARY
 - NEIGHBOURHOOD PARK
 - LINEAR PARK
 - PEDESTRIAN LINKAGE (ROAD RESERVE)
 - MAJOR LINEAR PARK
 - STORMWATER DETENTION
 - BALANCE LOT
 - FUTURE RAIL CORRIDOR

(subject to sepa
- DEV20



- LEGEND**
- SITE BOUNDARY
 - PRECINCT BOUNDARY
 - CONTOURS (1m INTERVALS)
 - RIPARIAN CORRIDOR BOUNDARY
 - NEIGHBOURHOOD PARK
 - LINEAR PARK
 - PEDESTRIAN LINKAGE (ROAD RESERVE)
 - MAJOR LINEAR PARK
 - STORMWATER DETENTION
 - BALANCE LOT
 - RAIL RESUMPTION
 - BIN PAD LOCATION
 - DRIVEWAY LOCATION
 - INDICATIVE CAR PARK LOCATION

PARKING BREAKDOWN

TOTAL VISITOR PARKING SPACES	197
TOTAL DWELLINGS	262
REQUIRED ON STREET PARKING SPACES (75% of total dwellings)	197



**ROUND MOUNTAIN, NEW BEITH - PRECINCT B
PARKING PLAN**



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1:2000 @ A3



DATE: 31.10.2025
JOB NO: P0055018
DWG NO: PRK-01



- LEGEND**
- SITE BOUNDARY
 - PRECINCT BOUNDARY
 - CONTOURS (1m INTERVALS)
 - RIPARIAN CORRIDOR BOUNDARY
 - NEIGHBOURHOOD PARK
 - LINEAR PARK
 - PEDESTRIAN LINKAGE (ROAD RESERVE)
 - MAJOR LINEAR PARK
 - STORMWATER DETENTION
 - BALANCE LOT
 - RAIL RESUMPTION
 - PROPOSED ROAD DEDICATION
 - B BUS STOP LOCATION
 - 400m WALKING CATCHMENT
 - 3m SEPARATED CYCLE
 - 3m FOOTPATH
 - 2.5m FOOTPATH
 - 1.5m FOOTPATH



Making Sustainability Happen