

Notes:

1. All development is to be undertaken in accordance with the Development Approval.
2. The maximum height of buildings shall be in the order of 6 metres.
3. Setbacks are as per the Plan of Development Table unless otherwise specified.
4. Built to boundary walls are optional. The location of the built to boundary walls shall be as per the Plan of Development Table in accordance with the Plan of Development Table.
5. Built to boundary walls are to have a maximum length of 15 metres and a maximum height of 3.5 metres.
6. Boundary setbacks are measured to the wall of the building or edge of balcony.
7. Eaves (except on a wall that is built to the boundary) should not encroach within 450mm of the side or rear boundaries. Gutters may not extend beyond the boundaries of the lot.
8. First floor setbacks on single and multi family allotments must not exceed the minimum ground floor setbacks.
9. Must be setback a minimum of 4.5m from the front property boundary.
10. Parking for Multiple Residential developments
 - 0.5 visitor space per dwelling unit
 - 0.5 visitor space per dwelling unit
 - do not dominate the streetscape

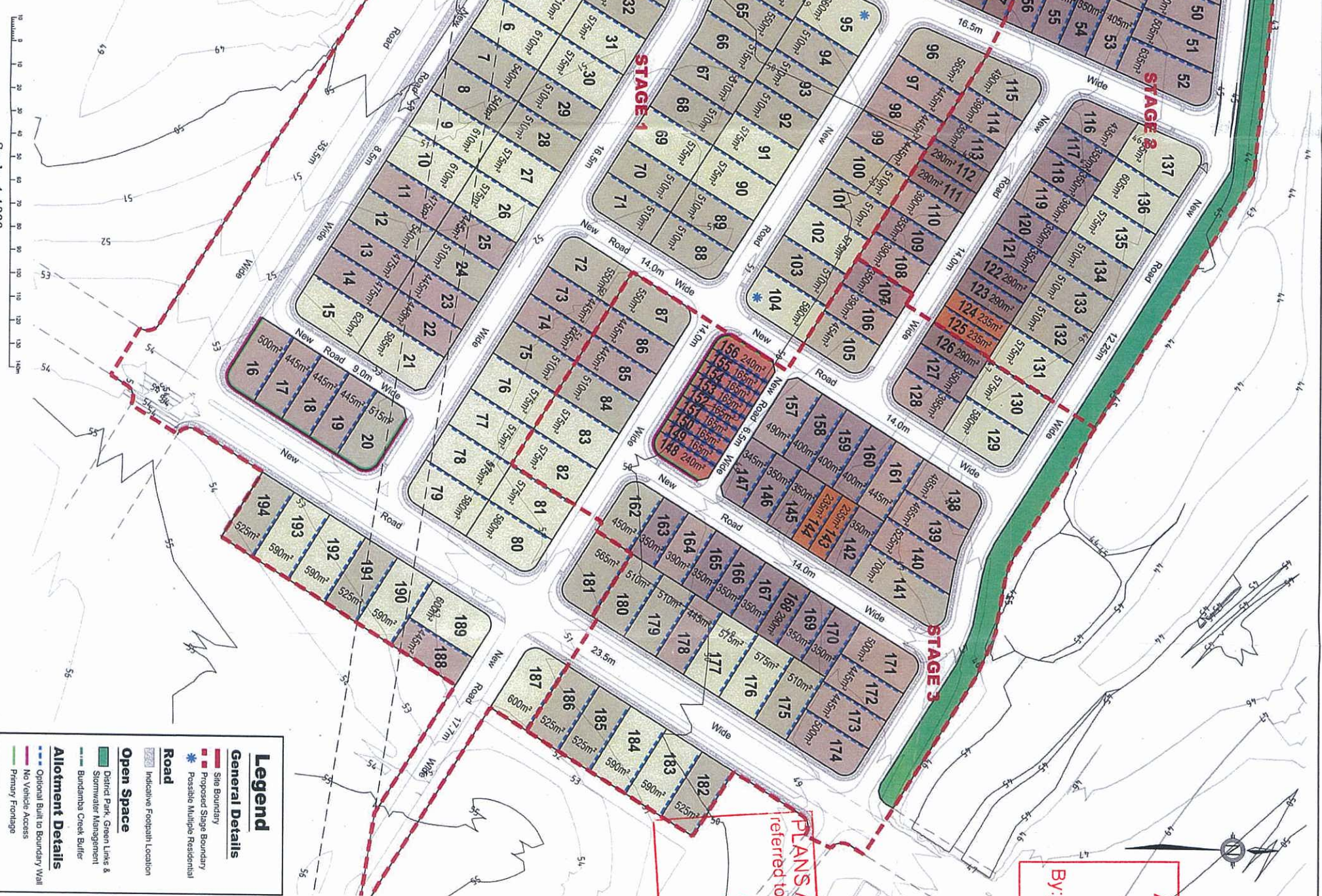
11. Site cover for each lot is not to exceed 70% of the lot.
12. Dwellings with their main living areas located at ground level should have a minimum area of private open space consisting of at least 120m² with a minimum width of 2.4 metres, preferably accessible from a living room.
13. Dwellings with their main living areas above ground floor level should have a minimum area of private open space consisting of a balcony or roof area open to the sky, with a minimum area of 5m² and a minimum dimension of 1.2 metres, directly accessible from a living room.
14. Multiple Residential developments are to be
 - a minimum dimension of 4.0m
 - a maximum gradient not exceeding 1 in 10
 - separated from any other private areas by fence or landscaping
 - Multiple Residential sites are not permitted to be developed with only one single dwelling. Maximum number of dwellings per Multiple Residential Site:
 - 450m² - 50m² maximum 3 dwellings
 - 650m² - 749m² maximum 4 dwellings
 - 750m² - 849m² maximum 5 dwellings
 - 15. Where Multiple Residential developments encompass more than one lot, the boundaries of the lots are to be designed to address each street frontage.
16. Fencing
 - 17. Fencing on all street frontages, to be 50% transparent or not to exceed 1.2 metres in height.

Definitions:

LaneWAY Allotment: Allotment serviced by a laneWAY.

Not included in the provisions of this Plan of Development.

Plan of Development Table		Terrace Allotments 6m & Cottage Allotments 8.5m		Villa Allotments 10.5m		Premium Villa Allotments 12.5m		Courtyard Allotments 14m		Traditional Allotments 16m		Premium Traditional Allotments 18m+	
	Ground Floor	First Floor	Ground Floor	First Floor	Ground Floor	First Floor	Ground Floor	First Floor	Ground Floor	First Floor	Ground Floor	First Floor	Ground Floor
Front/Primary Frontage	2.4	2.4	2.4	2.4	2.4	2.4	2.4	2.4	2.4	2.4	2.4	2.4	2.4
Garage	4.5	n/a	4.5	n/a	4.5	n/a	4.5	n/a	4.5	n/a	4.5	n/a	4.5
Rear	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0	1.0
Side - General Lots													
Built to Boundary	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0	0.0
Non Built to Boundary	0.9	0.9	0.9	0.9	0.9	0.9	0.9	0.9	0.9	0.9	0.9	0.9	0.9
Corner Lots - Secondary Frontage	1.5	1.5	1.5	1.5	1.5	1.5	1.5	1.5	1.5	1.5	1.5	1.5	1.5
LaneWAY Lots													
Rear of Lot (from laneWAY boundary)	1.0	1.0	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
Garage (from laneWAY boundary)	0.5	0.5	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a	n/a
On site parking requirements (minimum)	1 space to be covered and enclosed												
Garage location	Single or tandem garage acceptable		1 space to be covered and enclosed		1 space to be covered and enclosed		1 space to be covered and enclosed		1 space to be covered and enclosed		1 space to be covered and enclosed		
	Double garages are only permitted on laneWAY or two storey allotments		Double garages are only permitted on laneWAY or two storey allotments		Double garages are only permitted on laneWAY or two storey allotments		Double garages are only permitted on laneWAY or two storey allotments		Double garages are only permitted on laneWAY or two storey allotments		Double garages are only permitted on laneWAY or two storey allotments		
Garage location	Garages are to be located along the built to boundary wall												
Site Cover (maximum)	70%		70%		70%		70%		70%		70%		



AMENDED IN RED

16 MAY 2013

By: BRIANNA PUFFE (name)

MEDQ

PLANS AND DOCUMENTS referred to in

24 MAY 2013

MEDQ

CLIENT

AMEX

PROJECT

SECONDARY URBAN CENTRE EAST

PLAN OF DEVELOPMENT STAGES 1-3

Level Datum

Original

Date

13 MARCH 2013

Drawn By

AS

DWG Name

6837-121

Local Authority

IPSWICH CITY COUNCIL

Locality

RIPLEY

Job Reference

6837

Scale

1:1000

Plan Ref

6837-127

Rev

F

RPS

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OVERALL LAND USE BUDGET

Land Use Budget	Area	Percentage
Area of Stages 1-3	17,579 ha	100.0%
Saleable Area	8,882 ha	50.5%
Area of Residential Lots	0,590 ha	3.4%
Area of Sales & Convenience Centre	9,472 ha	53.9%
Total Area of Allotments	9,472 ha	53.9%
Roads	2,661 ha	15.3%
Trunk Collector Road	3,501 ha	19.9%
Local Roads	6,182 ha	35.2%
Total Area of New Road	1,925 ha	11.0%
Open Space	1,925 ha	11.0%
Area of District Park, Green Links & Stormwater Management	1,925 ha	11.0%
Total Area of Open Space	1,925 ha	11.0%
Road Type	Length	
Total Length of New Road	34.32m	
6.5m wide lane - 5.5m pavement	120m	
8.5m wide lane - 5.5m pavement	195m	
12.5m wide lane - 5.5m pavement	596m	
14m wide road - 6.5m pavement	786m	
16.5m wide road - 8m pavement	659m	
17.7m wide road - 8m pavement	118m	
23.5m wide road - 10m pavement	380m	
33.7m wide road	248m	
35.5m wide road	264m	

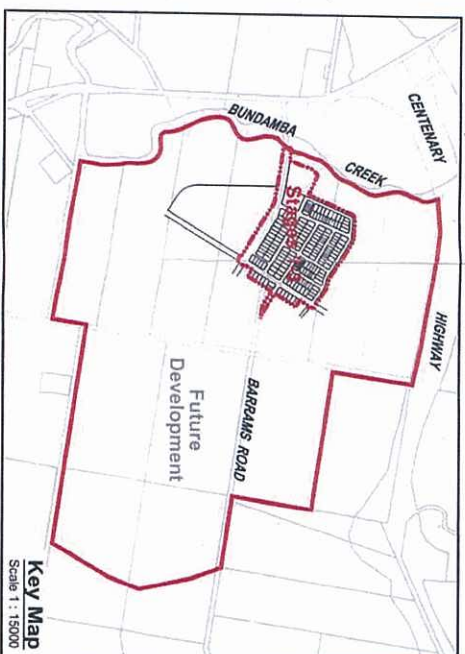
DEVELOPMENT STATISTICS

Allotment Type	Typical Lot Dimensions	Stage 1		Stage 2		Stage 3		Overall	
		No. of Lots	Percentage	No. of Lots	Percentage	No. of Lots	Percentage	No. of Lots	Percentage
Standard Allotment 28m Product	6m x 28m	0	0.0%	0	0.0%	9	14.5%	9	4.7%
Terrace Allotment	8.5m x 28m	0	0.0%	6	12.8%	2	3.2%	8	4.1%
Villa Allotment	10.5m x 28m	3	3.6%	4	8.5%	2	3.2%	9	4.7%
Premium Villa Allotment	12.5m x 28m	3	3.6%	11	23.4%	12	19.4%	26	13.5%
Courtyard Allotment	14m x 28m	1	1.2%	7	14.9%	3	4.8%	11	5.7%
Traditional	16m x 28m	0	0.0%	1	2.1%	2	3.2%	3	1.6%
Sub Total		7	8.3%	29	61.7%	30	48.4%	66	29.5%
Standard Allotment 32m Product	12.5m x 32m	0	0.0%	0	0.0%	3	4.8%	3	1.6%
Premium Villa Allotment	14m x 32m	16	19.0%	4	8.5%	12	19.4%	32	16.6%
Traditional	16m x 32m	33	39.3%	6	12.8%	9	14.5%	48	24.9%
Premium Traditional Allotment	18m x 32m	28	33.3%	8	17.0%	8	12.9%	44	22.8%
Sub Total		77	91.7%	18	38.3%	32	51.6%	127	65.8%
Total Standard Allotments		84	100.0%	47	100.0%	62	100.0%	193	100.0%
Density Calculation									
Net Residential Density - 195 dwellings (including residential lots, laneway lots and multiple residential dwellings) / 12.384ha (area of local roads and residential lots)									15.7

AMENDED IN RED

16 MAY 2013

By: BRIANNA PIPPE (name)
MEDQ



24 MAY 2013

PLANS AND DOCUMENTS referred to in the PDA APPROVAL

CLIENT: MEDQ
AMEX

PROJECT: SECONDARY URBAN CENTRE EAST

PLAN OF SUBDIVISION STAGES 1-3 ALLOTMENT LAYOUT

Date: 13 MARCH 2013
Drawn By: JLS
DWG Name: 6837-121
Local Authority: IPSWICH CITY COUNCIL

Locality: RIPLEY
200 Reference: 6837

Scale: 1:1000
Sheet: A1
Plan Ref: 6837-121
Rev: 1



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Legend	
General Details	
Site Boundary	
Possible Stage Boundary	
Possible Multiple Residential	
Road	
Indicative Footpath Location	
Open Space	
District Park, Green Links & Stormwater Management	
Bundamba Creek Buffer	
Facilities	
Stages & Governance Centre	

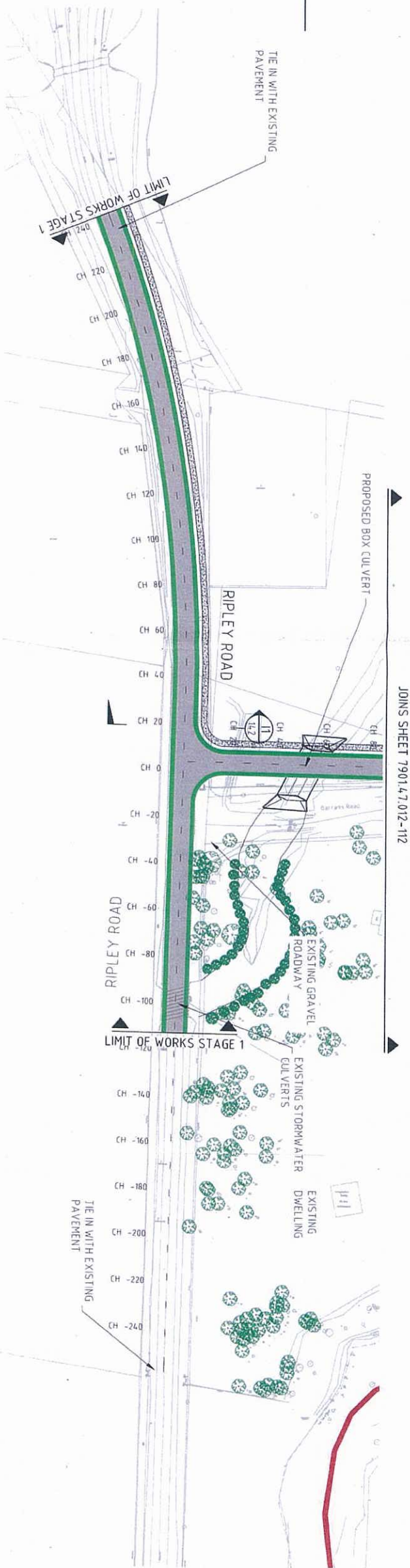
Potential sales and convenience centre will be subject to future application

LEGEND

- STAGE 1 BOUNDARY
- EXISTING LOT BOUNDARIES
- STAGE 1 LOT BOUNDARIES
- PROPOSED CYCLE LANE



RIPLEY ROAD INTERIM UPGRADE PLAN
SCALE 1:1000 @ A1



JOINS SHEET 7901.47.012-112

PLANS AND DOCUMENTS
referred to in the PDA APPROVAL
24 MAY 2013
MEDQ

Vertical Curve Length (m)	Vertical Curve Radius (m)	Vertical Geometry Grade (%)	Vertical Geometry Length (m)	Horizontal Curve Length (m)	Horizontal Curve Radius (m)	DATUM RL 37.000
2.134%	2.74%	14.047m / 254m	72.591m	131.207m	R 19.10m	
2.74%	4.156%	159.311m	127.951m	127.951m	R 3500m	
4.156%	-0.5%	101.597m				

DESIGN LEVELS	ROAD CONTROL LINE	EXISTING SURFACE LEVELS	CUT / FILL DEPTH TO EXISTING SURFACE	CONTROL LINE CHAINAGE
50.396	50.396	50.395	0.001	-243.836
50.502	50.501	50.501	0	-240.000
51.050	51.049	51.049	0	-220.000
51.493	51.563	51.563	-0.07	-200.000
51.728	51.860	51.860	-0.133	-180.000
51.768	51.990	51.990	-0.222	-167.619
51.753	51.966	51.966	-0.213	-160.000
51.568	51.611	51.611	-0.043	-140.000
51.174	51.024	51.024	0.15	-120.000
50.571	50.345	50.345	0.226	-100.000
49.776	49.598	49.598	0.178	-80.000
48.945	48.779	48.779	0.165	-60.000
48.164	47.988	47.988	0.176	-40.000
47.945	47.771	47.771	0.173	-33.787
47.497	47.318	47.318	0.179	-20.000
46.945	46.792	46.792	0.152	0.000
46.506	46.374	46.374	0.133	20.000
46.183	46.050	46.050	0.133	40.000
45.973	45.849	45.849	0.124	60.000
45.861	45.753	45.753	0.108	80.000
45.761	45.736	45.736	0.025	100.000
45.716	45.715	45.715	0.001	120.000
45.790	45.792	45.792	-0.003	140.000
45.865	45.865	45.865	0	160.000
45.945	45.945	45.945	0	180.000

INITIAL UPGRADE
SCALE: HORIZONTAL - 1:1000
VERTICAL - 1:100

RIPLEY ROAD - INTERIM UPGRADE WORKS
SCALE: HORIZONTAL - 1:1000
VERTICAL - 1:100



Rev	Date	Description	Drawn	Appr
A	23/01/2013	BARRAMS ROAD GRADE TO SUIT NEW FLOOD HIGH	SJT	P.O.



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Drawn	Check	Date	Drawn	Check	Date
B.V.	D.M.	24/02/2011	B.V.	D.M.	24/02/2011
C.P.	C.P.	24/02/2011	C.P.	C.P.	24/02/2011
DT	DT	24/02/2011	DT	DT	24/02/2011
DT	DT	24/02/2011	DT	DT	24/02/2011

AMEX PTY. LTD.
RIPLEY VALLEY - SUCE
STAGE 1 - TRUNK ROAD WORKS
LAYOUT PLAN & ROAD LONGITUDINAL SECTION
SHEET 1 OF 3

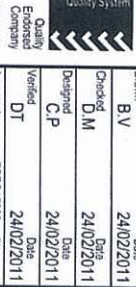
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Drawing Number: 7901.47.012-111
Scale: A1
Revision: A

Rev	Date	Description	Drawn	Check
A	15/02/2013	FOOTPATH ADDED SOUTH OF BARRAMS ROAD	SJT	PJO
B	23/01/2013	BARRAMS ROAD GRADED TO SUIT NEW ROAD MODEL	SJT	PJO



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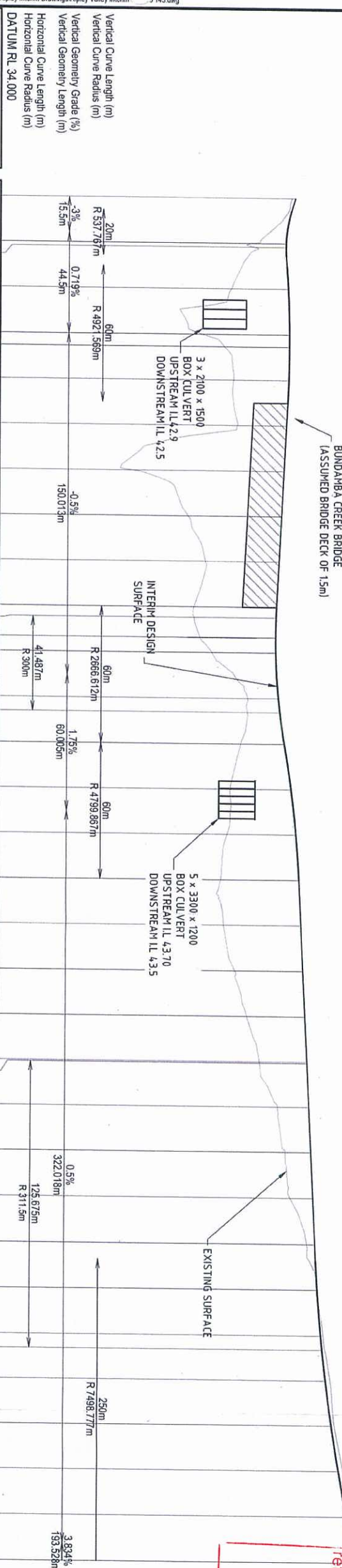


Drawn: BV Date: 24/02/2011
Designed: DM Date: 24/02/2011
Checked: CP Date: 24/02/2011
Verified: DT Date: 24/02/2011
Approved: RPD Date: 24/02/2011

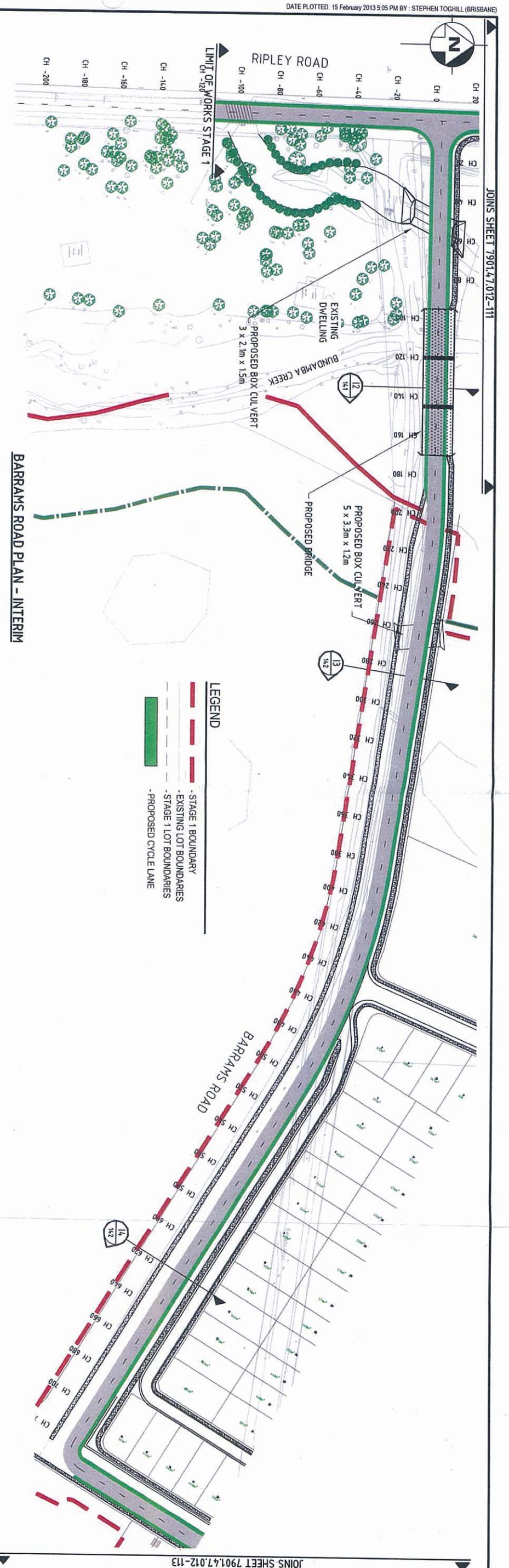
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Sheet: AS SHOWN
Revision: A1
Status: NOT FOR CONSTRUCTION

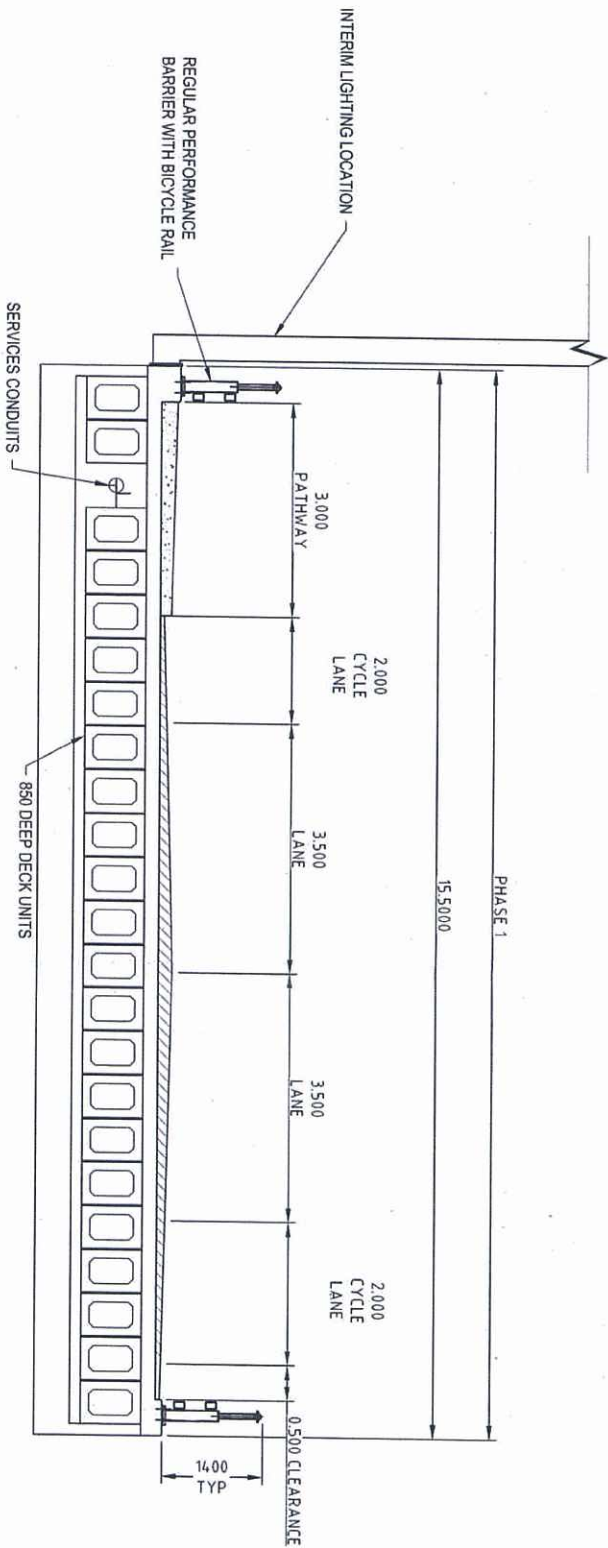
DESIGN LEVELS	ROAD CONTROL LINE	EXISTING SURFACE LEVELS	CUT / FILL DEPTH TO EXISTING SURFACE	CONTROL LINE CHAINAGE
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20.000	46.540	44.981	1.56	20.000
21.633	46.538	45.165	1.373	21.633
40.000	46.646	44.145	2.501	40.000
60.000	46.709	42.856	3.852	60.000
65.391	46.712	43.854	2.857	65.391
80.000	46.690	44.261	2.429	80.000
100.000	46.600	44.431	2.169	100.000
120.000	46.500	39.843	6.657	120.000
140.000	46.400	42.300	4.1	140.000
160.000	46.300	43.062	3.238	160.000
180.000	46.200	42.570	3.63	180.000
184.665	46.181	42.575	3.606	184.665
193.347	46.167	42.849	3.317	193.347
200.000	46.175	43.371	2.804	200.000
220.000	46.300	44.957	1.343	220.000
226.152	46.368	44.943	1.425	226.152
240.000	46.575	44.664	1.911	240.000
260.000	46.883	44.206	2.677	260.000
280.000	47.108	44.081	3.027	280.000
300.000	47.250	43.812	3.438	300.000
320.000	47.350	43.943	3.407	320.000
340.000	47.450	44.458	2.992	340.000
360.000	47.550	44.993	2.557	360.000
380.000	47.650	45.553	2.097	380.000
380.614	47.653	45.563	2.09	380.614
400.000	47.750	46.373	1.377	400.000
420.000	47.850	46.720	1.13	420.000
440.000	47.950	46.948	1.002	440.000
460.000	48.050	47.692	0.358	460.000
480.000	48.161	48.143	0.018	480.000
500.000	48.322	48.430	-0.107	500.000
506.289	48.384	48.506	-0.122	506.289
520.000	48.537	48.750	-0.213	520.000
540.000	48.805	48.949	-0.144	540.000
560.000	49.126	49.338	-0.212	560.000
580.000	49.501	49.709	-0.208	580.000
600.000	49.929	50.107	-0.178	600.000



PLANS AND DOCUMENTS
referred to in the PDA APPROVAL
24 MAY 2013
MEDQ





SECTION 12
150 112

TYPICAL BRIDGE SECTION
15.0m WIDE
(BARRAMS ROAD)

PLANS AND DOCUMENTS
referred to in the PDA APPROVAL
24 MAY 2013
MEDQ

Rev	Date	Description	Drawn	Appr
A	23/07/2013	BRIDGE PROFILE ADDED TO INCREASE BNE LANE WIDTHS	SJT	PJO
B	30/07/2013	LIGHTING COLUMN ADDED TO SECTION	SJT	PJO



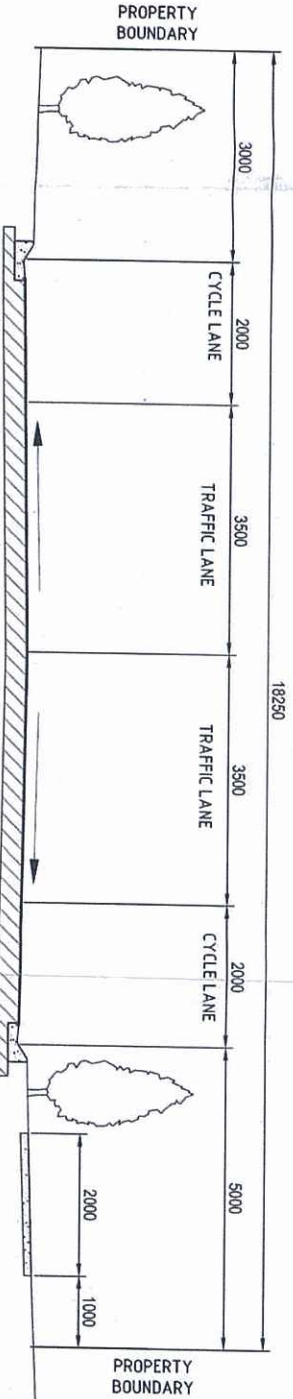
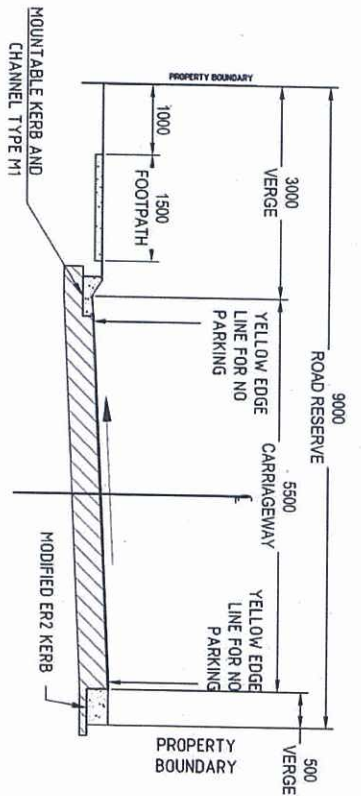
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Check	Design	Draw	Date
Checked D.M.	Designed C.P.	Drawn DT	24/02/2011

Client: AMEX PTY. LTD.
Project: RIPLEY VALLEY - SUCE
STAGE 1 - TRUNK ROAD WORKS
INTERIM CONSTRUCTION
Bridge Cross Section

Drawn	Check	Scale	Size
22/02/2011	24/02/2011	A.H.D.	AS SHOWN
7901.47.012-141			B

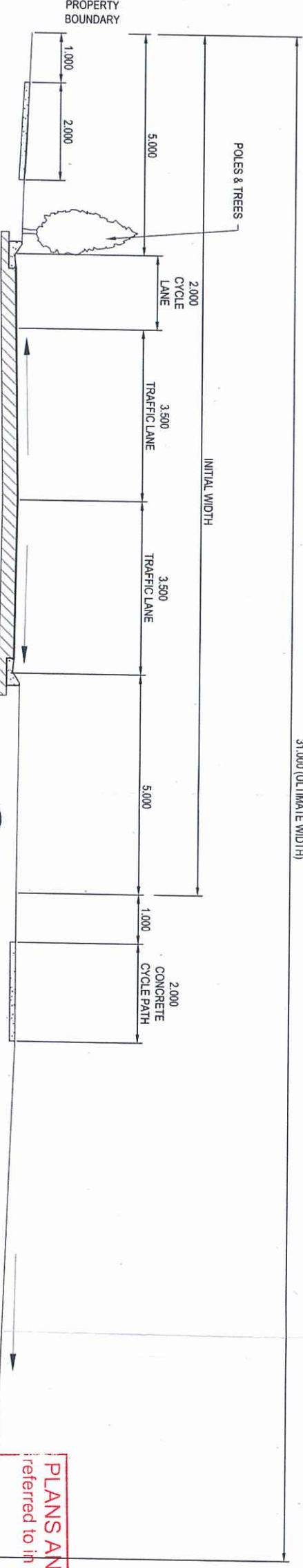


TYPICAL SECTION
9.0m LANEWAY

18.25M RIPLEY ROAD UPGRADE - INTERIM

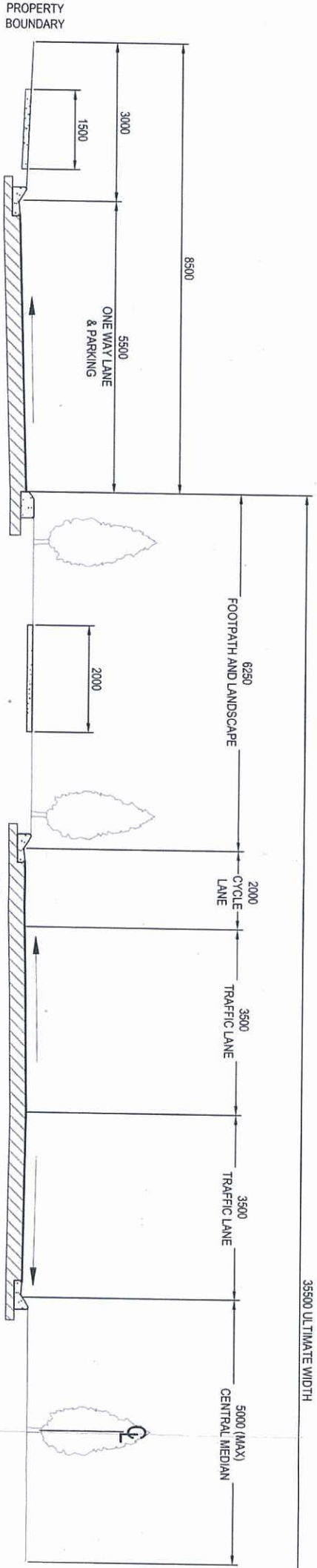
SECTION 11
150 111

31,000 (ULTIMATE WIDTH)



SUBARTERIAL ENTRY BOULEVARD WITHOUT PARKING - INTERIM.
(BARRAMS ROAD)

PLANS AND DOCUMENTS
referred to in the PDA APPROVAL
24 MAY 2013
MEDQ



SUBARTERIAL WITH SERVICE ROAD
(BARRAMS ROAD)

SECTION 14
150 112

Rev	Date	Description	Drawn	Appr
A	23/01/2013	SECTION 14 AMENDED TO INCREASE BKE LANE WIDTHS	SJT	PJO
B	30/01/2013	SECTION 14 AMENDED TO INCREASE BKE LANE WIDTHS	SJT	PJO
C	15/03/2013	SERVICE ROAD SECTION ADDED	SJT	DR
D	18/03/2013	NO PARKING LINE MARKING ADDED TO 9.0m LANEWAY SECTION	SJT	PJO



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Drawn	Rev	Date	Check	Date	Appr
B.V		24/02/2011	D.M	24/02/2011	
C.P		24/02/2011	C.P	24/02/2011	
DT		24/02/2011	DT	24/02/2011	
DT		24/02/2011	DT	24/02/2011	

AMEX PTY. LTD.
RIPLEY VALLEY - SUCE
STAGE 1 - TRUNK ROAD WORKS
INTERIM CONSTRUCTION
ROAD CROSS SECTIONS
SHEET 1 OF 2

Date	Drawn	Scale	Size
22/02/2011	A.H.D.	AS SHOWN	A1
7901.47.012-142			D

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