



PLANS AND DOCUMENTS
referred to in the PDA
DEVELOPMENT APPROVAL

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AMENDED IN RED

By: Jennifer Davison
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AURA

SOUTHERN LOCALITY

PRECINCT 18

PLAN OF DEVELOPMENT

PREPARED BY URBIS FOR STOCKLAND
JULY 2024



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1.1 PLAN OF DEVELOPMENT PURPOSE

This Plan of Development (PoD) has been prepared in accordance with the requirements of the approved Caloundra South Master Plan (Approved 15th June 2012) and will direct the development of land included within the Southern Locality of the Master Plan.

This PoD refines the outcomes approved in the Master Plan.

This PoD describes the development that may occur and provides the assessment criteria and/or approved plans that development must meet in any future assessment process.

The *Caloundra South Urban Development Area Development Scheme* (approved October 2011) sets the Vision for this community. The *Caloundra South Master Plan* identifies a range of principles, land use areas, localities and precinct entitlements and obligations.

This PoD applies to the area described as the Southern Locality – Sub-Precinct 18 (part) and provides the instrument for the delivery of development that seeks to achieve the Vision and Principles outlined by these two documents. This Vision and the area to which this PoD applies is graphically illustrated by **Figure 1: AURA SOUTHERN LOCALITY – Illustrative Vision Plan**.

1.2 DEFINED TERMS

Terms used in this PoD have the meaning assigned by the *Economic Development Act 2012* (ED Act), the *Caloundra South UDA Development Scheme 2011* and the *Caloundra South Master Plan* (June 2012).

If there are any inconsistencies between the definitions in these documents, the inconsistency is to be resolved by using the definition contained in the documents in the following order:

- (a) The ED Act; or if there is no definition in the ED Act;
- (b) The Caloundra South UDA Development Scheme; or if there is no definition in the Caloundra South UDA Development Scheme;
- (c) Appendix A of the *Caloundra South Master Plan* (June 2012);
- (d) Any executed infrastructure agreement.

A reference in the PoD to a specific resource document or standard means the current version of that resource document or standard at the date of the approval of this PoD.

A reference to Economic Development Queensland (EDQ) means (pursuant to section 88 of the ED Act) for a PDA development condition or approval – the entity so nominated under section 88(a).



Figure 1 - AURA Southern Locality - Illustrative Vision Plan

1.3 OVERALL OPERATION OF THIS POD AND RELATIONSHIPS TO OTHER APPROVALS

This PoD forms one element of the overall approval framework relevant to the Caloundra South Southern Locality (CSSL) – Precinct 18.

The relationship of this PoD to other approvals or statutory obligations is outlined by Figure 2: PoD Relationships and Operational Overview. This figure also provides a summary of the way in which future development can occur over the CSSL – Precinct 18 which are:

- 1. Approved Development (No Further Assessment): development in accordance with Plans and Development Controls comprising Exempt development, which may proceed to operational works and building works approvals;
- 2. Approved Development (Compliance Assessment): development in accordance with the PoD subject to Approved Compliance Assessment Process; and
- 3. Certification of Operational Works: certification of operational works is undertaken in accordance with the Self Certification Procedures Manual.

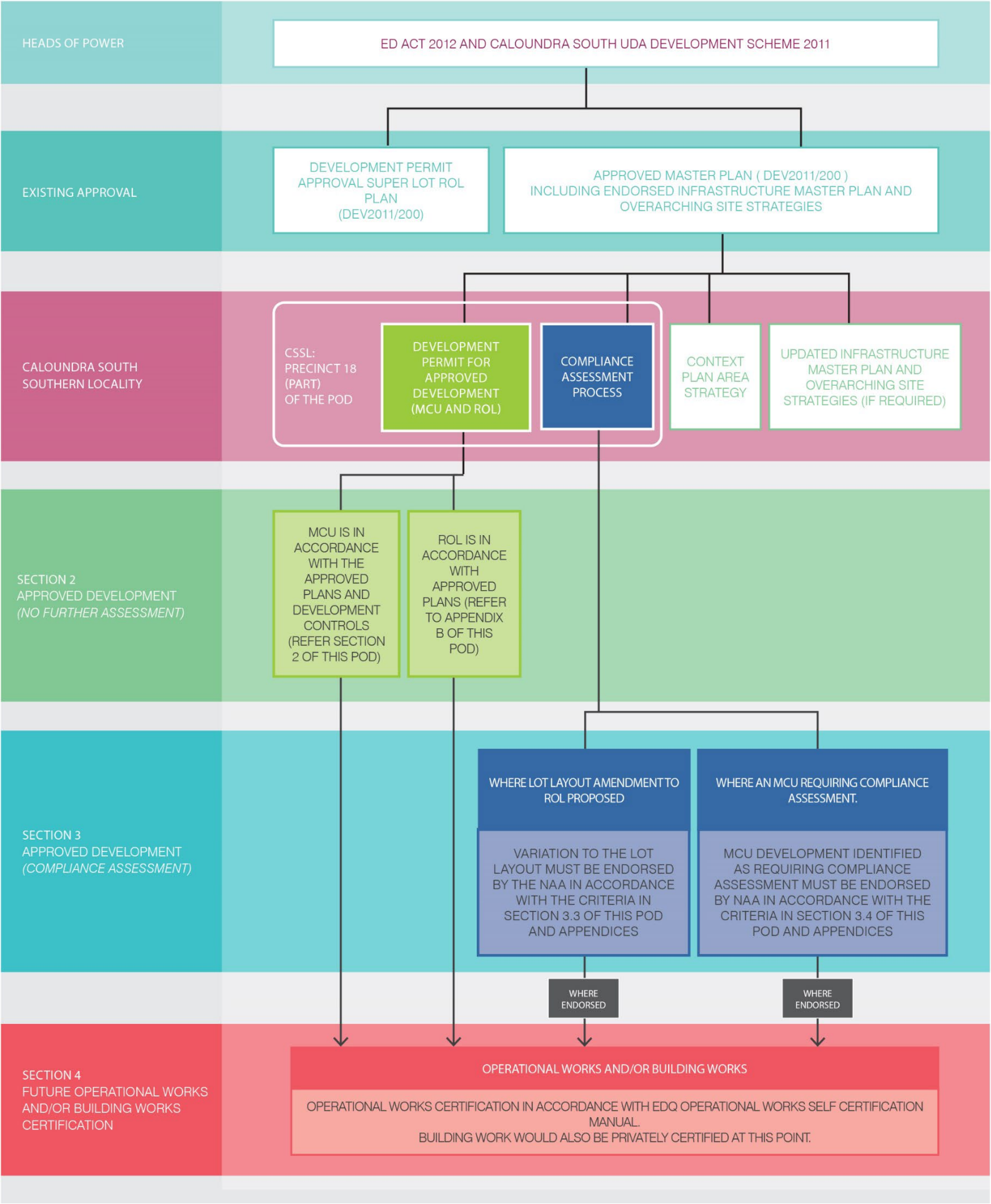
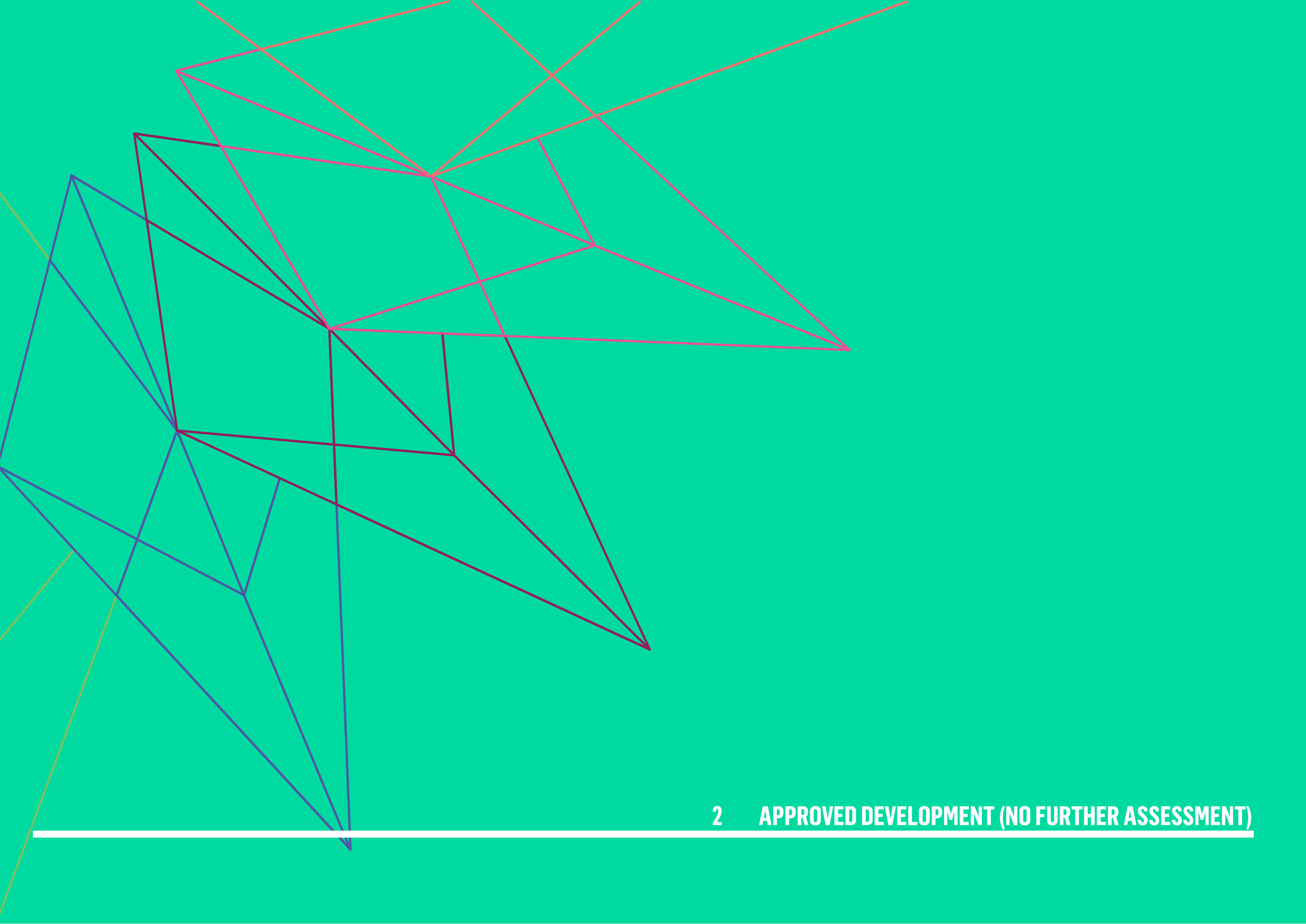


Figure 2 - PoD Relationships and Operational Overview



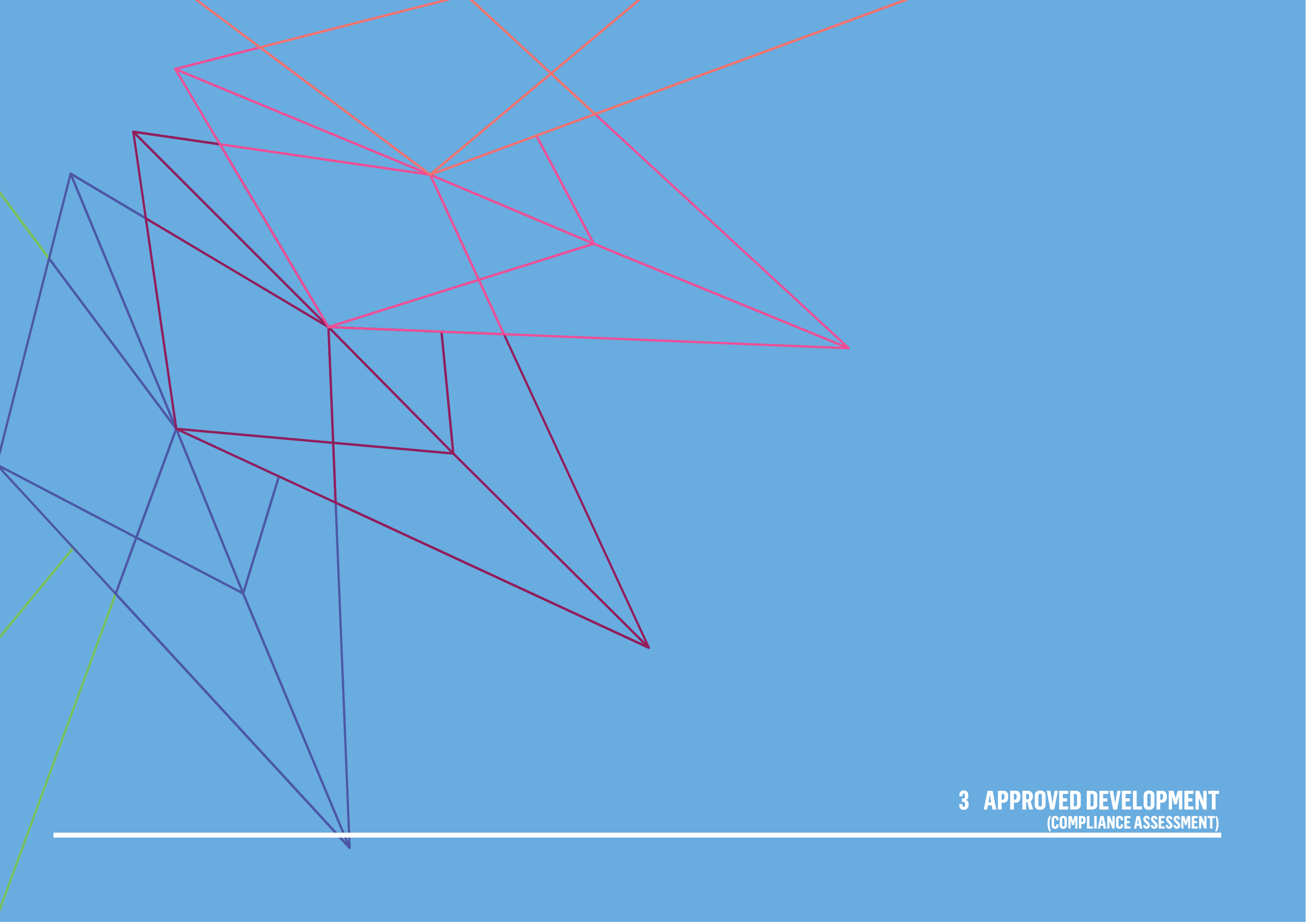
2.1 USES EXEMPT IN ACCORDANCE WITH THIS PLAN OF DEVELOPMENT

This section of the PoD applies to development that is exempt from further assessment where in accordance with the plans and design standards outlined in this section (and related appendices) and may proceed to operational works and building works approvals.

Table 1 - List of Approved Development (No Further Assessment) and Design Standards

| USES | DESIGN STANDARDS | | | | | | | | | | | | | | | | | | | | | | | | | | |
|---|--|---|--|---|--|--|--|--------------------------------|---|-----------------------------|---|---------------------------------|---|-----------------|---|--------------------|---|----------------------------|---|--------------------|---|----------------|---|------------------------------------|--|--------------------------|--|
| Reconfiguring a Lot | Reconfiguring a Lot plans have been prepared for the Southern Locality – Precinct 18 which reflect the proposed lot layout. These plans are included in Appendix A of the PoD for ease of reference. Reconfiguring a Lot undertaken in accordance with these plans is Approved Development (No Further Assessment) and can proceed to operational works certification and plan sealing in accordance with the process outlined in Section 1.3 | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Advertising Devices | Advertising devices in accordance with the controls in Appendix H is Approved Development (No Further Assessment) and can proceed to certification of operational works and building works. | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Car Park (where associated with Display Home and/or Sales Office); | A Car Park (where associated with Display Home and/or Sales Office) is Approved Development (No Further Assessment) and can proceed to certification of operational works and building works where: <ul style="list-style-type: none">The use is provided in conjunction with a Display Home and/or Sales Office; andSufficient car parking spaces are provided to accommodate number of vehicles likely to be parked at any one time. | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Home Based Business | A Home Based Business is exempt development where the use is in accordance with the provisions of the Home Based Business definition in <i>Caloundra South UDA Development Scheme</i> . | | | | | | | | | | | | | | | | | | | | | | | | | | |
| House or Display Home or Other Residential (where NDIS dwelling) | A House, Display Home or Other Residential (NDIS dwelling) is Approved Development (No Further Assessment) and can proceed to certification of operational works and building works where: <ul style="list-style-type: none">The use is generally in accordance with the Approved Plan of Development in Appendix B and Residential Design Controls in Appendix C; andSufficient car parking spaces are provided to accommodate number of vehicles likely to be parked at any one time. | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Secondary Dwellings | <div>Secondary Dwellings are Approved Development (no further assessment) provided they meet all of the criteria and development controls in Table 2 below and achieves compliance with QDC MP4.4 – Buildings in a Transport Noise Corridor.</div> <div>Table 2 - Secondary Dwelling Development Controls</div> <table><tr><th colspan="2">Secondary Dwelling Development Controls</th></tr><tr><td>Floor Area of Secondary Dwelling</td><td>Maximum 45m² GFA (Note: GFA excludes the garage and a 4m² size covered entry porch area only).</td></tr><tr><td>Design and Siting of buildings and structures</td><td>To be in accordance with the relevant Allotment Diagram in Appendix B. Where not on a corner lot, the dwelling/ secondary dwelling must have the design effect of one (1) single residential dwelling from the road/street frontage or within public view.</td></tr><tr><td>Materials and detailing</td><td>Materials, detailing, colours and roof form are consistent with those of the primary house.</td></tr><tr><td>Outdoor Living Space</td><td>Minimum 9m² with a minimum dimension of 3m and directly accessible from a main living area.</td></tr><tr><td>Car Parking and Garaging</td><td>Minimum one space in addition to the primary dwelling requirement – with minimum dimensions of 5m x 3m.</td></tr><tr><td>Driveway</td><td>Shared minimum driveway with the primary house.</td></tr><tr><td>Front Entry</td><td>If the lot is on a corner – dedicated pedestrian entry and door visible from and addressing the secondary street.</td></tr><tr><td>Street Surveillance</td><td>On a single street frontage the secondary dwelling entry must be hidden from view from the street so as to give the effect that the residential dwelling.</td></tr><tr><td>Letterboxes</td><td>Any Secondary Dwelling is not permitted its own letterbox and must be shared with the primary dwelling.</td></tr><tr><td>Titling</td><td>Any Secondary Dwelling is unable to be separately titled to the primary dwelling.</td></tr><tr><td>Rainwater Tank Requirements</td><td>The requirement of a 5000L rainwater tank is to be applied to the dwelling treated as one (1) combined dwelling.</td></tr><tr><td>Separate Services</td><td>No separate infrastructure servicing to the secondary dwelling is permitted (ie. water, gas, electricity).</td></tr></table> | Secondary Dwelling Development Controls | | Floor Area of Secondary Dwelling | Maximum 45m ² GFA (Note: GFA excludes the garage and a 4m ² size covered entry porch area only). | Design and Siting of buildings and structures | To be in accordance with the relevant Allotment Diagram in Appendix B . Where not on a corner lot, the dwelling/ secondary dwelling must have the design effect of one (1) single residential dwelling from the road/street frontage or within public view. | Materials and detailing | Materials, detailing, colours and roof form are consistent with those of the primary house. | Outdoor Living Space | Minimum 9m ² with a minimum dimension of 3m and directly accessible from a main living area. | Car Parking and Garaging | Minimum one space in addition to the primary dwelling requirement – with minimum dimensions of 5m x 3m. | Driveway | Shared minimum driveway with the primary house. | Front Entry | If the lot is on a corner – dedicated pedestrian entry and door visible from and addressing the secondary street. | Street Surveillance | On a single street frontage the secondary dwelling entry must be hidden from view from the street so as to give the effect that the residential dwelling. | Letterboxes | Any Secondary Dwelling is not permitted its own letterbox and must be shared with the primary dwelling. | Titling | Any Secondary Dwelling is unable to be separately titled to the primary dwelling. | Rainwater Tank Requirements | The requirement of a 5000L rainwater tank is to be applied to the dwelling treated as one (1) combined dwelling. | Separate Services | No separate infrastructure servicing to the secondary dwelling is permitted (ie. water, gas, electricity). |
| Secondary Dwelling Development Controls | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Floor Area of Secondary Dwelling | Maximum 45m ² GFA (Note: GFA excludes the garage and a 4m ² size covered entry porch area only). | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Design and Siting of buildings and structures | To be in accordance with the relevant Allotment Diagram in Appendix B . Where not on a corner lot, the dwelling/ secondary dwelling must have the design effect of one (1) single residential dwelling from the road/street frontage or within public view. | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Materials and detailing | Materials, detailing, colours and roof form are consistent with those of the primary house. | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Outdoor Living Space | Minimum 9m ² with a minimum dimension of 3m and directly accessible from a main living area. | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Car Parking and Garaging | Minimum one space in addition to the primary dwelling requirement – with minimum dimensions of 5m x 3m. | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Driveway | Shared minimum driveway with the primary house. | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Front Entry | If the lot is on a corner – dedicated pedestrian entry and door visible from and addressing the secondary street. | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Street Surveillance | On a single street frontage the secondary dwelling entry must be hidden from view from the street so as to give the effect that the residential dwelling. | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Letterboxes | Any Secondary Dwelling is not permitted its own letterbox and must be shared with the primary dwelling. | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Titling | Any Secondary Dwelling is unable to be separately titled to the primary dwelling. | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Rainwater Tank Requirements | The requirement of a 5000L rainwater tank is to be applied to the dwelling treated as one (1) combined dwelling. | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Separate Services | No separate infrastructure servicing to the secondary dwelling is permitted (ie. water, gas, electricity). | | | | | | | | | | | | | | | | | | | | | | | | | | |

| USES | DESIGN STANDARDS | | | | | | | | | | | | | | | | |
|--|--|-----------------------------------|--|---------------------------|---|-------------------------|---|--------------------|---|-------------------|--|----------------|---|----------------|--|-------------------------|---|
| Multiple Residential (Duplex) | <p>Multiple Residential (duplex) is Approved Development (No Further Assessment) and can proceed to certification of operational works and building works, where:</p> <ul style="list-style-type: none"> The land is identified on the Approved Plan of Development included at Appendix B; and In accordance with the relevant criteria in Appendix C (1.1.1 - House & Multiple Residential Allotments). | | | | | | | | | | | | | | | | |
| Park | A Park is exempt development in accordance with Schedule 1 of the Caloundra South Urban Development Area Development Scheme and can proceed to certification of operational works and building works. | | | | | | | | | | | | | | | | |
| Sales Office (where not greater than 400m ²) | <p>A Sales Office is Approved Development (No Further Assessment) and can proceed to certification of operational works and building works, where in accordance with the following criteria and development controls.</p> <p>Table 3 – Sales Office Development Controls</p> <table> <tr> <th colspan="2">Sales Office Development Controls</th></tr> <tr> <td>Hours of Operation</td><td>The hours of operation of the sales office do not commence before 8.00am or extend later than 6.00pm.</td></tr> <tr> <td>Gross Floor Area</td><td>The maximum GFA for a Sales Office is 400m²</td></tr> <tr> <td>Car Parking</td><td>A minimum of 2 on-site car parking spaces are provided.</td></tr> <tr> <td>Open Space</td><td>Private and public open space areas are turfed and landscaped.</td></tr> <tr> <td>Fencing</td><td>A 1.8 metre high solid screen fence is provided to each side and rear boundary that has residential uses adjoining.</td></tr> <tr> <td>Amenity</td><td>If the sales office is in conjunction with a Display Village which comprises 4 or more Display Homes, public toilet facilities are to be provided.</td></tr> <tr> <td>End of Operation</td><td>Any temporary building or structure associated with the operation of the sales office is removed from the site within 14 days of the end of the period of operation and the site is left in a clean and tidy condition.</td></tr> </table> | Sales Office Development Controls | | Hours of Operation | The hours of operation of the sales office do not commence before 8.00am or extend later than 6.00pm. | Gross Floor Area | The maximum GFA for a Sales Office is 400m ² | Car Parking | A minimum of 2 on-site car parking spaces are provided. | Open Space | Private and public open space areas are turfed and landscaped. | Fencing | A 1.8 metre high solid screen fence is provided to each side and rear boundary that has residential uses adjoining. | Amenity | If the sales office is in conjunction with a Display Village which comprises 4 or more Display Homes, public toilet facilities are to be provided. | End of Operation | Any temporary building or structure associated with the operation of the sales office is removed from the site within 14 days of the end of the period of operation and the site is left in a clean and tidy condition. |
| Sales Office Development Controls | | | | | | | | | | | | | | | | | |
| Hours of Operation | The hours of operation of the sales office do not commence before 8.00am or extend later than 6.00pm. | | | | | | | | | | | | | | | | |
| Gross Floor Area | The maximum GFA for a Sales Office is 400m ² | | | | | | | | | | | | | | | | |
| Car Parking | A minimum of 2 on-site car parking spaces are provided. | | | | | | | | | | | | | | | | |
| Open Space | Private and public open space areas are turfed and landscaped. | | | | | | | | | | | | | | | | |
| Fencing | A 1.8 metre high solid screen fence is provided to each side and rear boundary that has residential uses adjoining. | | | | | | | | | | | | | | | | |
| Amenity | If the sales office is in conjunction with a Display Village which comprises 4 or more Display Homes, public toilet facilities are to be provided. | | | | | | | | | | | | | | | | |
| End of Operation | Any temporary building or structure associated with the operation of the sales office is removed from the site within 14 days of the end of the period of operation and the site is left in a clean and tidy condition. | | | | | | | | | | | | | | | | |



3 APPROVED DEVELOPMENT (COMPLIANCE ASSESSMENT)



3.1 INTRODUCTION

This section provides the development controls for Approved Development (Compliance Assessment) for development under the process outlined in Section 1.3 of this PoD and includes the following components:

1. A statement and illustration of the overall intent for the Sub-Precinct and land uses proposed; and
2. Uses that are Approved Development subject to Compliance Assessment and the corresponding design standards.

3.2 USES SUBJECT TO COMPLIANCE ASSESSMENT

Uses listed in Table 2 and complying with the relevant design standards outlined in this section (and related appendices) are approved subject to Compliance Assessment in accordance with the process outlined in Figure 2.

Table 2 – Uses subject to compliance assessment

| Land uses | | | |
|---|--|---|--|
| Lot 8000 | Lot 8001, 8002 and 8004 | Lot 8003 and 8006 | Lot 9000 |
| <ul style="list-style-type: none">Child Care Centre | <ul style="list-style-type: none">Multiple Residential | <ul style="list-style-type: none">Educational EstablishmentCommunity Use | <ul style="list-style-type: none">Outdoor Sport and Recreation |

3.3 OVERALL SUB-PRECINCT INTENT LAND USES

The Precinct is predominately of a low density nature with a range of low-medium rise residential dwelling types strategically positioned to take advantage of view lines and connection to the abundant green edges and recreational parks. The Sub-Precinct delivers a high-quality arrival experience focusing on landscaping to extend green elements into the residential areas through view corridors, green streets and pedestrian links. The built form of the Sub-Precinct reflects the residential nature of the area, addressing the street and promotes overlooking of the street to maximise casual surveillance.

The Sub-Precinct will also facilitate opportunities for the delivery of Multiple Residential dwellings on super lots, strategically positioned adjoining parkland and open space areas which are interactive, functional and suitable for both passive and active recreation.

Land uses within this Sub-Precinct do not compromise the intended role or successful functioning of the Town Centre or future Centres. Urban Design and built form outcomes are intended to create a functional and attractive streetscape with buildings addressing the street and promoting overlooking of the street to maximise casual surveillance. Collectively, the Sub-Precinct will create an innovative and sustainable community, where residents experience a sense of belonging and engagement.

Provided below is a summary of the overall land uses proposed for within the Sub-Precinct which are subject to ‘compliance assessment’. The criteria and development controls for these uses are detailed in **Section 3.3**.

Table 3 – Summary of Compliance Assessable Uses and Yields

| Sub-precinct | Total residential lots (minimum +/- 15%) | Total multiple residential (other than duplex) (minimum +/- 15%) | Max retail use GFA | Max commercial use GFA | Community facility provision | Total park area |
|--------------|--|--|--------------------|------------------------|------------------------------|--|
| 18.1 | 744 standard residential lots 5 x multiple residential (duplex) allotments (10 dwellings max.) Total of 749 max. dwellings including multiple residential (duplex) dwellings | 169 dwellings | N/A | N/A | 1.0ha Emerging Community | 0.663ha neighbourhood recreation park 0.822ha local recreation parks 6.507ha neighbourhood sports park 0.663ha neighbourhood recreation park 1.494ha local linear park |

Table 4 - Total dwellings - comparison with Master Plan

| Precinct | Current application total dwelling yield (precinct 18.1) | Future application total dwelling yield (precinct 18) | Total yield for precinct 18 |
|-------------|--|---|-----------------------------|
| 18.1 | 918 dwellings | 1,432 dwellings | 2,350 dwellings |
| Total yield | 918 dwellings | 1,432 dwellings | 2,350 dwellings |

3.3.1 Lot 8000 (Child Care Centre) – Design Standards

Application for compliance assessment for a Child Care Centre on Lot 8000 in Sub-Precinct 18.1 is required to be assessed against and fulfil all the Design Standards identified in **Table 5** below.

Table 5 – Lot 8000 (Child Care Centre) Design Standards

| ELEMENT | DESIGN STANDARDS |
|------------|--|
| Use | DS1.1 – Land uses comprise the uses specified for the Sub-Precinct in Table 4 (i.e. Child Care Centre) |
| | DS1.2 – Development is to be delivered generally in accordance with the Plan of Development for Lot 8000 as shown on Figure 3 . |
| Built form | DS2.1 – Building height is consistent with that identified on Figure 3 . |
| | DS2.2 - The Child Care centre has a maximum site cover of 50%. |
| | DS2.3 – All buildings, structures and outdoor play areas are set back at least 3 metres from all site boundaries adjoining a residential use (or future residential use). |
| | DS2.4 - Where development is located on a 'Desired Building Frontage' identified on Figure 3 . a. A building must: <ul style="list-style-type: none">• Present a minimum of 70% of the building as active frontage;• Include a combination of design elements such as projections, recesses and openings;• Generates visual interest at the street level, having regard to the proportion of openings, windows, materials and features. Blank walls are avoided;• 'Indicative Landscape Screening' to provide a soft landscaped strip of at least 2m wide along the boundary;• Address the street and/or carpark frontage or frontages by:<ul style="list-style-type: none">i. Providing clear, legible entry points for both pedestrians and vehicles;ii. Maximising opportunities for overlooking and casual surveillance of streets, public spaces, parking areas and pedestrian/cycling paths; |
| | b. A car park is generally located in accordance with Figure 4 and reduce visual dominance from the public realm by providing: <ul style="list-style-type: none">• A soft landscaped strip of at least 2m wide along the boundary. 1 shade tree per 6 car parking spaces for open air car parks unless otherwise provided by shade structures. |
| | DS2.5 - Child care centres are to be located on a site having: a. A slope of not more than 10%; b. A regular shape; and c. A minimum area of 1,500m ² |
| | DS2.6 – Convenient, safe and clearly visible pedestrian access is available within and to the site which does not cross across driveways. |
| | DS2.7 – Development provides loading, servicing and back of house areas generally in accordance with the locations shown in Figure 3 . |

table 2

| ELEMENT | DESIGN STANDARDS |
|-----------------|--|
| | DS2.8 - Development on a 'Key Corner Site' in Figure 3 provides a landscape or built form statement to this site which: <ul style="list-style-type: none">a. Ensure that blank walls of buildings or back of house areas are not located on external corner frontages;b. Built form is provided to this external frontage and is articulated through the use of glass, openings and recesses; andc. Building treatments are designed to respond to the primacy of the corner through a combination of architectural treatments, additional building height and responsive roof articulation. |
| Building Design | DS3.1 – Building design is climatically responsive. Buildings include the following: <ul style="list-style-type: none">• Buildings have cross ventilation through habitable rooms• Buildings are orientated to promote seasonal solar heat gain or loss taking into consideration major site views and vistas• Large building facades incorporate architectural wall shading to reduce solar heat gain• External windows have sun shading |
| | DS3.2 – Mechanical plant, equipment and storage areas equipment are: <ul style="list-style-type: none">• If on the roof:<ul style="list-style-type: none">- Screened and designed as an architectural feature of buildings; and- Incorporated into the roof form.• Effectively screened from view from adjoining streets so as to provide an attractive address to streets and adjoining properties. |
| | DS3.3 – External facade materials include a mix of two or more of the following: <ul style="list-style-type: none">a. Glazing clear tinted or colour backed;b. Brickwork;c. Timber;d. Coloured rendered/bagged finish or split face concrete block work;e. Precast concrete panels;f. Economical panel systems including prefinished metal panels, tiles, stones; or Recycled materials (e.g. timber). |
| | DS3.4 – The maximum length of a uniform elevation treatment above ground storey without variation or articulation is 40m. |
| Public realm | DS4.1 – A minimum of 10% of the site is comprised of planted landscapes. |
| | DS4.2 – Street frontages are unfenced or where street frontage fencing is required for security and CPTED purposes, it should be transparent (minimum 70% open). |

| ELEMENT | DESIGN STANDARD |
|---------------------------|---|
| Parking and access | DS5.1 – On site car parking is provided at the following rate: <ul style="list-style-type: none"> 1 employee space / employee + 1 customer space / 5 children |
| | DS5.2 – Service vehicle space is provided in accordance with the following: <ul style="list-style-type: none"> VAN + WCV (where >200m² GFA); and All vehicles to enter and leave in a forward motion. |
| | DS5.3 - Vehicular access points do not result in queuing across pedestrian/cycle paths and do not cause interruption to traffic on surrounding roads. |
| | DS5.4 - Parking bays, manoeuvring areas, queuing areas, set down/pickup areas, aisles and driveways are designed in accordance with the dimensions and to the standards specified in: <ul style="list-style-type: none"> AS2890.1 Parking Facilities – Off-street Car Parking, as amended; and AS2890.2 Parking Facilities – Off-street Commercial Vehicle facilities. |
| | DS5.5 – Access to the site provides separation of a minimum 10m to an intersecting street where the driveway is on the same side of the street. |
| | DS5.6 – Bicycle parking spaces are provided in accordance with the following rate: <ul style="list-style-type: none"> Employee parking spaces - 1 per 190m² GFA Visitor/customer parking spaces – 1 per 700m² GFA Motorcycle parking spaces – 1 per 100m² GFA |
| | DS5.7 –Engineering design of all parking and manoeuvring areas is in accordance with Councils adopted standards. |
| Environment | DS6.1 – Development achieves the noise generation levels set out in the Environmental Protection (Noise) Policy 2008, as amended. |
| | DS6.2 – Development achieves the air quality objectives set out in the Environmental Protection (Air) Policy 2008, as amended. |
| | DS6.3 – Light emanating from any source complies with Australian Standard AS4282 Control of the Obtrusive Effects of Outdoor Lighting, as amended. |
| | DS6.4 – Outdoor lighting is provided in accordance with Australian Standard AS 1158.1.1 –Road Lighting – Vehicular Traffic (Category V) Lighting – Performance and Installation Design Requirements, as amended. Where car park lighting is proposed, this is provided in accordance with Australian Standard AS1158.3 |
| | DS6.5 – Any reflective glass material has: <ol style="list-style-type: none"> A level of light reflectivity of not greater than 20%; and A level of heat transmission of not less than 20%. |
| | DS6.6 – Development does not include the storage of dangerous goods as defined by the Work Health and Safety Act 2011, as amended. |
| | DS6.7 – All onsite landscaping adjacent to accessway and car parking area to allow for passive irrigation. Site specific stormwater quality treatment is not required. |
| Infrastructure | DS7.1 - All development is connected to reticulated water supply, sewerage, stormwater drainage and telecommunication infrastructure and has an electricity supply. |

| ELEMENT | DESIGN STANDARD |
|------------------------|---|
| Rainwater tanks | DS8.1 – All rainwater tanks are designed to ensure 75% roof capture and reuse, with collected water used for all suitable applications within the property. |
| | DS8.2 - Tanks must be installed in accordance with all Council, State Government, Federal Government and industry plumbing standards for rainwater tanks |
| | DS8.3 – Tanks must be connected to and supply water to all of the following: <ul style="list-style-type: none"> Toilets (all toilets) Urinals (all urinals) Laundry (all cold taps in laundry) Outdoor taps (all out door taps). |

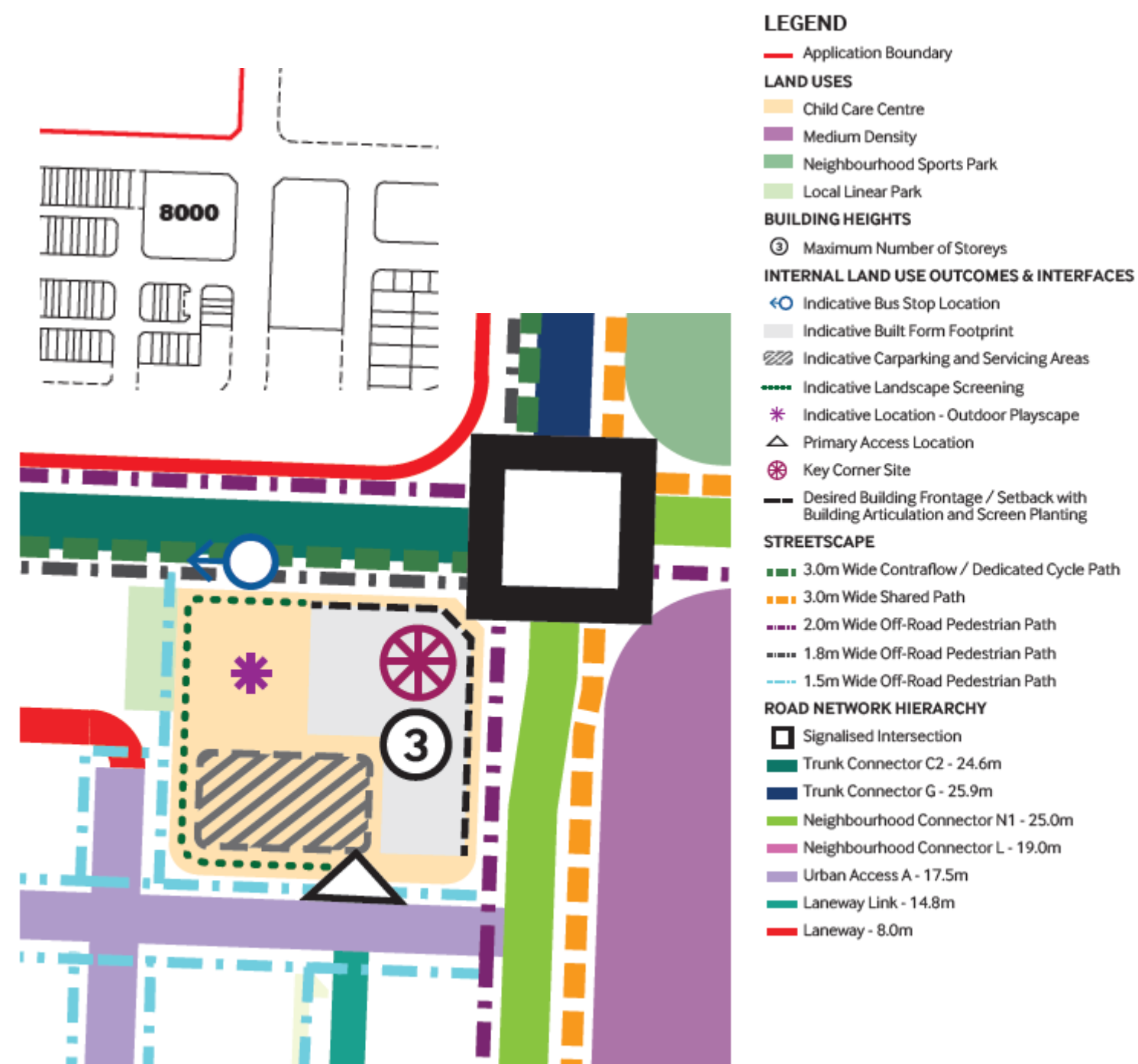


Figure 3 – Lot 8000 – Plan of Development

3.3.2 Lot 8001, 8002 and 8004 (Multiple Residential – Other than Duplex) – Design Standards

Applications for compliance assessment for Multiple Residential (Other than Duplex) on Lots 8001, 8002 and 8004 in Sub-Precinct 18.1 is required to be assessed against and fulfil all the Design Standards identified in Table 6 below.

Table 6 – Lot 8001, 8002 & 8004 (Multiple Residential – Other than Duplex) Design Standards

| ELEMENT | DESIGN STANDARDS | ELEMENT | DESIGN STANDARDS | | | | | | | | | | | | | | | | | | | | | | |
|--|--|--------------------------------------|--|--|-----------------------------|------------------------------|-----------|---------------------------------|--|---------------------|--|--|------|--|----|--------------------------------|--|---|-------|--------------------------------|--|---|-------|--|---|
| Use | DS1.1 – Land uses comprise the uses specified for the Sub-Precinct in Table 2 (i.e. Multiple Residential – Other than Duplex). | Built form (Cont.) | <ul style="list-style-type: none">The number of dwellings, windows and balconies of habitable rooms that address adjoining streets, communal recreation areas and open spaces is optimised; andProvides a dedicated car wash bay per Lot. | | | | | | | | | | | | | | | | | | | | | | |
| | DS1.2 – Development is to be delivered generally in accordance with the Plan of Development for Lots 8001, 8002 and 8004 as shown on Figure 4 . | | | | | | | | | | | | | | | | | | | | | | | | |
| Built form | DS2.1 – The number of dwellings is in accordance with Table 4: Compliance Assessable Uses . | | | | | | | | | | | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | | | | | | | | | | | | |
| | DS2.2 – Building height is consistent with that identified on Figure 4 . | | DS2.6 – Non-habitable rooms of one dwelling are not located opposite windows of another dwelling unless views are controlled by screening devices, distance, landscaping or design of the opening. | | | | | | | | | | | | | | | | | | | | | | |
| | DS2.3 - The site cover for Multiple Residential buildings does not exceed 60%. | | DS2.7 – Where habitable room windows look directly at habitable room windows and adjacent dwelling within 2 metres at the ground storey, or 9 metres at levels above the ground storey, privacy is protected by: a. Window sill heights being a minimum of 1.5 metres above floor level; or b. Fixed translucent glazing being applied to any part of a window below 1.5 metres above floor level; or c. Fixed external screens; or d. If at ground level screen fencing to a maximum height of 1.8 metres. | | | | | | | | | | | | | | | | | | | | | | |
| | DS2.4 – Building setbacks comply with those prescribed below: <table><tr><td></td><td>Medium rise (up to 3 storeys)</td></tr><tr><td></td><td>Residential elements</td></tr><tr><td>Maximum podium height</td><td>2 storeys</td></tr><tr><td colspan="2">Front setbacks (minimum)</td></tr><tr><td>Ground floor</td><td></td></tr><tr><td><ul style="list-style-type: none">Active frontage</td><td>0.0m</td></tr><tr><td><ul style="list-style-type: none">Non-active frontage</td><td>2m</td></tr><tr><td colspan="2">Side setbacks (minimum)</td></tr><tr><td><ul style="list-style-type: none">Levels 1-3</td><td>0.0m.</td></tr><tr><td colspan="2">Rear setbacks (minimum)</td></tr><tr><td><ul style="list-style-type: none">Levels 1-3</td><td>0.0m.</td></tr></table> | | Medium rise (up to 3 storeys) | | Residential elements | Maximum podium height | 2 storeys | Front setbacks (minimum) | | Ground floor | | <ul style="list-style-type: none">Active frontage | 0.0m | <ul style="list-style-type: none">Non-active frontage | 2m | Side setbacks (minimum) | | <ul style="list-style-type: none">Levels 1-3 | 0.0m. | Rear setbacks (minimum) | | <ul style="list-style-type: none">Levels 1-3 | 0.0m. | | DS2.8 – The length of any unarticulated elevation of a building, fence or other structure visible from the street does not exceed 15 metres. |
| | | Medium rise (up to 3 storeys) | | | | | | | | | | | | | | | | | | | | | | | |
| | | Residential elements | | | | | | | | | | | | | | | | | | | | | | | |
| | Maximum podium height | 2 storeys | | | | | | | | | | | | | | | | | | | | | | | |
| | Front setbacks (minimum) | | | | | | | | | | | | | | | | | | | | | | | | |
| | Ground floor | | | | | | | | | | | | | | | | | | | | | | | | |
| <ul style="list-style-type: none">Active frontage | 0.0m | | | | | | | | | | | | | | | | | | | | | | | | |
| <ul style="list-style-type: none">Non-active frontage | 2m | | | | | | | | | | | | | | | | | | | | | | | | |
| Side setbacks (minimum) | | | | | | | | | | | | | | | | | | | | | | | | | |
| <ul style="list-style-type: none">Levels 1-3 | 0.0m. | | | | | | | | | | | | | | | | | | | | | | | | |
| Rear setbacks (minimum) | | | | | | | | | | | | | | | | | | | | | | | | | |
| <ul style="list-style-type: none">Levels 1-3 | 0.0m. | | | | | | | | | | | | | | | | | | | | | | | | |
| | | | DS2.9 – Front fences and walls: a. Have a maximum height of not more than: b. 1.8 metres if 50% transparent; or c. 1.2 metres if solid. | | | | | | | | | | | | | | | | | | | | | | |
| | | | DS2.10 – Building bulk is reduced by incorporating a combination of the following elements in building design: a. Variations in plan shape, such as curves, steps, recesses, projections or splays; b. Variations in vertical profile, with steps or slopes at different levels; c. Variations in the treatment and patterning of windows, sun protection and shading devices, or other elements of a façade treatment at a finer scale than the overall building structure; d. Balconies, verandahs or terraces; and e. Planting, particularly on podiums, terraces and low level roof decks. | | | | | | | | | | | | | | | | | | | | | | |
| | | | DS2.11 – Entry areas for the residents and visitors provide: a. For safe entry from streets, car parking areas and servicing areas; b. Clearly marked, safe and secure parking areas are provided for residents and visitors which is separate from parking areas provided for servicing; and | | | | | | | | | | | | | | | | | | | | | | |
| | | | | | | | | | | | | | | | | | | | | | | | | | |

table 3 - Summary of compliance assessable uses and yields

| ELEMENT | DESIGN STANDARDS |
|-----------------------|---|
| Built form (Cont.) | c. Security measures are installed such that building users do not have access to areas that are intended for the exclusive use of residents and visitors. |
| | DS2.12 - Where development is located on a 'Desired Building Frontage' identified on Figure 4 . |
| | a. A building must: <ul style="list-style-type: none"> • Present a minimum of 70% of the building as active frontage; • Include a combination of design elements such as projections, recesses and openings to enhance the sense of arrival to the precinct; • Generates visual interest at the street level, having regard to the proportion of openings, windows, materials and features. Blank walls are avoided; • Address the street frontage or frontages by: <ol style="list-style-type: none"> i. Providing clear, legible entry points for both pedestrians and vehicles; ii. Maximising opportunities for overlooking and casual surveillance of streets, public spaces, parking areas and pedestrian/cycling paths; |
| | b. A car park is generally located in accordance with Figure 4 and reduce visual dominance from the public realm by providing: <ul style="list-style-type: none"> • A soft landscaped strip of at least 2m wide along the boundary. • 1 shade tree per 6 car parking spaces for open air car parks unless otherwise provided by shade structures. |
| | DS2.13 – Building design must ensure privacy for habitable spaces in dwellings. Acceptable treatments include: <ul style="list-style-type: none"> • Providing a minimum separation distance of: <ul style="list-style-type: none"> ○ Where 5 storeys or less, 12m between windows or between a window and a balcony that are offset by less than 45 degrees, or 9m between windows or between a window and a balcony that are offset by 45 degrees or more, or ○ For a wall containing windows or balconies, 6m to a side or rear boundary where there is no existing or approve development and the future privacy and development potential of the adjoining site should be protected. • Window sill heights a minimum of 1.5m above floor level • Fixed opaque glazing in any part of a window below 1.5m above floor level • Fixed external screens to balconies or windows (or intermediate fencing at ground level). External screens or fences provided to ensure privacy should be either solid, of translucent material or present an appearance of no more than 25% openings when viewed from the nearby balcony or window. |
| | DS2.14 - Development on a 'Key Corner Site' in Figure 4 provides a landscape or built form statement to this site which: <ol style="list-style-type: none"> a. Ensure that blank walls of buildings or back of house areas are not located on external corner frontages; b. Built form is provided to this external frontage and is articulated through the use of glass, openings and recesses; and c. Building treatments are designed to respond to the primacy of the corner through a combination of architectural treatments, additional building height and responsive roof articulation. |

| ELEMENT | DESIGN STANDARDS |
|---------------------|--|
| Building Design | DS3.1 – Building design is climatically responsive. Buildings include the following: <ul style="list-style-type: none"> • Buildings have cross ventilation through habitable rooms • Buildings are orientated to promote seasonal solar heat gain or loss taking into consideration major site views and vistas • Large building facades incorporate architectural wall shading to reduce solar heat gain • External windows have sun shading |
| | DS3.2 – Mechanical plant, equipment and storage areas equipment are: <ul style="list-style-type: none"> • If on the roof: <ul style="list-style-type: none"> - Screened and designed as an architectural feature of buildings; and - Incorporated into the roof form. • Effectively screened from view from adjoining streets so as to provide an attractive address to streets and adjoining properties. |
| | DS3.3 – External facade materials include a mix of two or more of the following: <ol style="list-style-type: none"> a. Glazing clear tinted or colour backed; b. Brickwork; c. Timber; d. Coloured rendered/bagged finish or split face concrete block work; e. Precast concrete panels; f. Economical panel systems including prefinished metal panels, tiles, stones; or g. Recycled materials (e.g. timber). |
| | DS3.4 – The maximum length of a uniform elevation treatment above ground storey without variation or articulation is 40m. |
| Open Space | DS4.1 – Provide the following: <ul style="list-style-type: none"> • A 2 metre wide landscaped buffer strip is provided along the full frontage (non-active frontage only) of the site; • A minimum 1.8 metre high solid screen fence is provided and maintained along the full length of any side or rear boundary except where adjoining open space, which requires fencing to be 25% transparent, or 1.5m high and solid. |
| Communal Open Space | DS5.1 – Where more than 6 dwellings, 10% of the site area is provided as communal open space exclusive required buffer strips and clothes drying areas. |
| | DS5.2 – The development provides social and recreational facilities in the form of: <ol style="list-style-type: none"> a. Communal dining room; b. Communal indoor social/recreation space; and c. A diversity of informal indoor and outdoor social spaces. |
| | DS5.3 – Communal buildings are easily accessible and centrally located, and residents are able to easily navigate the site on foot or with the assistance of mobility aids. |
| Private Open Space | DS6.1 – For all dwellings uses involving more than two units: |

| ELEMENT | DESIGN STANDARDS |
|-----------------------------------|---|
| Private Open Space (Cont.) | <p>a. Each ground floor dwelling has a courtyard or similar private open space of not less than 15m² with a minimum dimension of 3m directly accessible from the main living area;</p> <p>b. Each dwelling above ground level has a balcony or similar private open space area of not less than 9m² for a 1 bedroom unit, and 16m² for 2+ bedroom units (with a minimum dimension of 3m) directly accessible from the living area of the dwelling or rooming unit.</p> |
| Parking and access | <p>DS7.1 – On site car parking is provided at the following rate:</p> <ul style="list-style-type: none"> 1 space / dwelling + 1 visitor space / 4 dwellings |
| | <p>DS7.2 – Service vehicle space is provided in accordance with the following:</p> <ul style="list-style-type: none"> Where ≤ 10 dwellings and requiring access via a street – MRV (Type B Access) + VAN Where > 10 dwellings or requiring access via a road – MRV (Type A Access) + VAN + WCV All vehicles to enter and leave in a forward motion. |
| | <p>DS7.3 - Vehicular access points do not result in queuing across pedestrian/cycle paths and do not cause interruption to traffic on surrounding roads.</p> |
| | <p>DS7.4 - Parking bays, manoeuvring areas, queuing areas, set down/pickup areas, aisles and driveways are designed in accordance with the dimensions and to the standards specified in:</p> <ul style="list-style-type: none"> AS2890.1 Parking Facilities – Off-street Car Parking, as amended; and AS2890.2 Parking Facilities – Off-street Commercial Vehicle facilities. |
| | <p>DS7.5 – Access to the site provides separation of a minimum 10m to an intersecting street where the driveway is on the same side of the street.</p> |
| | <p>DS7.6 – Bicycle parking spaces are provided in accordance with the following rate:</p> <ul style="list-style-type: none"> Employee parking spaces - 1 per dwelling Visitor parking spaces – 1 per 4 dwellings Motorcycle parking spaces – 1 per 10 dwellings |
| | <p>DS7.7 – Engineering design of all parking and manoeuvring areas is in accordance with Councils adopted standards.</p> |

| ELEMENT | DESIGN STANDARDS |
|-----------------------------------|--|
| Parking and access (Cont.) | <p>DS7.8 – Any car parking area or other associated structures are integrated into the design of the development such that:</p> <p>a. They are screened from view from frontages to streets, parks and adjoining land; and</p> <p>b. They are not located between the building and the street unless contemplated by the Plan of Development.</p> |
| Environment | <p>DS8.1 – Development achieves the noise generation levels set out in the Environmental Protection (Noise) Policy 2008, as amended.</p> |
| | <p>DS8.2 – Development achieves the air quality objectives set out in the Environmental Protection (Air) Policy 2008, as amended.</p> |
| | <p>DS8.3 – Light emanating from any source complies with Australian Standard AS4282 Control of the Obtrusive Effects of Outdoor Lighting, as amended.</p> |
| | <p>DS8.4 – Outdoor lighting is provided in accordance with Australian Standard AS 1158.1.1 –Road Lighting – Vehicular Traffic (Category V) Lighting – Performance and Installation Design Requirements, as amended.</p> <p>Where car park lighting is proposed, this is provided in accordance with Australian Standard AS1158.3.</p> |
| | <p>DS8.5 – Any reflective glass material has:</p> <p>a. A level of light reflectivity of not greater than 20%; and</p> <p>b. A level of heat transmission of not less than 20%.</p> |
| | <p>DS8.6 – Development does not include the storage of dangerous goods as defined by the Work Health and Safety Act 2011, as amended.</p> |
| | <p>DS8.7 - All rainwater tanks are designed to ensure 75% roof capture and reuse, with collected water used for all suitable uses within the property.</p> |
| Infrastructure | <p>DS8.10 – All onsite landscaping adjacent to accessway and car parking area to allow for passive irrigation. Site specific stormwater quality treatment is not required.</p> |
| | <p>DS9.1 - All development is connected to reticulated water supply, sewerage, stormwater drainage and telecommunication infrastructure and has an electricity supply.</p> |

DS9.2 Include electric vehicle readiness in the development as follows:

- Provision of electrical capacity for Basic (slow) EVSE chargers for 100% of all parking bays, including visitor spaces.
- Provision of conduits, cable trays and/or wiring from car park distribution boards to individual parking spaces to all unit parking bays (during construction) to enable future Basic (slow) EVSE installation.
- Electric vehicle charging shall be capable of electrical load control to manage the development's electricity demand profile in the context of the network supply profile and designed with regard to fire retardance and ventilation.

The requirements under DS9.2 are not applicable for Multiple Residential lots used exclusively for social / community housing.

DS8.8 - Tanks must be installed in accordance with all Council, State Government, Federal Government and industry plumbing standards for rainwater tanks

DS8.9 - tanks must be connected to and supply water to all for the following:

- Toilets (all toilets)
- Urinals (all urinals)
- Laundry (all cold taps in laundry)
- Outdoor taps (all out door taps).

LEGEND

Application Boundary

LAND USES

Child Care Centre

Medium Density

Neighbourhood Sports Park

Neighbourhood Recreation Park

Local Recreation Park

Local Linear Park

BUILDING HEIGHTS

③ Maximum Number of Storeys

INTERNAL LAND USE OUTCOMES & INTERFACES

Indicative Built Form Footprint

Indicative Carparking and Servicing Areas

Primary Access Location

Key Corner Site

Indicative Landscape Screening

Desired Building Frontage / Setback with Building Articulation and Screen Planting

STREETSCAPE

3.0m Wide Contraflow / Dedicated Cycle Path

3.0m Wide Shared Path

2.0m Wide Off-Road Pedestrian Path

1.8m Wide Off-Road Pedestrian Path

1.5m Wide Off-Road Pedestrian Path

ROAD NETWORK HIERARCHY

Signalised Intersection

Trunk Connector C2 - 25.0m

Trunk Connector G - 25.9m

Neighbourhood Connector N1 - 25.0m

Neighbourhood Connector L - 19.0m

Urban Access A - 19.5m

Access Street P - 15.5m

Access Street Esplanade - 13.5m

Laneway Link - 14.8m

Laneway - 8.0m

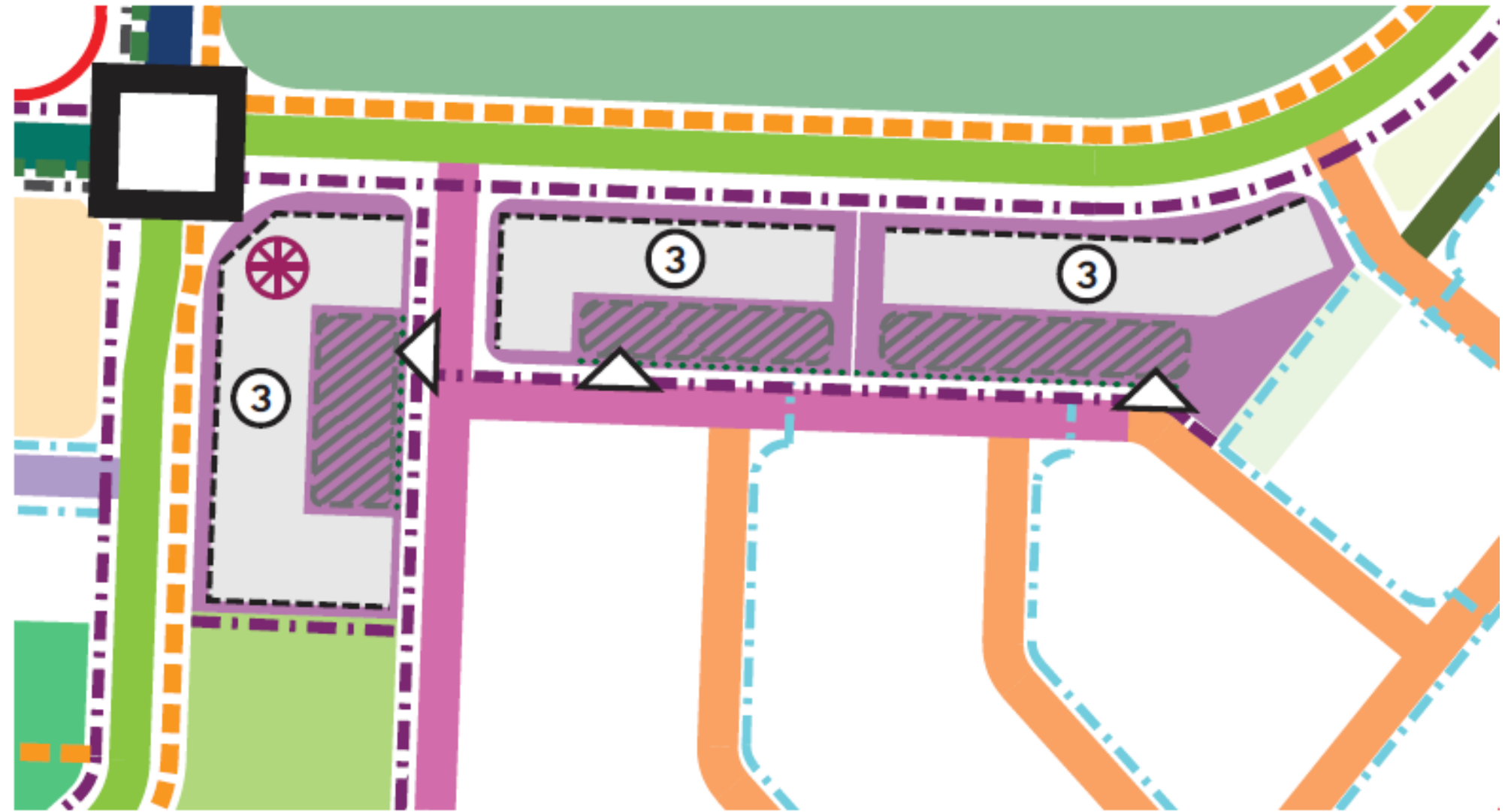


Figure 4 – Lot 8001, 8002 and 8004 - Plan of Development



3.3.3 Lot 8003, 8006 and 9000 (State Primary School, Emerging Community Lot and Neighbourhood Sports Park) – Design Standards

Application for compliance assessment for Lot 8003, 8005 and 9000 in Sub-Precinct 18.1 is required to be assessed against and fulfil all the Design Standards identified in Table 7 below.

Table 7 – Lot 8003, 8005 and 9000 (State Primary School, Emerging Community Lot and Neighbourhood Sports Park) Design Standards

| ELEMENT | DESIGN STANDARD |
|--------------|---|
| General | |
| Use | DS1.1 – Land uses comprise the uses specified for the Sub-Precinct in Table 2 (i.e. Educational Establishment). |
| | DS1.2 – Development is to be delivered generally in accordance with the Plan of Development for Lot 8003 and 8006 as shown on Figure 5. |
| Built form | DS2.1 – Building height is consistent with that identified on Figure 5. |
| | DS2.2 - Development on a ‘Key Corner Site’ in Figure 5 provides a landscape or built form statement to this site which: a. Ensure that blank walls of buildings or back of house areas are not located on external corner frontages; b. Built form is provided to this external frontage and is articulated through the use of glass, openings and recesses; and c. Building treatments are designed to respond to the primacy of the corner through a combination of architectural treatments, additional building height and responsive roof articulation. |
| Public realm | DS3.1 – Development ensures that the built form for the State Primary School and Emerging Community Site does not compromise the function of the Neighbourhood Sports Park. |
| | DS3.2 – Development ensures that ‘Pedestrian and Cycle Links’ are provided in the general location as identified in Figure 5. |
| | DS3.3 – A car park must: • Provide a soft landscaped strip of at least 2m wide along the boundary. • Provide 1 shade tree per 6 car parking spaces for open air car parks unless otherwise provided by shade structures. |
| | DS3.4 – The boundary between the Neighbourhood Sports Park and the school is to ensure the transition between the two uses ensures pedestrian accessibility, safety and amenity are prioritised. |

| ELEMENT | DESIGN STANDARD |
|--------------------|---|
| Parking and Access | DS4.1 - Development facilitates delivery of a road network hierarchy in accordance with Figure 6 to the extent relevant. |
| | DS4.2 - Where an on-site waste collection area is provided, access and manoeuvring areas must provide for the specified vehicle. |
| | DS4.3 – State Primary School drop off area and School Bus Stop Zone are indicatively shown in Figure 5. |
| | DS4.4 – Access arrangements are to be located and arranged in accordance with Figure 9. |
| | DS4.5 – Development of the school is to have due regard to the ‘Planning for Safe Transport Infrastructure at Schools’ document. |
| Environment | DS5.1 – For School / Community / Sporting Oval lots, Gross Pollutant Management (litter and coarse sediment) is to be provided on all lots for ground level runoff. Management is to be provided in a range of forms to ensure the lot design including underground GPTs or stormwater pit basket inserts to all ground level pits. |
| | DS5.2 – Development of the school is to have due regard to the ‘Development Affected by Environmental Emissions from Transport Policy 2017’ document. |
| Rainwater tanks | DS6.1 - Rainwater tanks are to be installed to ensure minimum 50% roof capture and reuse. Tanks are to be sized as 1KL per toilet or urinal (with a minimum size of 5KL). |
| | DS6.2 - Tanks must be installed in accordance with all Council, State Government, Federal Government and industry plumbing standards for rainwater tanks |
| | DS6.3 – Tanks must be connected to and supply water to all of the following: • Toilets (all toilets) • Urinals (all urinals) • Laundry (all cold taps in laundry) • Outdoor taps (all out door taps) |

LEGEND

Application Boundary

LAND USES

State Primary School

Emerging Community

Neighbourhood Sports Park

Child Care Centre

Medium Density

Local Linear Park

Local Linear Park

Local Linear Park

Local Linear Park

Local Linear Park

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INTERNAL LAND USE OUTCOMES & INTERFACES

Indicative Bus Stop Location

Indicative School BUZ Stop Location

Indicative Built Form Footprint

Indicative Carparking and Servicing Areas

Primary Access Location

Desired Building Frontage / Setback with Building Articulation and Screen Planting

Key Corner Site

Bus Drop Off

School Bus Stop Zone

Indicative Sports Fields

Indicative Sports Fields

Indicative Sports Fields

Indicative Sports Fields

Indicative Sports Fields

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Indicative Sports Fields

ROAD NETWORK HIERARCHY

Signalised Intersection

Trunk Connector C2 - 24.6m

Trunk Connector G - 25.9m

Neighbourhood Connector N1 - 25.0m

Neighbourhood Connector N2 - 25.0m

Neighbourhood Connector L - 19.0m

Access Street P - 15.5m

Access Street Esplanade - 13.5m

Laneway - 8.0m

Shared Vehicular / Pedestrian Access (Driveway) - 12.0m

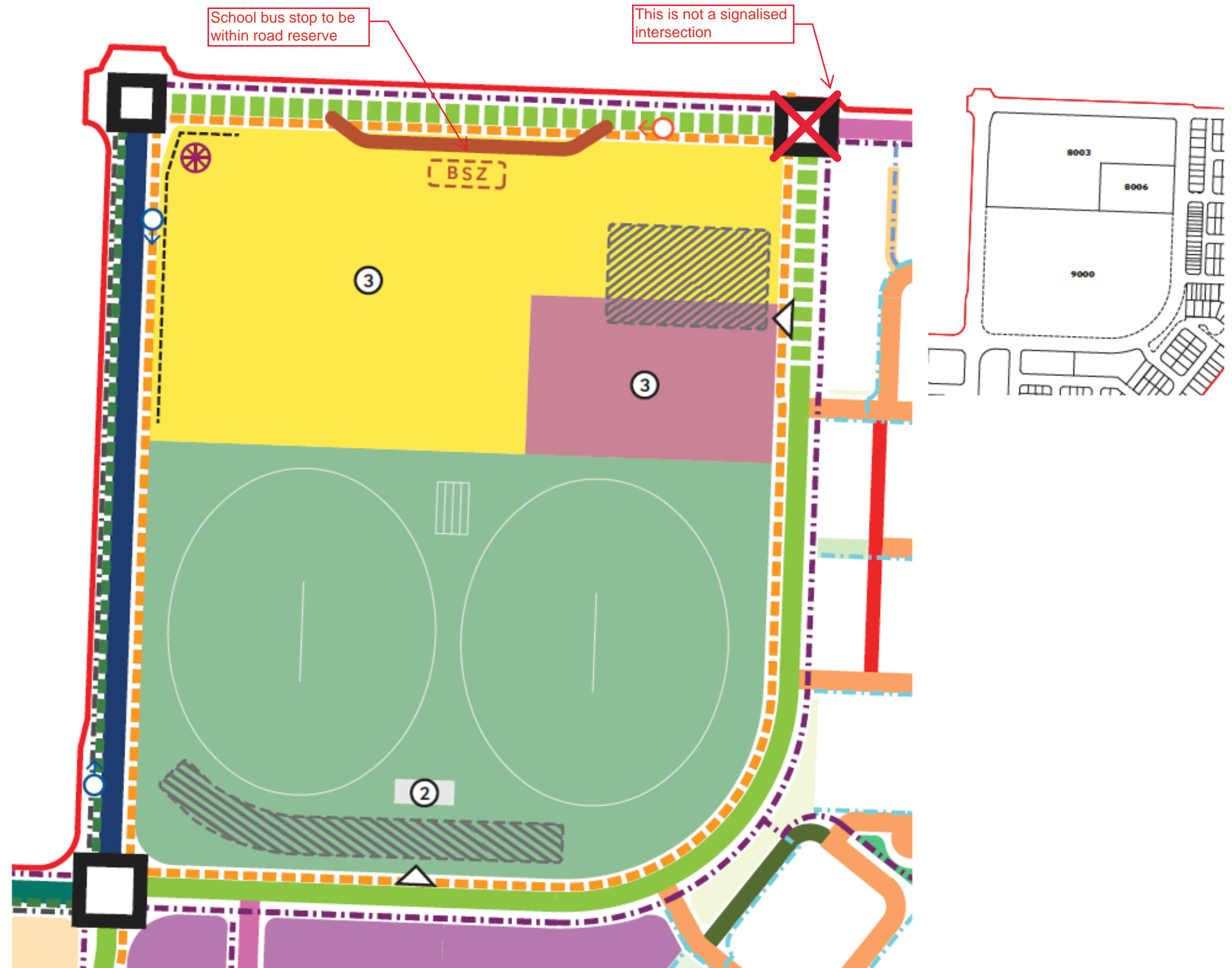
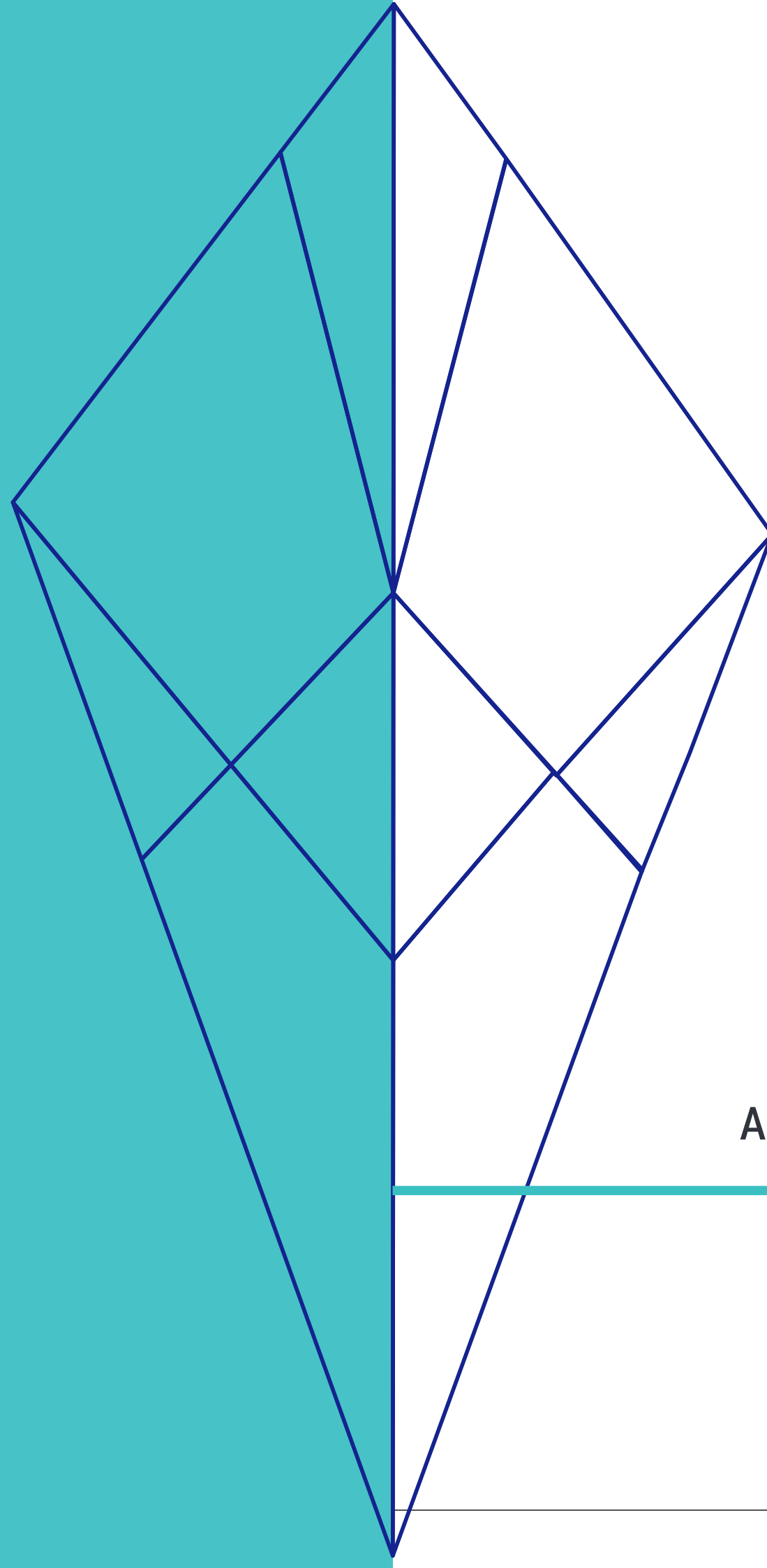


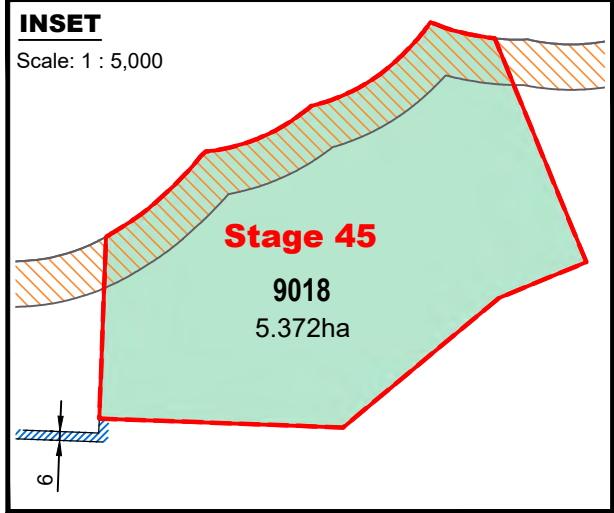
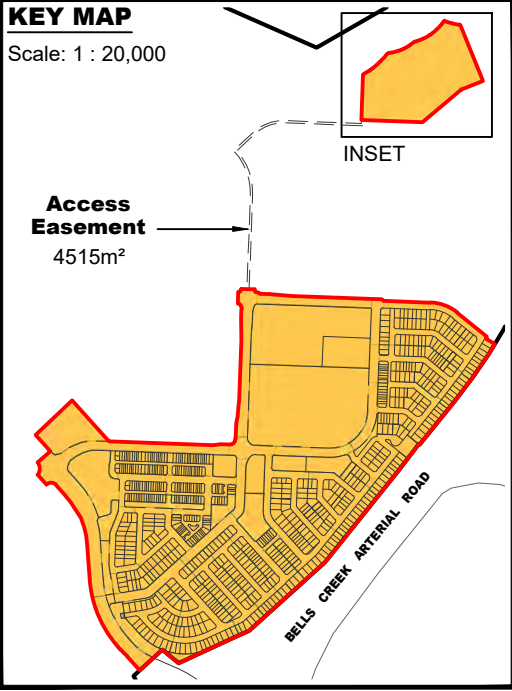
Figure 5 – Lot 8003, 8005 and 9000 – Plan of Development



APPENDICES



APPENDIX A APPROVED RECONFIGURING A LOT PLANS



0 20 40 60 80 100 1:4,000 @ A3

AURA LAKES

PLAN OF SUBDIVISION P18 OVERALL

PLAN REF: **AU12885 – 24**
Rev No: **C**
DATE: 04 JULY 2024
CLIENT: STOCKLAND
DRAWN BY: MD / JC
CHECKED BY: MD

Note:
All Lot Numbers, Dimensions and Areas are approximate only, and are subject to survey and Council approval.
Dimensions have been rounded to the nearest 0.1 metres.
Areas have been rounded down to the nearest 5m².
The boundaries shown on this plan should not be used for final detailed engineers design.

Source Information:
Site boundaries: RPS Survey.
Adjoining information: RPS Survey.
Contours: Stantec.
Environment constraints: RPS Survey.

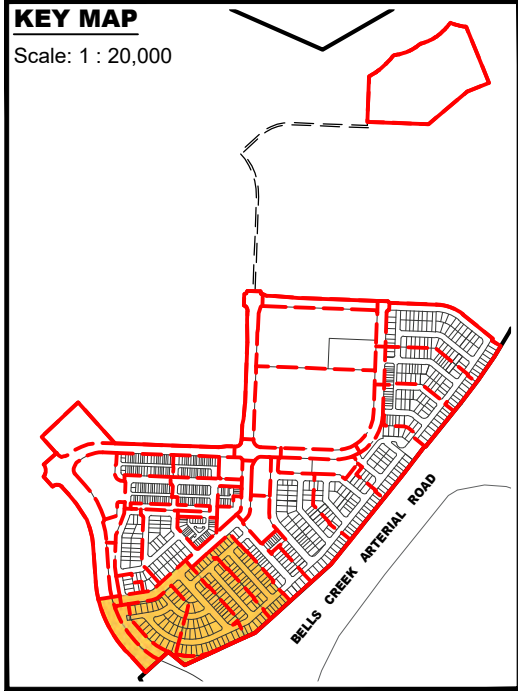
AURA

URBAN DESIGN
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W rpsgroup.com

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AURA LAKES

PLAN OF SUBDIVISION

STAGE 1 - 6

PLAN REF: **AU12885 - 25**
Rev No: **C**
DATE: 04 JULY 2024
CLIENT: STOCKLAND
DRAWN BY: MD / JC
CHECKED BY: MD

Note:
All Lot Numbers, Dimensions and Areas are approximate only, and are subject to survey and Council approval.

Dimensions have been rounded to the nearest 0.1 metres.

Areas have been rounded down to the nearest 5m².

The boundaries shown on this plan should not be used for final detailed engineers design.

Source Information:
Site boundaries: RPS Survey.
Adjoining information: RPS Survey.
Contours: Stantec.
Environment constraints: RPS Survey.



URBAN DESIGN
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Legend

- Aura Lakes Site Boundary
- P18 Stage Boundary
- Stage Boundary
- 1m Contours
- Advertising Lot

Open Space

- Local Linear Park
- Road Reserve Embellishment / Pedestrian Linkage

Residential Allotments

25m Deep

- Urban Villa - 8.5m x 25m
- Villa - 10m x 25m
- Premium Villa - 12.5m x 25m
- Courtyard - 14m x 25m
- Traditional - 16m x 25m

28m Deep

- Front Loaded Terrace - 7.5m x 28m
- Front Loaded Terrace - 9m+ x 28m (Ends)
- Urban Villa - 8.5m x 28m
- Villa - 10m x 28m
- Premium Villa - 12.5m x 28m
- Courtyard - 14m x 28m
- Traditional - 16m x 28m

AURA LAKES

PLAN OF SUBDIVISION
STAGE 7 and 26 - 33

PLAN REF: AU12885 - 27

Rev No: C

DATE: 04 JULY 2024

CLIENT: STOCKLAND

DRAWN BY: MD / JC

CHECKED BY: MD

Note:

All Lot Numbers, Dimensions and Areas are approximate only, and are subject to survey and Council approval.

Dimensions have been rounded to the nearest 0.1 metres.

Areas have been rounded down to the nearest 5m².

The boundaries shown on this plan should not be used for final detailed engineers design.

Source Information:

Site boundaries: RPS Survey.

Adjoining information: RPS Survey.

Contours: Stantec.

Environment constraints: RPS Survey.

Residential Allotments**Other**

Urban Warehouse - 12m x 14m

21m Deep

Laneway Terrace - 4.6m x 21m

Laneway Terrace - 6.6m x 21m

Laneway Terrace - 6.6m+ x 21m (Ends)

25m Deep

Laneway Terrace - 4.6m x 25m

Laneway Terrace - 6.6m x 25m

Laneway Terrace - 6.6m+ x 25m (Ends)

Front Loaded Terrace - 7.5m x 25m

Front Loaded Terrace - 9m+ x 25m (Ends)

Urban Villa - 8.5m x 25m

Villa - 10m x 25m

Premium Villa - 12.5m x 25m

Courtyard - 14m x 25m

28m Deep

Front Loaded Terrace - 7.5m x 28m

Front Loaded Terrace - 9m+ x 28m (Ends)

Urban Villa - 8.5m x 28m

Villa - 10m x 28m

Premium Villa - 12.5m x 28m

Courtyard - 14m x 28m

Traditional - 16m x 28m

30m Deep

Villa - 10m x 30m

Premium Villa - 12.5m x 30m

Courtyard - 14m x 30m

Traditional - 16m x 30m

Legend

P18 Stage Boundary

Stage Boundary

1m Contours

Super Allotment

Medium Density

Open Space

Local Linear Park

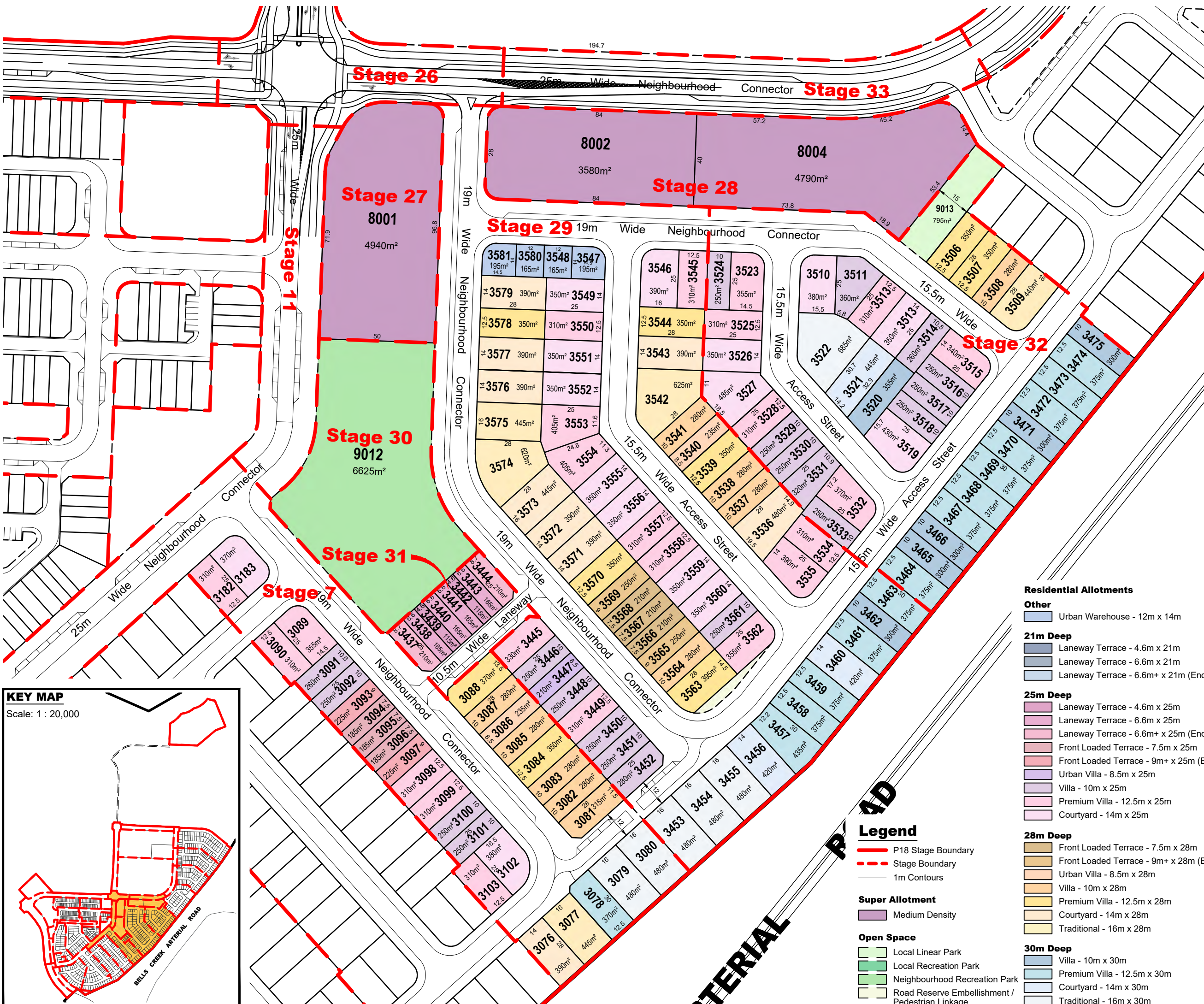
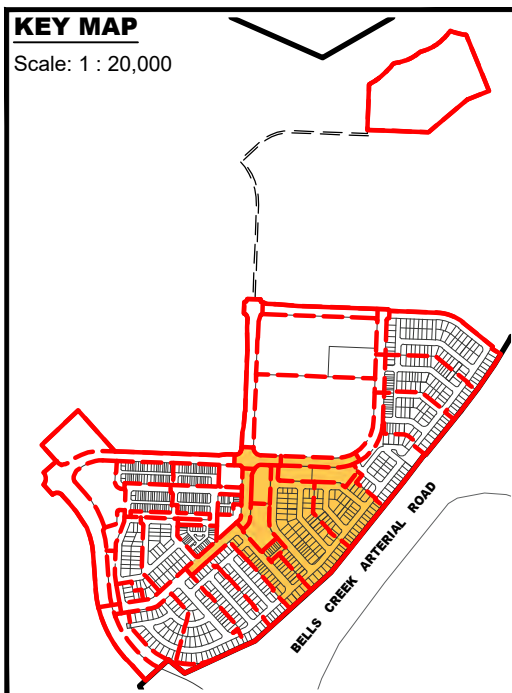
Local Recreation Park

Neighbourhood Recreation Park

Road Reserve Embellishment / Pedestrian Linkage

KEY MAP

Scale: 1 : 20,000

 Stockland
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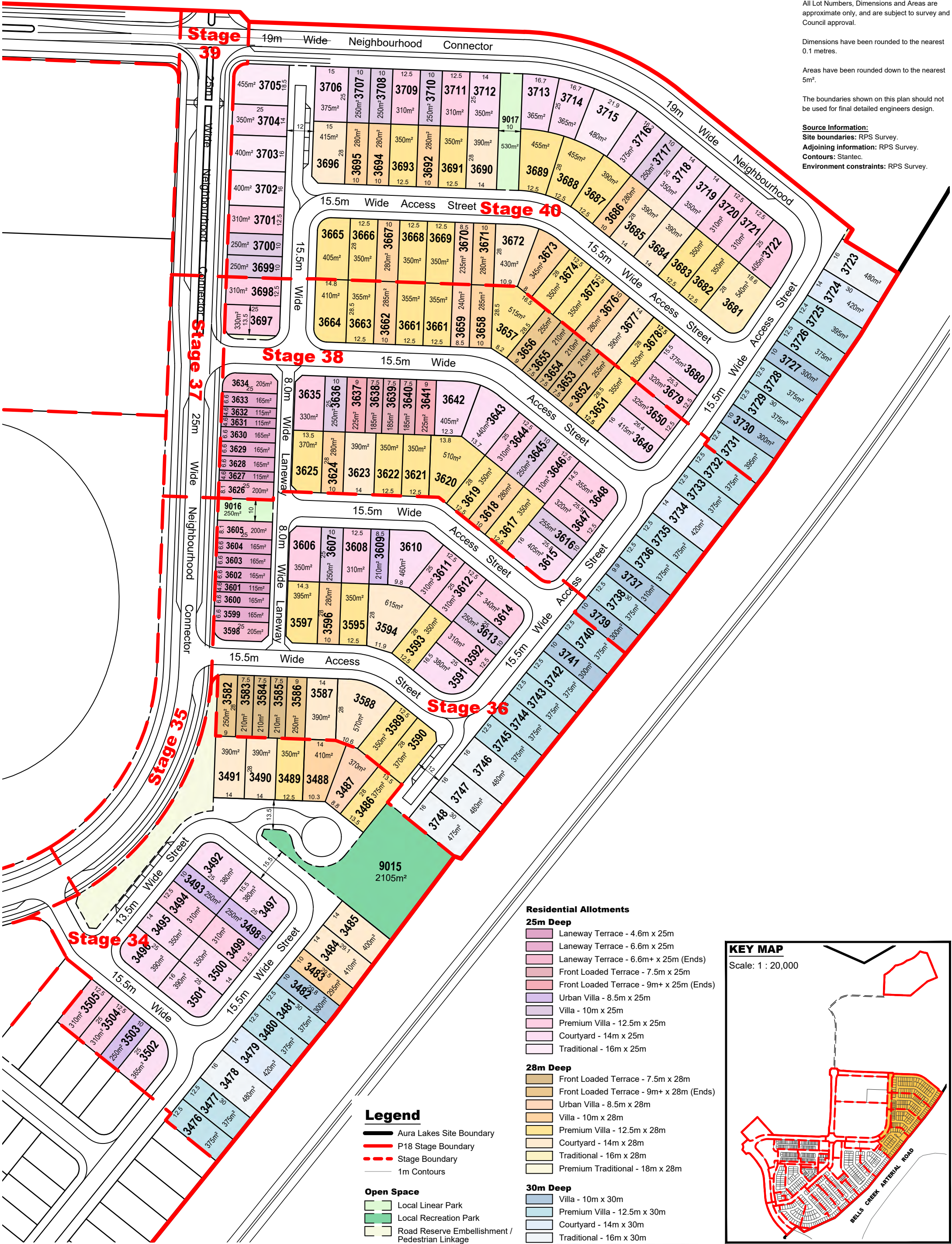
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Areas have been rounded down to the nearest 5m².

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Source Information:
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Adjoining information: RPS Survey.
Contours: Stantec.
Environment constraints: RPS Survey.



PLAN REF: **AU12885 - 28**
Rev No: **C**
DATE: 04 JULY 2024
CLIENT: STOCKLAND
DRAWN BY: MD / JC
CHECKED BY: MD



AURA LAKES
PLAN OF SUBDIVISION
STAGE 34 - 40



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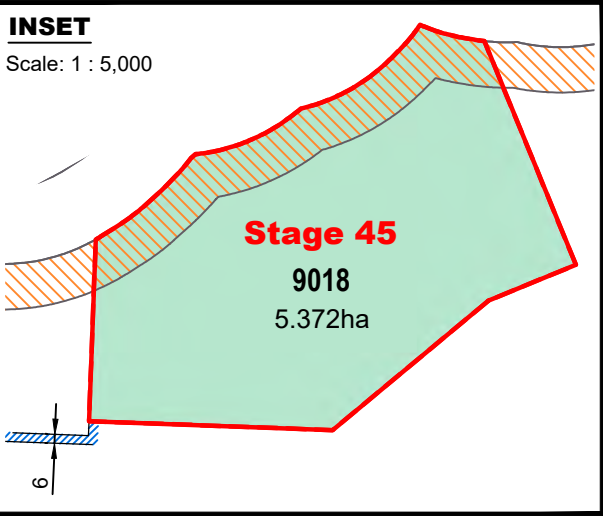
0 20 40 60 80 100 1 : 1,500 @ A3

Legend

- P18 Stage Boundary
- Stage Boundary
- 1m Contours
- Super Allotment**
 - State Primary School
 - Emerging Community
- Open Space**
 - Neighbourhood Sports Park

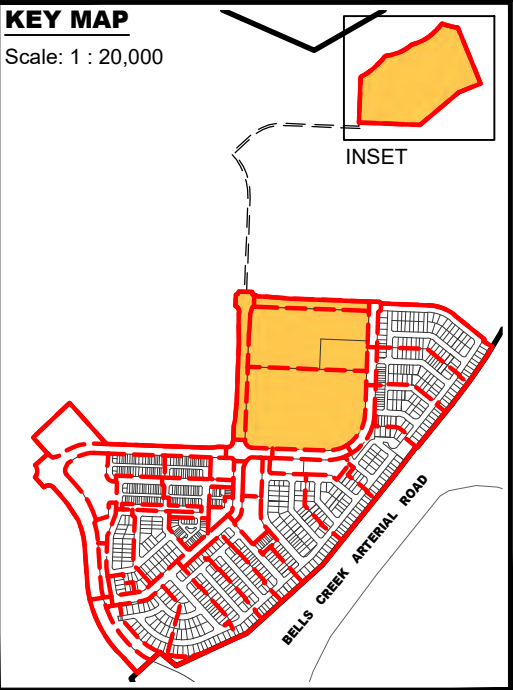
INSET

Scale: 1 : 5,000



KEY MAP

Scale: 1 : 20,000



Proposed Access Easement

4515m²

Stage 41

25m Wide Neighbourhood Connector

Stage 43

8003

4.0ha

8006

1.0ha

Stage 44

9000

6.5ha

Note:

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Areas have been rounded down to the nearest 5m².

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Adjoining information: RPS Survey.

Contours: Stantec.

Environment constraints: RPS Survey.

PLAN REF: AU12885 – 29

Rev No: C

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PLAN OF SUBDIVISION
STAGE 41 - 45

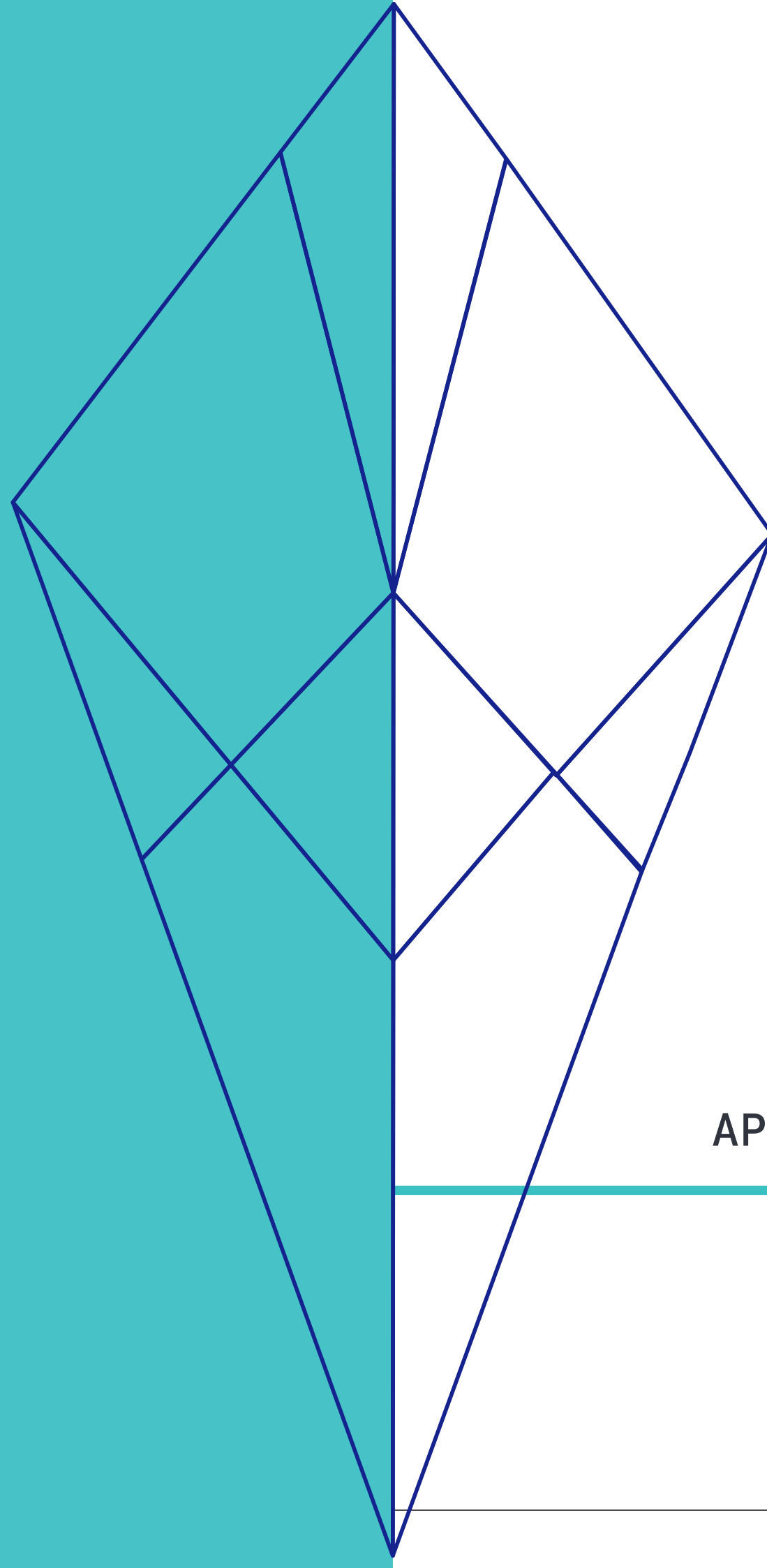


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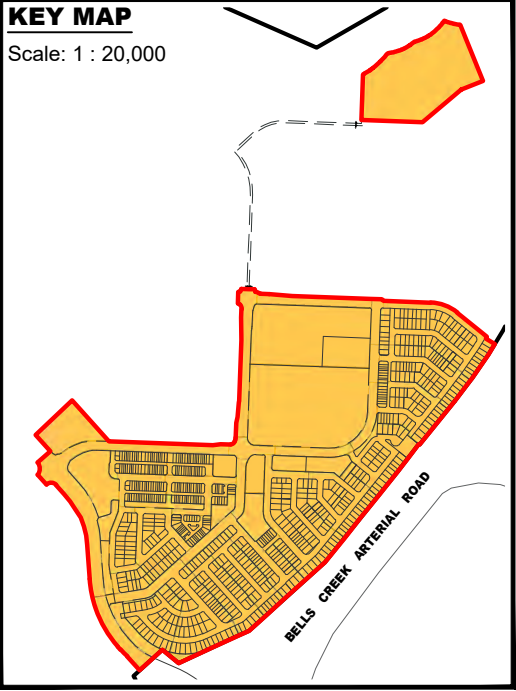


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APPENDIX B APPROVED PLAN OF DEVELOPMENT PLANS



Parking Breakdown

| | |
|---|------|
| Total On-Street Visitor Parking Spaces Provided | 594 |
| Total Residential Dwellings | 749 |
| Total On-Street Visitor Parking Spaces Required | 562 |
| (based on 0.75 per dwelling) | |
| Parking Ratio Achieved | 0.79 |

0 20 40 60 80 100 1 : 4,000 @ A3

AURA LAKES

PLAN OF DEVELOPMENT

P18 OVERALL

PLAN REF: **AU12885 – 30**
Rev No: **C**
DATE: 04 JULY 2024
CLIENT: STOCKLAND
DRAWN BY: MD / JC
CHECKED BY: MD

Note:
On lots marked on plan as potentially impacted by road traffic noise:

- Dwellings must be designed such that internal noise levels comply with AS/ NZS2107-2016 'Acoustics - Recommended design sound level and reverberation times for building interiors' or MP4.4 as relevant; and
- Dwellings must be designed as recommended in the 'Traffic Noise Impact Assessment' document by Trinity Consultants (latest approved report version).

Note:
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Dimensions have been rounded to the nearest 0.1 metres.

Areas have been rounded down to the nearest 5m².

The boundaries shown on this plan should not be used for final detailed engineers design.

Source Information:
Site boundaries: RPS Survey.
Adjoining information: RPS Survey.
Contours: Stantec.
Environment constraints: RPS Survey.



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Legend

- Aura Lakes Site Boundary
- P18 Stage Boundary
- Stage Boundary
- 1m Contours
- Mandatory Built to Boundary Wall
- Optional Built to Boundary Wall
- Mandatory 2-storey
- Preferred Driveway Location
- Bins Pads
- Multiple Dwelling Allotment (Duplex Lot)
- Lots Potentially Impacted By Road Traffic Noise
- Alternate Location for Private Open Space (For Lots Adjoining BCA Acoustic Fence)
- On-Street Carparking

Open Space

- Drainage
- Local Linear Park
- Local Recreation Park
- Neighbourhood Recreation Park
- Neighbourhood Sports Park

Residential Allotments

- Other**
- Urban Warehouse - 12m x 14m
 - Urban Loft Type A - 8.2m x 12m
 - Urban Loft Type C - 8.5m x 12m

16.5m Deep

- Laneway Terrace - 6.6m x 16.5m
- Laneway Terrace - 6.6m+ x 16.5m (Ends)

21m Deep

- Laneway Terrace - 4.6m x 21m
- Laneway Terrace - 6.6m x 21m
- Laneway Terrace - 6.6m+ x 21m (Ends)

25m Deep

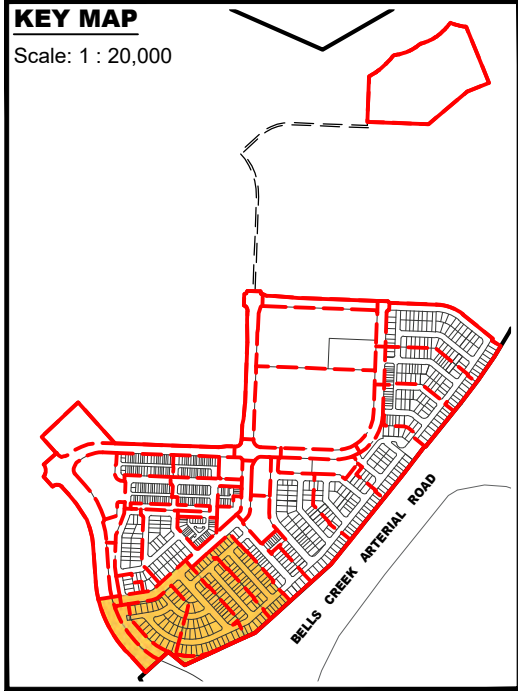
- Laneway Terrace - 4.6m x 25m
- Laneway Terrace - 6.6m x 25m
- Laneway Terrace - 6.6m+ x 25m (Ends)
- Front Loaded Terrace - 7.5m x 25m
- Front Loaded Terrace - 9m+ x 25m (Ends)
- Urban Villa - 8.5m x 25m
- Villa - 10m x 25m
- Premium Villa - 12.5m x 25m
- Courtyard - 14m x 25m
- Traditional - 16m x 25m

28m Deep

- Front Loaded Terrace - 7.5m x 28m
- Front Loaded Terrace - 9m+ x 28m (Ends)
- Urban Villa - 8.5m x 28m
- Villa - 10m x 28m
- Premium Villa - 12.5m x 28m
- Courtyard - 14m x 28m
- Traditional - 16m x 28m

30m Deep

- Villa - 10m x 30m
- Premium Villa - 12.5m x 30m
- Courtyard - 14m x 30m
- Traditional - 16m x 30m



0 10 20 30 40 50 1:1,500 @ A3

AURA LAKES

PLAN OF DEVELOPMENT

STAGE 1 - 6

PLAN REF: **AU12885 – 31**
Rev No: **C**
DATE: 04 JULY 2024
CLIENT: STOCKLAND
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CHECKED BY: MD

Note:
On lots marked on plan as potentially impacted by road traffic noise:
• Dwellings must be designed such that internal noise levels comply with AS/ NZS2107-2016 'Acoustics - Recommended design sound level and reverberation times for building interiors' or MP4.4 as relevant; and
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Dimensions have been rounded to the nearest 0.1 metres.

Areas have been rounded down to the nearest 5m².

The boundaries shown on this plan should not be used for final detailed engineers design.

Source Information:
Site boundaries: RPS Survey.
Adjoining information: RPS Survey.
Contours: Stantec.
Environment constraints: RPS Survey.



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| Residential Allotments | |
|------------------------|---|
| 25m Deep | |
| | Urban Villa - 8.5m x 25m |
| | Villa - 10m x 25m |
| | Premium Villa - 12.5m x 25m |
| | Courtyard - 14m x 25m |
| | Traditional - 16m x 25m |
| 28m Deep | |
| | Front Loaded Terrace - 7.5m x 28m |
| | Front Loaded Terrace - 9m+ x 28m (Ends) |
| | Urban Villa - 8.5m x 28m |
| | Villa - 10m x 28m |
| | Premium Villa - 12.5m x 28m |
| | Courtyard - 14m x 28m |
| | Traditional - 16m x 28m |

- Legend**
- Aura Lakes Site Boundary
 - P18 Stage Boundary
 - Stage Boundary
 - 1m Contours
- Open Space**
- Local Linear Park
 - Road Reserve Embellishment / Pedestrian Linkage
- Development Controls**
- Mandatory Built to Boundary Wall
 - Optional Built to Boundary Wall
 - Preferred Driveway Location
 - Bins Pads
 - Multiple Dwelling Allotment (Duplex Lot)
 - Lots Potentially Impacted By Road Traffic Noise
 - Alternate Location for Private Open Space (For Lots Adjoining BCA Acoustic Fence)
 - On-Street Carparking

Parking Breakdown

| | |
|---|------|
| Total On-Street Visitor Parking Spaces Provided | 116 |
| Total Residential Dwellings | 154 |
| Total On-Street Visitor Parking Spaces Required | 116 |
| (based on 0.75 per dwelling) | |
| Parking Ratio Achieved | 0.75 |



0 10 20 30 40 50 1:1,500 @ A3

AURA LAKES

PLAN OF DEVELOPMENT

STAGE 8 - 10, 12 - 17 and 19 - 25

PLAN REF: AU12885 – 32
Rev No: C
DATE: 04 JULY 2024
CLIENT: STOCKLAND
DRAWN BY: MD / JC
CHECKED BY: MD

Note:
On lots marked on plan as potentially impacted by road traffic noise:
• Dwellings must be designed such that internal noise levels comply with AS/ NZS2107-2016 'Acoustics - Recommended design sound level and reverberation times for building interiors' or MP4.4 as relevant; and
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Areas have been rounded down to the nearest 5m².

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Adjoining information: RPS Survey.
Contours: Stantec.
Environment constraints: RPS Survey.



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- Residential Allotments**
- Other**
- Urban Loft Type A - 8.2m x 12m
 - Urban Loft Type C - 8.5m x 12m
- 16.5m Deep**
- Laneway Terrace - 6.6m x 16.5m
 - Laneway Terrace - 6.6m+ x 16.5m (Ends)
- 21m Deep**
- Laneway Terrace - 4.6m x 21m
 - Laneway Terrace - 6.6m x 21m
 - Laneway Terrace - 6.6m+ x 21m (Ends)
- 25m Deep**
- Laneway Terrace - 4.6m x 25m
 - Laneway Terrace - 6.6m x 25m
 - Laneway Terrace - 6.6m+ x 25m (Ends)
 - Villa - 10m x 25m
 - Premium Villa - 12.5m x 25m
 - Courtyard - 14m x 25m
 - Traditional - 16m x 25m
- 28m Deep**
- Villa - 10m x 28m
 - Premium Villa - 12.5m x 28m
 - Courtyard - 14m x 28m
 - Traditional - 16m x 28m
- 30m Deep**
- Villa - 10m x 30m
 - Premium Villa - 12.5m x 30m
 - Courtyard - 14m x 30m

- Legend**
- P18 Stage Boundary
 - Stage Boundary
 - 1m Contours
- Development Controls**
- Mandatory Built to Boundary Wall
 - Optional Built to Boundary Wall
 - Mandatory 2-storey
 - Preferred Driveway Location
 - Bins Pads
 - Lots Potentially Impacted By Road Traffic Noise
 - Alternate Location for Private Open Space (For Lots Adjoining BCA Acoustic Fence)
 - On-Street Carparking
- Open Space**
- Drainage
 - Local Linear Park
 - Local Recreation Park
 - Road Reserve Embellishment / Pedestrian Linkage

Parking Breakdown

| | |
|--|------|
| Total On-Street Visitor Parking Spaces Provided | 212 |
| Total Residential Dwellings | 253 |
| Total On-Street Visitor Parking Spaces Required (based on 0.75 per dwelling) | 190 |
| Parking Ratio Achieved | 0.84 |

AURA LAKES

PLAN OF DEVELOPMENT

STAGE 7 and 26 - 33

PLAN REF: **AU12885 – 33**
 Rev No: **C**
 DATE: 04 JULY 2024
 CLIENT: STOCKLAND
 DRAWN BY: MD / JC
 CHECKED BY: MD

Note:
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- Dwellings must be designed such that internal noise levels comply with AS/ NZS2107-2016 'Acoustics - Recommended design sound level and reverberation times for building interiors' or MP4.4 as relevant; and
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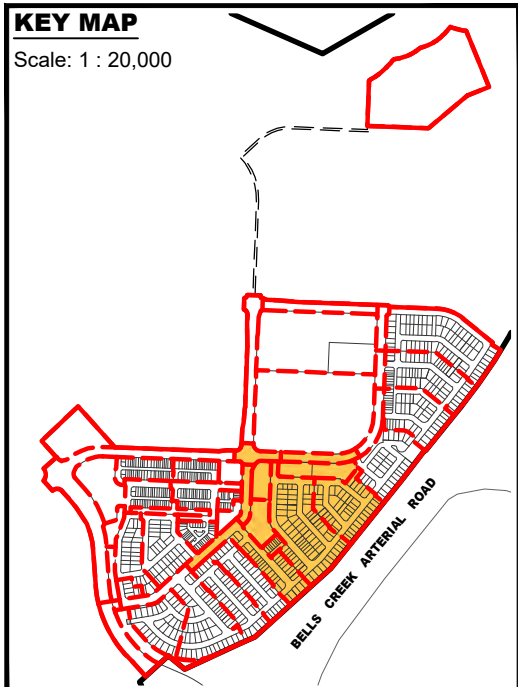
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 Contours: Stantec.
 Environment constraints: RPS Survey.



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KEY MAP
 Scale: 1 : 20,000

Parking Breakdown

| | |
|---|------|
| Total On-Street Visitor Parking Spaces Provided | 112 |
| Total Residential Dwellings | 145 |
| Total On-Street Visitor Parking Spaces Required | 109 |
| (based on 0.75 per dwelling) | |
| Parking Ratio Achieved | 0.77 |

Legend

- P18 Stage Boundary
- Stage Boundary
- 1m Contours

Open Space

- Local Linear Park
- Local Recreation Park
- Neighbourhood Recreation Park
- Road Reserve Embellishment / Pedestrian Linkage

Development Controls

- Mandatory Built to Boundary Wall
- Optional Built to Boundary Wall
- Mandatory 2-storey
- Preferred Driveway Location
- Bins Pads
- Multiple Dwelling Allotment (Duplex Lot)
- Lots Potentially Impacted By Road Traffic Noise
- Alternate Location for Private Open Space (For Lots Adjoining BCA Acoustic Fence)
- On-Street Carparking

Residential Allotments

- Other
- Urban Warehouse - 12m x 14m

25m Deep

- Laneway Terrace - 4.6m x 25m
- Laneway Terrace - 6.6m x 25m
- Laneway Terrace - 6.6m+ x 25m (Ends)
- Front Loaded Terrace - 7.5m x 25m
- Front Loaded Terrace - 9m+ x 25m (Ends)
- Urban Villa - 8.5m x 25m
- Villa - 10m x 25m
- Premium Villa - 12.5m x 25m
- Courtyard - 14m x 25m

28m Deep

- Front Loaded Terrace - 7.5m x 28m
- Front Loaded Terrace - 9m+ x 28m (Ends)
- Urban Villa - 8.5m x 28m
- Villa - 10m x 28m
- Premium Villa - 12.5m x 28m
- Courtyard - 14m x 28m
- Traditional - 16m x 28m

30m Deep

- Villa - 10m x 30m
- Premium Villa - 12.5m x 30m
- Courtyard - 14m x 30m
- Traditional - 16m x 30m

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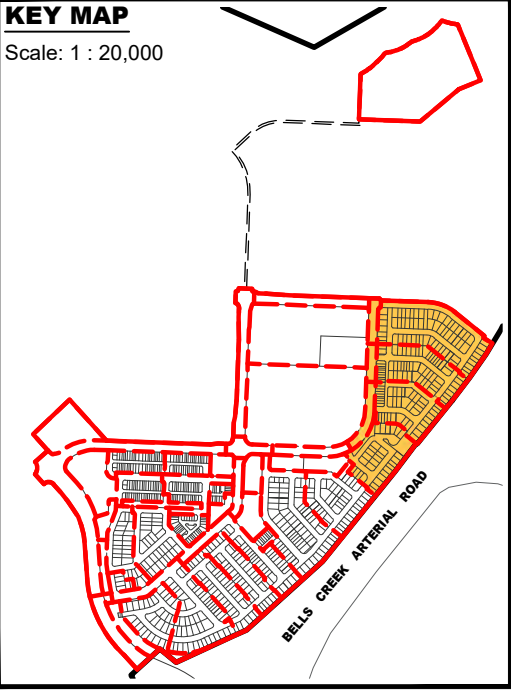
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Adjoining information: RPS Survey.
Contours: Stantec.
Environment constraints: RPS Survey.

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- Dwellings must be designed as recommended in the 'Traffic Noise Impact Assessment; document by Trinity Consultants (latest approved report version).

| Parking Breakdown | |
|---|------|
| Total On-Street Visitor Parking Spaces Provided | 154 |
| Total Residential Dwellings | 197 |
| Total On-Street Visitor Parking Spaces Required | 148 |
| (based on 0.75 per dwelling) | |
| Parking Ratio Achieved | 0.78 |



PLAN REF: AU12885 - 34
Rev No: C
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DRAWN BY: MD / JC
CHECKED BY: MD



AURA LAKES PLAN OF SUBDIVISION STAGE 34 - 40



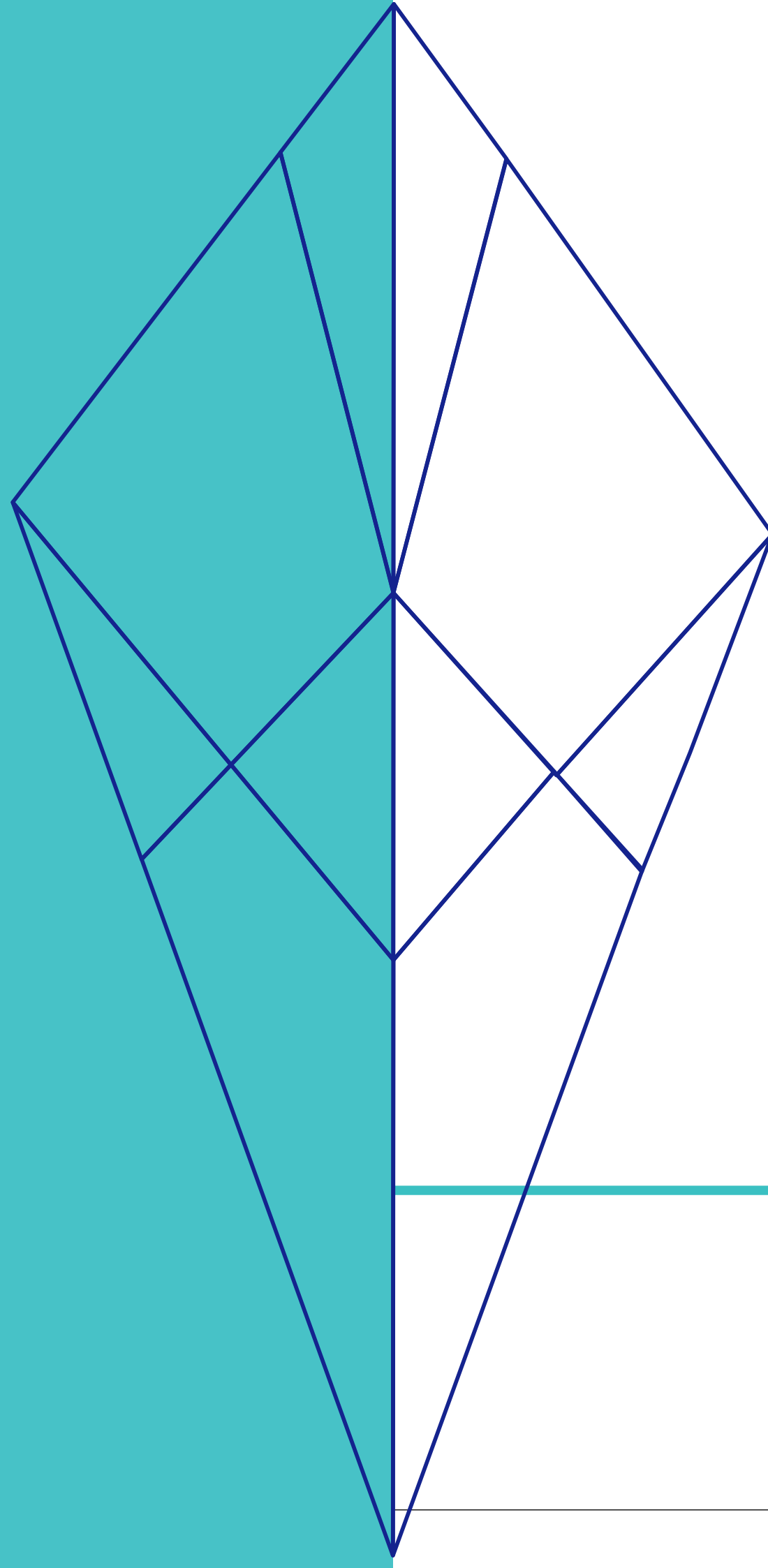
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0 20 40 60 80 100 1 : 1,500 @ A3

| Yield Breakdown | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
|---|---------|---------|---------|---------|---------|---------|---------|---------|---------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|----------|---------|-------|
| | Stage 1 | Stage 2 | Stage 3 | Stage 4 | Stage 5 | Stage 6 | Stage 7 | Stage 8 | Stage 9 | Stage 10 | Stage 11 | Stage 12 | Stage 13 | Stage 14 | Stage 15 | Stage 16 | Stage 17 | Stage 18 | Stage 19 | Stage 20 | Stage 21 | Stage 22 | Stage 23 | Stage 24 | Stage 25 | Stage 26 | Stage 27 | Stage 28 | Stage 29 | Stage 30 | Stage 31 | Stage 32 | Stage 33 | Stage 34 | Stage 35 | Stage 36 | Stage 37 | Stage 38 | Stage 39 | Stage 40 | Stage 41 | Stage 42 | Stage 43 | Stage 44 | Stage 45 | Overall | |
| Lot Type | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | Yield | % | | |
| Other | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Urban Warehouse - 12m x 14m | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 4 | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 4 | 0.5% | |
| Urban Loft Type A - 8.2m x 12m | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 1 | — | — | 2 | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 3 | 0.4% | |
| Urban Loft Type C - 8.5m x 12m | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 1 | — | — | 2 | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 3 | 0.4% | |
| Sub-total | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 2 | — | — | 4 | — | — | — | — | — | — | — | — | 4 | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 10 | 1.3% | |
| 16.5m Deep | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Laneway Terrace - 6.6m x 16.5m | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 4 | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 4 | 0.5% |
| Laneway Terrace - 6.6m+ x 16.5m (Ends) | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 2 | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 2 | 0.3% |
| Sub-total | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 6 | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 6 | 0.8% |
| 21m Deep | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Laneway Terrace - 4.6m x 21m | — | — | — | — | — | — | — | — | — | — | — | 8 | 3 | — | — | 6 | 10 | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 27 | 3.6% |
| Laneway Terrace - 6.6m x 21m | — | — | — | — | — | — | — | — | — | — | — | 5 | 3 | — | — | 7 | 7 | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 22 | 2.9% |
| Laneway Terrace - 6.6m+ x 21m (Ends) | — | — | — | — | — | — | — | — | — | — | — | 2 | 4 | — | — | 2 | 2 | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 10 | 1.3% |
| Sub-total | — | — | — | — | — | — | — | — | — | — | — | 15 | 10 | — | — | 15 | 19 | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 59 | 7.9% |
| 25m Deep | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Laneway Terrace - 4.6m x 25m | — | — | — | — | — | — | — | — | — | — | — | 7 | 2 | — | — | 4 | 8 | — | 19 | 21 | — | — | — | — | — | — | — | — | — | — | 2 | — | — | — | — | — | 1 | — | 3 | — | — | — | — | — | — | 67 | 8.9% |
| Laneway Terrace - 6.6m x 25m | — | — | — | — | — | — | — | — | — | — | — | 6 | — | — | — | 3 | 8 | — | 9 | 13 | — | — | — | — | — | — | — | — | — | — | 4 | — | — | — | — | 5 | — | 4 | — | — | — | — | — | — | 52 | 6.9% | |
| Laneway Terrace - 6.6m+ x 25m (Ends) | — | — | — | — | — | — | — | — | — | — | — | 2 | 2 | — | — | 2 | 1 | — | 4 | 6 | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 23 | 3.1% | |
| Front Loaded Terrace - 7.5m x 25m | — | — | — | — | — | — | 3 | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 3 | — | — | — | — | — | — | — | 6 | 0.8% |
| Front Loaded Terrace - 9m+ x 25m (Ends) | — | — | — | — | — | — | 2 | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 4 | 0.5% |
| Urban Villa - 8.5m x 25m | — | — | — | — | 2 | 2 | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 1 | — | — | — | — | 3 | — | 1 | — | — | — | — | — | — | — | — | — | 9 | 1.2% |
| Villa - 10m x 25m | — | 2 | 1 | — | 4 | 4 | 4 | 2 | 5 | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 6 | — | — | 10 | — | — | 2 | — | 3 | — | 6 | — | — | — | — | — | 49 | 6.5% | |
| Premium Villa - 12.5m x 25m | — | 4 | 4 | — | 10 | 13 | 6 | 4 | 9 | 4 | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 11 | — | — | 6 | — | 4 | — | 6 | — | 8 | — | 7 | — | — | — | — | — | 96 | 12.8% |
| Courtyard - 14m x 25m | — | 8 | 3 | — | 5 | 4 | 2 | 2 | 2 | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 8 | — | — | 5 | — | 7 | — | 2 | — | 4 | — | 9 | — | — | — | — | — | 61 | 8.1% |
| Traditional - 16m x 25m | — | 1 | 3 | — | — | — | — | 1 | 2 | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 4 | — | — | — | — | — | — | 11 | 1.5% |
| Sub-total | — | 15 | 11 | — | 21 | 23 | 17 | 9 | 18 | 4 | — | 15 | 4 | — | — | 9 | 17 | — | 32 | 40 | — | — | — | — | — | — | — | — | 26 | — | 8 | 21 | — | 14 | — | 19 | — | 29 | — | 26 | — | — | — | — | 378 | 50.5% | |
| 28m Deep | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | | |
| Front Loaded Terrace - 7.5m x 28m | — | 3 | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 3 | — | — | — | — | — | — | 3 | — | 3 | — | — | — | — | — | — | — | 12 | 1.6% |
| Front Loaded Terrace - 9m+ x 28m (Ends) | — | 2 | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 2 | — | — | — | — | 2 | — | 2 | — | — | — | — | — | — | — | — | 8 | 1.1% | |
| Urban Villa - 8.5m x 28m | — | 1 | — | — | 1 | — | 1 | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 1 | — | — | — | — | 1 | — | — | — | 1 | — | — | — | — | — | — | — | 7 | 0.9% |
| Villa - 10m x 28m | — | 7 | 2 | — | 3 | 6 | 5 | 6 | 1 | 1 | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 4 | — | — | 1 | — | 2 | — | 1 | — | 4 | — | 8 | — | — | — | — | — | 51 | 6.8% |
| Premium Villa - 12.5m x 28m | — | 14 | 6 | — | 7 | 7 | 2 | 9 | — | 1 | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 5 | — | — | 2 | — | 2 | — | 5 | — | 12 | — | 14 | — | — | — | — | — | 86 | 11.5% |
| Courtyard - 14m x 28m | — | 6 | 8 | — | 4 | 2 | 1 | 2 | 2 | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | — | 8 | — | — | — | — | 4 | — | 3 | — | 1 | | | | | | | | | |



APPENDIX C RESIDENTIAL DEVELOPMENT CONTROLS

DOCUMENT PREPARED BY BOTH RPS AND URBIS



1.1.1 HOUSE AND MULTIPLE RESIDENTIAL ALLOTMENTS (DUPLEX)

Development Controls

General

1. All development inclusive of Single and Multiple Residential Allotments (Duplex) is to be undertaken in accordance with the Development Approval.

2. Building setbacks and built-to-boundary wall locations shown are subject to future proposed easements and/or underground services.

3. Maximum building height is 2 storeys (9m) for all dwellings or 3 storeys where identified on a Fixed Elements Plan or Plan of Development. Rooftop terraces are defined as a 'storey'.

4. Noise affected allotments are to be developed in accordance with AS3671-1989 'Acoustics - Road Traffic Noise Intrusion - building siting and construction' to achieve the satisfactory internal noise levels stipulated in AS2107-2000 'Acoustics - Recommended design sound levels and reverberation times for building interiors'.

5. Where nominated as a duplex lot, the two (2) dwelling units must be attached.

Setbacks

6. Setbacks are as per the Plan of Development Table unless otherwise specified. In the case of Courtyard and Traditional Corner Lots, an additional setback from the street corner is applicable. The setback applies to any building or structure greater than 2m high. The setback is measured as the line that joins the points on the front and side street boundaries of the lot that are located 9m back from the point of intersection of these two boundaries.

7. A corner lot, for the purposes of determining setbacks, is a lot that adjoins the intersection of two streets (connector, access street or access place). This excludes those lots that abut a shared access driveway, a laneway or a pedestrian link / landscape buffer and therefore in these cases a secondary street setback does not apply.

8. Corner lots are interpreted as having two front boundaries and two side boundaries for the purposes of determining building setbacks (no rear boundary setback applies).

9. Built to Boundary walls are recommended where road frontage widths are less than 12.49m. Built to Boundary walls are optional for lots with road frontage widths equal to, or in excess of 12.5m. Where Built to Boundary walls are not adopted side setbacks shall be in accordance with the Plan of Development Table.

10. Built to Boundary walls are to have a maximum length of 15m or 50% of the length of the lot depth (whichever is greater) and a maximum height of 3.5m.

11. Boundary setbacks are measured to the wall of the structure. Eaves should not encroach closer than 450mm to the lot boundary except in the case of:

a) The Primary Street Frontage where eaves should not be closer than 1950mm (1.95m);

b) Walls that are Built to Boundary.

12. First floor setbacks must not exceed the minimum ground floor setbacks, as documented in the Plan of Development Table.

13. Roofed gatehouses and arches are permitted within the front setback (and the secondary frontage for corner lots) provided they meet the requirements of the Queensland development Code, having:

- a maximum area of 4m²; and

- not more than 2m wide elevation to the street; and

- not more than 3m in height.

Site Cover and Amenity

14. Site cover for each lot is not to exceed the percentages outlined in the Plan of Development Table.

15. Private open space, must be provided and is not less than 15m² per dwelling with a minimum dimension of 3.0m, and is directly accessible from a ground floor living area.

Privacy and Overlooking

16. Buildings must be sited and designed to provide adequate visual privacy for neighbours:

- Where the distance separating a window or balcony of an adjoining dwelling from the side or rear boundary is less than 1.5m:

a) a permanent window and a balcony has a window/balcony screen extending across the line of sight from the sill to at least 1.5m above the adjacent floor level; or

b) a window has a sill height more than 1.5m above the adjacent floor level, or

c) a window has obscure glazing below 1.5m;

Where Window and Window/Balcony Screen has the same meaning as in the Queensland Development Code.

Fencing

For detached lots:

17. Fencing along primary street frontages must be either 50% transparent or a maximum of 1.2m in height. Fencing to secondary street frontages may be screen fencing of 1.8m high maximum and extend up to the front building line (main facade). For lots identified as being subject to rear fence controls on the Plan of Development, the fence must be a minimum 1.5m high solid screen fence and may extend up to a maximum 1.8m where the upper 0.3m is 30% transparent.

18. For Multiple Residential (Duplex lots): Fencing along primary and secondary street frontages (where it adjoins private open space) must be a minimum 1.5m high solid screen fencing and may extend up to a maximum 1.8m where the upper 0.3m is 50% transparent.

Parking and Driveways

For all allotments the following applies:

19. Double garages will not be permitted on a single storey dwelling, on a lot less than 10m wide.

20. Double garages are permitted:

- On any Premium Villa, Courtyard, Premium Traditional or Multiple Residential (Duplex) Lot;

- On any 7.5m wide to less than 12.5m wide where the dwelling is more than one storey in height, and where the garage is setback at least 1m behind the main facade, excluding balconies, of the dwelling;

- On any corner lot if the garage fronts the secondary street frontage as shown in the setback diagrams for detached corner allotments and minimum garage setbacks can be met;

- On a single storey dwelling on any Villa Lots where the following design criteria are met:

a) The front facing building wall, which comprises the garage doors, may not exceed an external width of 5.7m; and

b) The garage door width must not exceed 4.8m; and

c) The garage door must have a minimum 450mm eave above it and be setback a minimum of 240mm behind the pillar of the garage door; and

d) Must have a sectional, tilt or roller door; and

e) The front facade of the dwelling must include the following:

- A front entrance door and windows with a sidelight.

- A front verandah, portico or porch located over the front entrance, which extends a minimum of 1.6m forward of the entrance door.

- The verandah, portico or porch is to include front piers with distinct materials and/or colours

f) The garage has one side constructed as a built to boundary wall in a position consistent with the Plan of Development for the lot; and

g) The driveway of the garage is to taper from the garage opening down to 3m at the lot boundary. The taper must occur on the central edge of the driveway.

- Any combination of a 'garage', 'carport' or 'open carport' as defined in the Queensland Development Code (QDC) are taken to be a 'garage' under this POD.

21. The maximum width of a driveway at the lot boundary and where crossing the verge:

- serving a double garage shall be 4.8m; and

- serving a single garage shall be 3.0m.

22. Parking spaces on driveways do not have to comply with gradients in AS2890.

23. A maximum of one driveway per dwelling is permitted (2 for Multiple Residential (duplex) allotments).

24. Driveways should avoid on-street works such as dedicated on-street parking bays, drainage pits and service pillars.

25. The minimum distance of a driveway from an intersection of one street with another street (not a laneway) shall be 6.0 metres.

26. The minimum distance between driveways on the same Multiple Residential (including duplex) lot shall be 3.0 metres at the boundary.

Rainwater Tanks

27. All dwellings (including Duplex dwellings) must have:

- Lots >300m2 = a 5kL rainwater tank

- Lots 225-300m2 = 3kL rainwater tank

- Lots <225m2 = no tank required

28. All tanks to residential dwellings must collect a minimum 50% roof area capture. All tanks must supply water to toilets, laundry and have a connection for external usage and must have a backup supply from the main potable water system. The tank must be located outside of the minimum area required for private open space.

Definitions

Site Cover

The proportion of the site covered by buildings, including roof overhangs.

PLAN OF DEVELOPMENT TABLE

| | | URBAN VILLA ALLOTMENTS | | VILLA ALLOTMENTS | | PREMIUM VILLA ALLOTMENTS | | COURTYARD AND TRADITIONAL ALLOTMENTS | | PREMIUM TRADITIONAL ALLOTMENTS | | MULTIPLE RESIDENTIAL ALLOTMENTS (DUPLEX) | |
|--|--------|---|--------------------------|--|--------------------------|---|--------------------------|--|--------------------------|--|--------------------------|--|--------------------------|
| | | 8.5m–9.99m Wide | | 10m–12.49m Wide | | 12.5m–13.99m Wide | | 14m - 17.99m Wide | | 18.0m Wide + | | | |
| | | Ground Floor | First Floor | Ground Floor | First Floor | Ground Floor | First Floor | Ground Floor | First Floor | Ground Floor | First Floor | Ground Floor | First Floor |
| Front and Rear (metres) ² | | | | | | | | | | | | | |
| Front / Primary Frontage | | 2.4 | 2.4 | 2.4 | 2.4 | 2.4 | 2.4 | 2.4 | 2.4 | 2.4 | 2.4 | | |
| Garage | Single | 5.0 | n/a | 5.0 | n/a | 5.0 | n/a | 5.0 | n/a | 5.0 | n/a | 2.4 | 2.4 |
| | Double | 5.0 | | 5.0 | | 5.0 | | 5.0 | | 5.0 | | 5.0 | 5.0 |
| Rear | | 1.5 | 3.0 | 1.5 | 3.0 | 1.5 | 3.0 | 1.5 | 3.0 | 1.5 | 3.0 | 1.0 ¹ | 1.0 ¹ |
| Rear Setback for Lots Adjoining the BCA Acoustic Fence | | 4.0 - 6.0 ^{3 4} | 4.0 - 6.0 ^{3 4} | 4.0 - 6.0 ^{3 4} | 4.0 - 6.0 ^{3 4} | 4.0 - 6.0 ^{3 4} | 4.0 - 6.0 ^{3 4} | 4.0 - 6.0 ^{3 4} | 4.0 - 6.0 ^{3 4} | 4.0 - 6.0 ^{3 4} | 4.0 - 6.0 ^{3 4} | 4.0 - 6.0 ^{3 4} | 4.0 - 6.0 ^{3 4} |
| Side (metres) ² | | | | | | | | | | | | | |
| Built to Boundary | | 0.0 | 0.9 | 0.0 | 0.9 | 0.0 | 1.0 | 0.0 | 1.0 | 0.0 | 1.5 | 0.0 | 1.0 |
| Non Built to Boundary | | 0.9 | 0.9 | 0.9 | 0.9 | 1.0 | 1.0 | 1.0 | 1.0 | 1.5 | 1.5 | 1.0 | 1.0 |
| Corner Lots — Secondary frontage | | 2.4 | 2.4 | 2.4 | 2.4 | 2.4 | 2.4 | 2.4 | 2.4 | 2.4 | 2.4 | 2.4 | 2.4 |
| Garage and On-site Car Parking | | | | | | | | | | | | | |
| On site parking requirements (minimum) | | 1 space to be covered and enclosed. Single garages (up to 3.5m in width) or tandem garages are acceptable. | | 1 space to be covered and enclosed. Single garages (up to 3.5m in width) or tandem garages are acceptable. Double garages (greater than 3.5m in width) are permitted for: - 2 storey dwellings; and - single storey dwellings provided ^{note 2} above is met. | | 1 space per dwelling to be covered and enclosed. Single, tandem or double garage acceptable. Double garages must be located on any dwelling with 4 bedrooms or greater. | | 1 space to be covered and enclosed. Single, tandem or double garage acceptable. Double garages must be located on any dwelling with 4 bedrooms or greater. | | 1 space to be covered and enclosed. Single, tandem or double garage acceptable. Double garages must be located on any dwelling with 4 bedrooms or greater. | | 1 space per dwelling to be covered and enclosed. Double garages must be located on any dwelling with 4 bedrooms or greater. | |
| | | | | | | | | | | | | | |
| Garage location | | Garages are to be located along the built to boundary wall. | | Garages are to be located along the built to boundary wall. | | Garages are to be located along the built to boundary wall. | | Garages are to be located along the built to boundary wall. | | Garages are to be located along the western or southern boundary where appropriate. | | Garages are to be located along the western or southern boundary where appropriate. | |
| Site Cover (maximum) | | 60% | | 60% | | 60% | | 60% | | 60% | | 75% | |

¹ Second side setback as no rear setback applies.

² Setbacks for buildings and structures less than 2m in height are to be in accordance with the Queensland Development Code.

³ 6.0m rear setback applies to lots 3078 – 3080, 3453 – 3485 and 3723 - 3748. 4.0m rear setback applies to lots 3061 - 3077.

⁴ Where a lot adjoins the BCA Acoustic Fence and the private open space area is proposed to be at the front or side of the property, the rear setback may be reduced down to 1.5m at ground floor and 3.0m at first floor (see diagrams below).

LEGEND

— Lot boundary

--- Maximum building envelope (ground floor)

1.0m Minimum building setback dimensions

— Mandatory built to boundary wall

.... Optional built to boundary wall

— Garage setback from front boundary

Corner setbacks

Preferred private open space location (min. dimension of 3.0m)

Alternative location for private open space

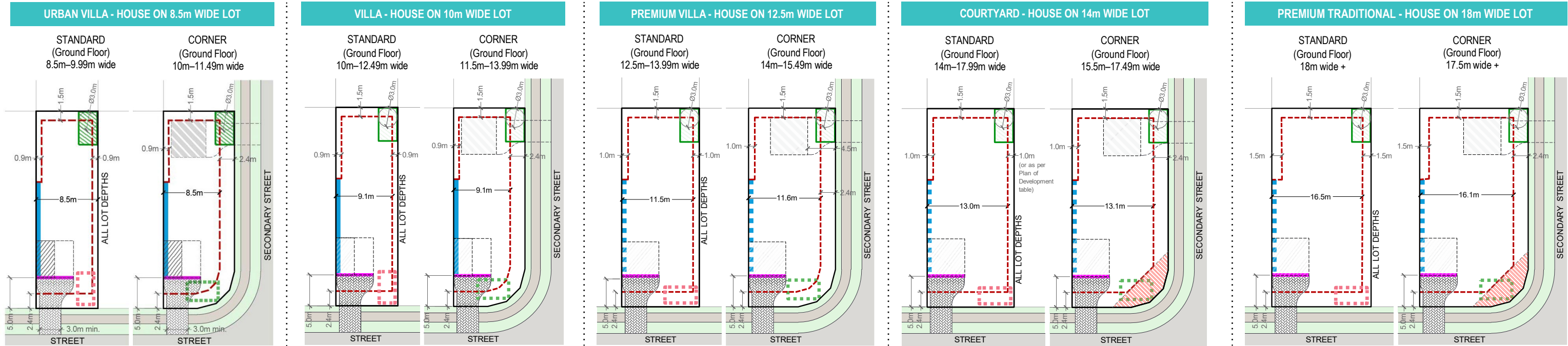
Alternative location for private open space (for lots adjoining the BCA Acoustic Fence)

Indicative garage location

Single garage for single storey dwelling, double garage for double storey dwelling

Alternative location of garage on corner lots

Indicative driveway location



1.1.2 FRONT LOADED TERRACE ALLOTMENTS - 7.5M WIDE

Development Controls

General

- All development is to be undertaken in accordance with the Development Approval.
- Building setbacks and build-to-boundary wall locations shown are subject to future proposed easements and/or underground services.
- Maximum building height is 3 storeys for all dwellings where permitted in the Plan of Development, and 2 storeys (9m) otherwise. Rooftop terraces are defined as a 'storey'.
- Noise affected allotments are to be developed in accordance with AS3671-1989 'Acoustics - Road Traffic Noise Intrusion - building siting and construction' to achieve the satisfactory internal noise levels stipulated in AS2107-2000 Acoustics - Recommended design sound levels and reverberation times for building interiors'.

Setbacks

- Setbacks are as per the Plan of Development Table unless otherwise specified.
- Where Built to Boundary walls are not adopted side setbacks for non-built to boundary walls apply.
- A corner lot, for the purposes of determining setbacks, is a lot that adjoins the intersection of two streets (collector, access street or access place). This excludes those lots that abut a shared access driveway, a laneway, or a pedestrian link landscape buffer and therefore in these cases a secondary frontage setback does not apply.
- Built to Boundary wall shall be no more than 85% of the length of the boundary and may be higher than 3.5m where in accordance with approved house plans and where appropriate building staging and construction techniques are demonstrated.
- Upper floor setbacks must not exceed the minimum ground floor setbacks, including roof top terraces.
- Boundary setbacks are measured to the wall of the structure. Eaves should not encroach closer than 450mm to the lot boundary except in the case of:
 - The Primary Street Frontage where eaves should not be closer than 1500mm; and
 - Walls that are Built to Boundary.
- Roofed gatehouses and arches are permitted within the front setback (and the secondary frontage for corner lots) provided they meet the requirements of the Queensland Development Code, having:

- a maximum area of 4m²; and
- not more than 2m wide elevation to the street; and
- not more than 3m height.

Site Cover and Amenity

- Site cover for each lot is not to exceed that shown in the Plan of Development table.
- Private open space, must be provided and is not less than 15m² with a minimum dimension of 3.0m and is directly accessible from a ground floor living area.

Privacy and Overlooking

- Buildings must be sited and designed to provide adequate visual privacy for neighbours:
 - Where the distance separating a window or balcony of an adjoining dwelling from the side or rear boundary is less than 1.5 m:
 - a permanent window and a balcony has a window/balcony screen extending across the line of sight from the sill to at least 1.5m above the adjacent floor level; or
 - a window has a sill height more than 1.5m above the adjacent floor level, or
 - a window has obscure glazing below 1.5m;
 - where: Window and Window/Balcony Screen has the same meaning as in the Queensland Development Code.

Fencing

- Fencing along the primary street frontage must be either 50% transparent or max. 1.2m high. Fencing to secondary street frontages may be screen fencing up to 1.8m high up to the front building line (main facade).

Parking and Driveways

- For all allotments the following applies:
 - Double garages will not be permitted on a single storey dwelling;
 - Double garages may be permitted where the dwelling is more than one storey in height, and where the garage is setback at least 1m behind the main facade, excluding balconies, of the dwelling;
 - The maximum width of a driveway at the lot boundary and where crossing the verge:
 - serving a double garage shall be 4.8m; and
 - serving a single garage shall be 3.0m.

- Parking spaces on driveways do not have to comply with gradients in AS2890.
- A maximum of one driveway per dwelling is permitted.
- Driveways should avoid on-street works such as dedicated on-street parking bays, drainage pits and service pillars.
- The minimum distance of a driveway from an intersection of one street with another street (not a laneway) shall be 6.0 metres.

Rainwater Tanks

- All dwellings (including Duplex dwellings) must have:
 - Lots >300m² = a 5kL rainwater tank
 - Lots 225-300m² = 3kL rainwater tank
 - Lots <225m² = no tank required

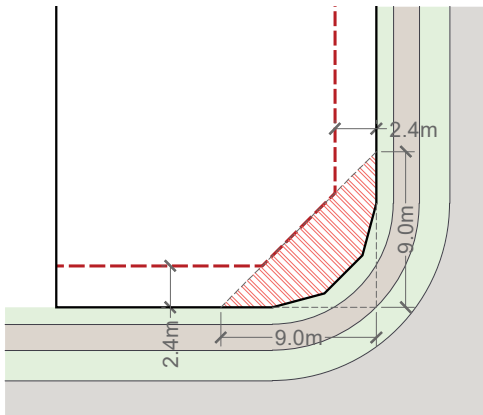
All tanks to residential dwellings must collect a minimum 50% roof area capture. All tanks must supply water to toilets, laundry and have a connection for external usage and must have a backup supply from the main potable water system. The tank must be located outside of the minimum area required for private open space.

Definitions

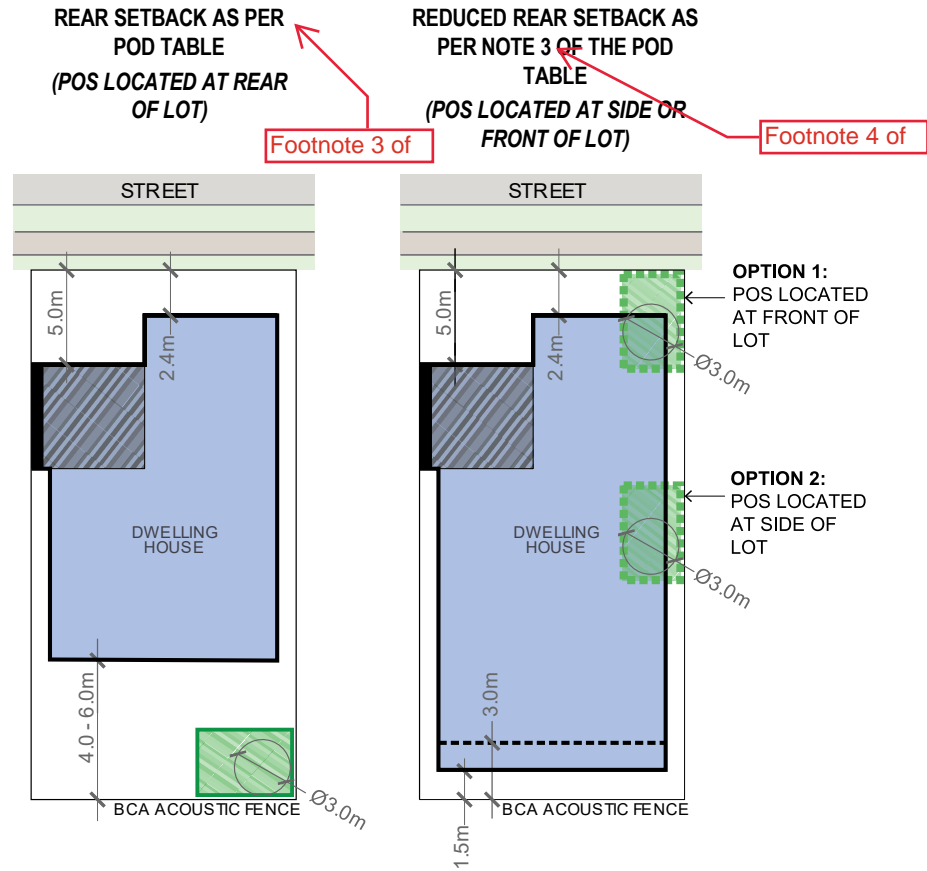
Site Cover

The proportion of the site covered by buildings, including roof overhangs.

TYPICAL CORNER TRUNCATION



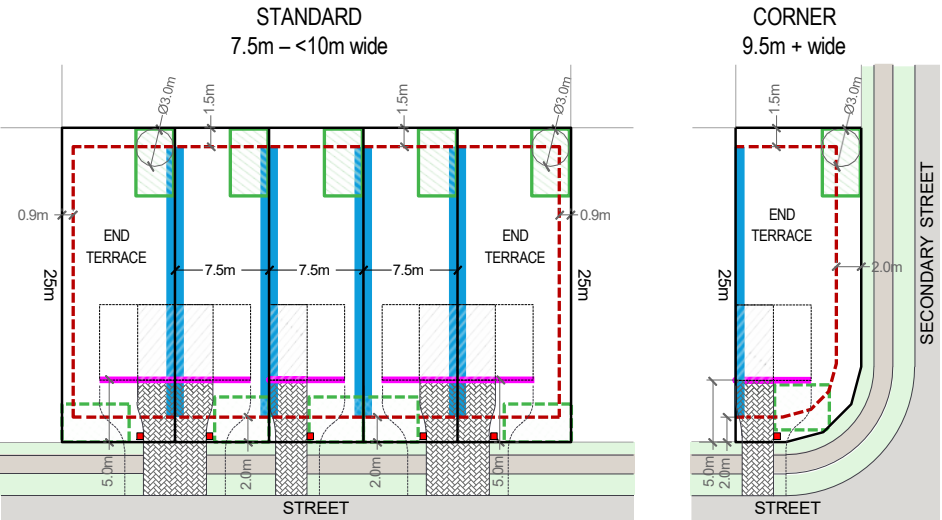
REAR SETBACK AND POS LOCATIONS FOR LOTS ADJOINING BCA ACOUSTIC FENCE



LEGEND

- Lot boundary
- Maximum dwelling house footprint (ground floor)
- Minimum building setback dimensions
- Indicative built to boundary wall
- Garage location
- Typical location of private open space at rear of lot (min. dimension of 3.0m)
- Alternate location of private open space at side or rear of lot (allowing reduced setbacks at rear)
- Reduced rear setback (where compliant) for first floor

TYPICAL TERRACE ALLOTMENTS (FRONT LOADED)



LEGEND

- Lot boundary
- Maximum building envelope (ground floor)
- Building setback dimensions
- Mandatory built to boundary wall
- Garage setback from front boundary
- Preferred private open space location
- Alternate location for private open space
- Single garage for single storey dwelling double garage when 2 storey dwelling
- Indicative driveway location
- Preferred letterbox location

PLAN OF DEVELOPMENT TABLE

| | TERRACE ALLOTMENTS 7.5M-<10M WIDE (FRONT LOADED) | |
|--|---|--|
| | Ground Floor | First Floor (includes Loft Dwellings) |
| Front and Rear (metres) | | |
| Front / Primary Frontage | 2.0 | 2.0 |
| Garage | 5.0m for single, tandem and double garages | n/a |
| Rear | 1.5 | 1.5 |
| Side (metres) | | |
| Built to Boundary | 0.0 | 0.0 |
| Non Built to Boundary ¹ | 0.9 | 0.9 |
| Corner Lots — Secondary frontage | 2.0 | 2.0 |
| Garage and On-site Car Parking | | |
| On site parking requirements (minimum) | 1 space to be covered and enclosed. Single or tandem garages are acceptable. Double garages are permitted on two storey dwellings only. | |
| Garage Location | Garages are to be located along the built to boundary wall. | |
| Site Cover (maximum) | 75% | |

¹ 1.5m where abutting a laneway as per the Laneway Terrace Allotments Diagram

1.1.3 LANEWAY TERRACE ALLOTMENTS - 25m – 28m DEEP (INCLUDING SINGLE STOREY TERRACE ALLOTMENTS)

Development Controls

General

- 1. All development is to be undertaken in accordance with the Development Approval.
- 2. Building setbacks and built-to-boundary wall locations shown are subject to future proposed easements and/or underground services.
- 3. Maximum building height is 3 storeys for all dwellings where permitted in the Plan of Development, and 2 storeys (9m) otherwise, unless a Single Storey terrace type allotment, to which a 1 storey maximum building height applies. Rooftop terraces are defined as a 'storey'.
- 4. Noise affected allotments are to be developed in accordance with AS3671-1989 'Acoustics - Road Traffic Noise Intrusion - building siting and construction' to achieve the satisfactory internal noise levels stipulated in AS2107-2000 'Acoustics - Recommended design sound levels and reverberation times for building interiors'.

Setbacks

- 5. Setbacks are as per the Plan of Development Table unless otherwise specified.
- 6. Built to Boundary Walls:
 - a) Built to boundary wall dimensions are limited by the applicable building setbacks and building height limit. Building plans and details of appropriate development staging and construction methods must be submitted for approval;
 - b) where not adopted, the standard setbacks in the Plan of Development Table apply.
- 7. Boundary setbacks are measured to the main wall of the structure. Minor nib walls (required for fire protection purposes only) may encroach into the setback provided they are wholly contained within the property boundary. Eaves should not encroach closer than 450mm to the lot boundary except in the case of:
 - The Primary Street/Park Frontage where eaves should not be closer than 1500mm;
 - The Secondary Street Frontage where eaves should not be closer than 1000mm; and
 - Walls that are Built to Boundary.
- 8. Upper floor setbacks must not exceed the minimum ground floor setbacks (including rooftop terraces).
- 9. For Angled Terrace Allotments, the built form is to be as generally shown in the Angled Terrace Allotment Diagram.
- 10. Roofed gatehouses and arches are permitted within the front setback (and the secondary frontage for corner lots) provided they meet the requirements of the Queensland Development Code, having:
 - a maximum area of 4m²; and
 - not more than 2m wide elevation to the street; and
 - not more than 3m in height.

Privacy and Overlooking:

- 11. Buildings must be sited and designed to provide adequate visual privacy for neighbouring dwellings:
 - Where the distance separating a window or balcony of an adjoining dwelling from the side or rear boundary is less than 1.5m:
 - a) a permanent window and a balcony has a window/balcony screen extending across the line of sight from the sill to at least 1.5m above the adjacent floor level; or
 - b) a window has a sill height more than 1.5m above the adjacent floor level, or
 - c) a window has obscure glazing below 1.5m;
- where: Window and Window/Balcony Screen has the same meaning as in the Queensland Development Code.

Site Cover and Amenity

- 12. Site cover for each lot is not to exceed that shown in the Plan of Development table.
- 13. Private open space, must be provided and is not less than 15m² with a minimum dimension of 3.0m and is directly accessible from a living area. Where these private open spaces are provided in the form of an upper level balcony, a ground level courtyard must also be provided to assist with cross ventilation and provide space for clothes drying.
- 14. Fencing along street and park frontages must be either 50% transparent or max. 1.2m high. Fencing to the lane may be screen fencing to 1.8m high.
- 15. Letterboxes for dwellings shall be located on the primary street frontage, or if fronting a park within the laneway.
- 16. Allotments adjoining a laneway may have their garage fronting the laneway, comply with the setbacks, and have fencing to the laneway in accordance with that shown in the Laneway Terrace Allotment diagram.
- 17. Parking spaces on driveways do not have to comply with gradients in AS2890.
- 18. A maximum of one driveway per dwelling is permitted.
- 19. Driveways should have the minimum width required to facilitate vehicle access in order to maintain space between driveways, pathways and any bin storage areas for amenity planting.
- 20. Driveways should avoid on-street works such as dedicated on-street parking bays, drainage pits and service pillars.
- 21. The minimum distance of a driveway from an intersection of one street with another street (not a laneway) shall be 6.0 metres.

Site Services and Bin Storage

- 22. All dwellings on Laneway Terrace lots with a width of 4.6m or less are to be designed to ensure the following:
 - a) There are no conflicts between services (electricity/water) and pedestrian access to the front and rear of the lot, or vehicular access to the rear of the lot

- b) A dedicated area is provided within the lot to accommodate refuse bin storage. This is preferred to be located outside of the garage, but accessible to, and screened from, the laneway. In the case of 4m wide lots, the bin storage area may be accommodated within the garage, provided the garage is appropriately dimensioned to ensure the bins can be removed whilst the car is parked in the garage.

Rainwater Tanks

- 23. All dwellings (including Duplex dwellings) must have:
 - Lots >300m² = a 5kL rainwater tank
 - Lots 225-300m² = 3kL rainwater tank
 - Lots <225m² = no tank required
- All tanks to residential dwellings must collect a minimum 50% roof area capture. All tanks must supply water to toilets, laundry and have a connection for external usage and must have a backup supply from the main potable water system. The tank must be located outside of the minimum area required for private open space.

Facade Treatment, Passive Ventilation and Natural Light Provisions

- 24. On all terrace lots the main façade (facing the primary street frontage) must comprise at least two different materials and at least one horizontal or vertical step in the façade;
- 25. Each dwelling unit must not present the same façade design to the street as the immediate adjoining dwellings;
- 26. The design of the dwelling must consider passive ventilation and natural light provision through a combination of the following elements:
 - inclusion of a mid-block courtyard space, light wells or recess that extends into the dwelling to allow multiple rooms to have windows/openings onto that space;
 - each living area that adjoins an external wall (not built to boundary) has an open-able window or door.

Secondary Dwelling Above Garage

- 27. Secondary Dwellings above Garages are permitted on lots identified in the Plan of Development plans in Appendix E of this document. They shall be generally as shown in the diagram: Typical Diagram for Secondary Dwellings above Garages in this PoD.
- 28. Where secondary dwellings above garages are built adjacent the laneway, the design of windows, balconies and other amenity spaces shall be located and adequately screened to avoid overlooking of, and from, secondary dwellings on the opposite side of the laneway.
- 29. The relationship of garages to the laneway and bin storage areas shall be generally as shown in the diagrams: Typical Laneway Interface and Typical Product for Secondary Dwelling above Garage, in this PoD.
- 30. Secondary Dwellings must also comply with Table 6 in Part 2.6 of this PoD (Section 1) to remain Exempt.

Definitions

Site Cover

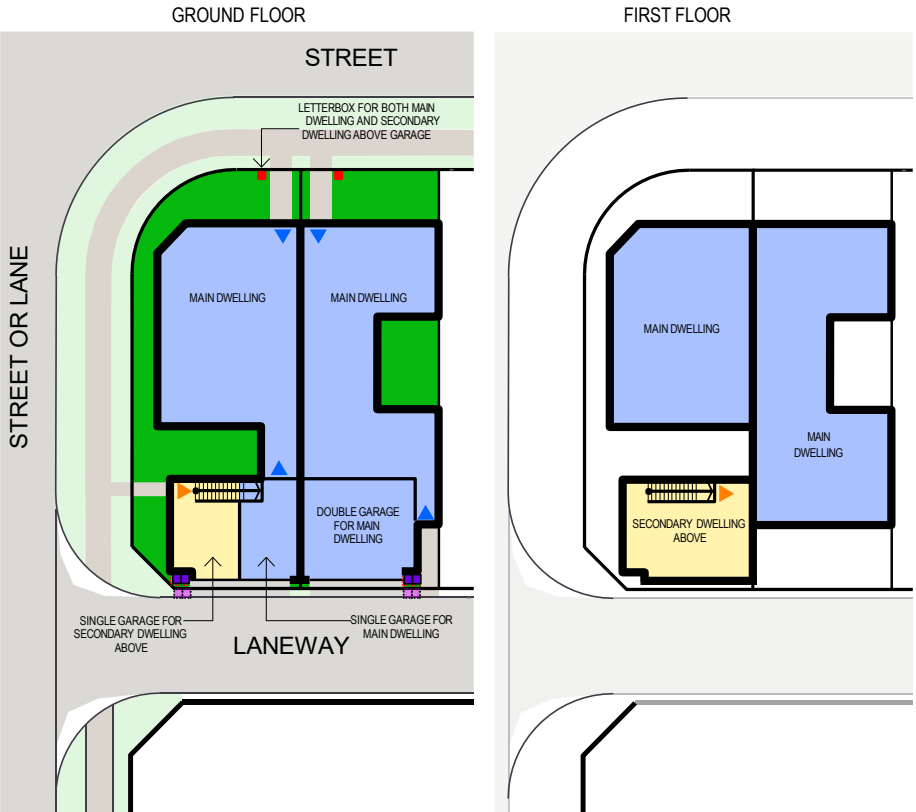
The proportion of the site covered by buildings, including roof overhangs.

PLAN OF DEVELOPMENT TABLE

| | LANEWAY TERRACE ALLOTMENTS 4M- <14M WIDE (REAR LOADED) | |
|--|--|-------------|
| | Ground Floor | First Floor |
| Front and Rear (metres) | | |
| Front / Primary Frontage | 2.0 | 2.0 |
| Garage | 0.5 | n/a |
| Rear | 0.5 | 0.0 |
| Side (metres) | | |
| Built to Boundary | 0.0 | 0.0 |
| Non Built to Boundary ¹ | 0.9 | 0.9 |
| Corner Lots — Secondary street frontage | 1.5 | 1.5 |
| Corner Lots where secondary frontage shares a boundary with a laneway or linear open space or ped link | 0.9 | 0.9 |
| Garage and On-site Car Parking | | |
| On site parking requirements (minimum) | 1 space to be covered and enclosed. Single, tandem or double garages are acceptable depending on lot width. | |
| Garage Location | Garages are to be located along the built to boundary wall. | |
| Site Cover (maximum) | 85% | |

¹ 1.5m where abutting a laneway as per the Laneway Terrace Allotments Diagram

TYPICAL PRODUCT SECONDARY DWELLING ABOVE GARAGE



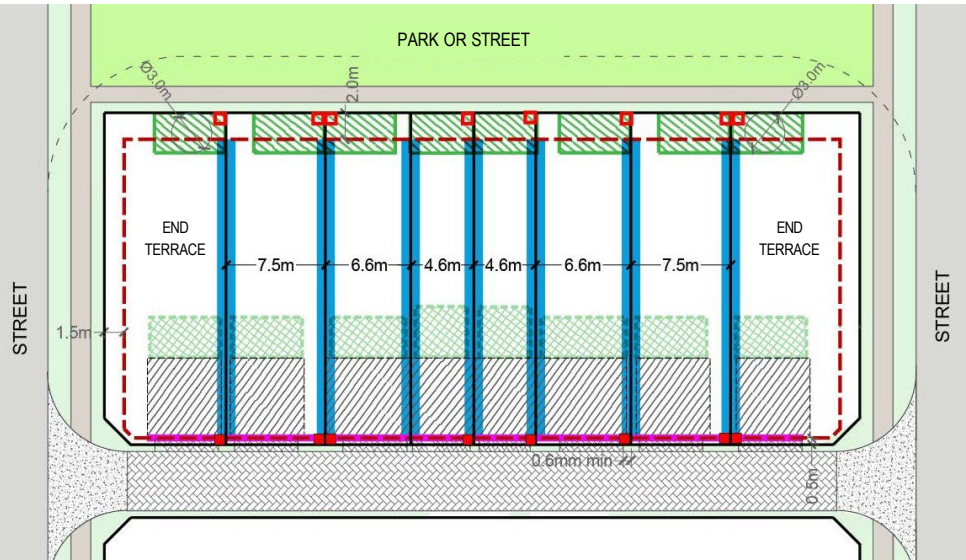
LEGEND

- Lot boundary
- Indicative built form
- Main dwelling
- Secondary dwelling above garage
- Preferred letterbox locations
- Preferred location of bin storage (and indicative screening)
- Preferred location of bin collection
- Entrance to main dwelling (indicative location)
- Entrance to secondary dwelling (indicative location)
- Indicative private open space / landscaping locations

The above should be read in conjunction with Section 2 of the PoD and comply with the Table 6 Secondary Dwelling Controls.

Table 2

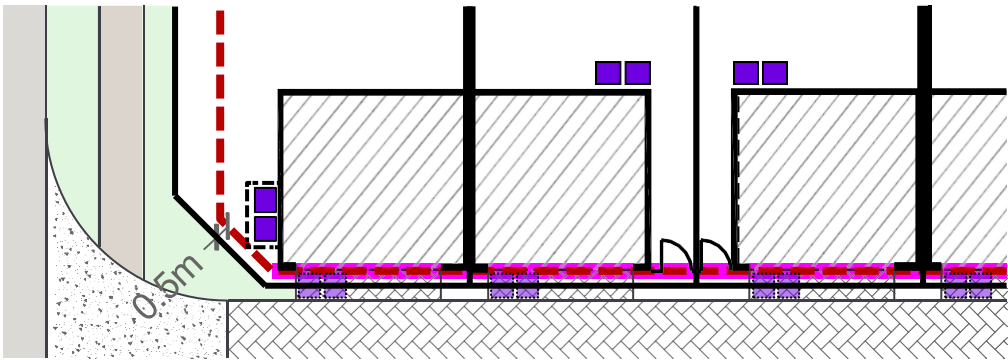
TYPICAL TERRACE FRONTING PARK OR STREET (REAR LOADED)



LEGEND

- Lot boundary
- Maximum building envelope (ground floor)
- Building setback dimensions
- Mandatory built to boundary wall
- Garage setback from front boundary
- Preferred private open space location
- Alternate location for private open space
- Garage location
- Letterbox location (if primary frontage is park)
- Letterbox location (if primary frontage is a street)

TYPICAL LANEWAY INTERFACE



LEGEND

- Lot boundary
- Building setback dimensions
- Indicative driveway location
- Preferred location of bin storage (main dwelling and loft)
- Preferred location of bin collection (main dwelling and loft)
- Screened bin enclosure

1.1.4 LANEWAY TERRACE ALLOTMENTS – 16.5m AND21m DEEP

Development Controls

General

1.

All development is to be undertaken in accordance with the Development Approval.
2.

Building setbacks and built-to-boundary wall locations shown are subject to future proposed easements and/or underground services.
3.

Maximum building height is 3 storeys for all dwellings where permitted in the Plan of Development, and 2 storeys (9m) otherwise. Rooftop terraces are defined as a 'storey'.
4.

Noise affected allotments are to be developed in accordance with AS3671-1989 'Acoustics - Road Traffic Noise Intrusion - building siting and construction' to achieve the satisfactory internal noise levels stipulated in AS2107-2000 'Acoustics - Recommended design sound levels and reverberation times for building interiors'.

Setbacks

5.

Setbacks are as per the Plan of Development Table unless otherwise specified.
6.

Built to Boundary Walls:

a)

Built to boundary wall dimensions are limited by the applicable building setbacks and building height limit. Building plans and details of appropriate development staging and construction methods must be submitted for approval;

b)

where not adopted, the standard setbacks in the Plan of Development Table apply.
7.

Boundary setbacks are measured to the main wall of the structure. Nib walls may encroach into the setback provided they are wholly contained within the property boundary. Eaves should not encroach closer than 450mm to the lotboundary except in the case of:

-

The Primary Street/Park Frontage where eaves can about the property boundary;

-

The Secondary Street Frontage where eaves should not be closer than 750mm; and

-

Walls that are Built to Boundary.
8.

Feature end treatment of the built to boundary wall is required where abutting the site boundary at the primary frontage. Feature end treatment to be in a material consistent with materials used on the primary frontage building facade.

Privacy and Overlooking:

9.

Buildings must be sited and designed to provide adequate visual privacy for neighbouring dwellings:

-

Where the distance separating a window or balcony of an adjoining dwelling from the side or rear boundary is less than 1.5m:

a)

a permanent window and a balcony has a window/balcony screen extending across the line of sight from the sill to at least 1.5m above the adjacent floor level; or

b)

a window has a sill height more than 1.5m above the adjacent floor level, or

c)

a window has obscure glazing below 1.5m;

where: Window and Window/Balcony Screen has the same meaning as in the Queensland Development Code.

Site Cover and Amenity

10.

Site cover for each lot is not to exceed that shown in the Plan of Development table.
11.

Private open space must be provided in accordance with the below. This area may be roofed and take the form of an upper level balcony or rooftop terrace:

-

2 bedroom dwellings - 9m² with a minimum dimension of 2.5m;

-

3+ bedroom dwellings - 12m² with a minimum dimension of 2.5m.
12.

Fencing to Primary Frontages must be in the form of planter boxes and/or privacy screening for residents, in particular where there is a front open space.
13.

If provided, privacy screening must either be of solid material (e.g. timber, steel), opaque screens, perforated panels, or trellises that are permanently fixed, and are to have a maximum of 50 per cent openings.
14.

Fencing to the lane may be screen fencing to 1.8m high.
15.

Fencing to the Secondary Frontage must be either 50% transparent or max. 1.2m high.
16.

Letterboxes for dwellings shall be located on the primary street frontage, or if fronting a park within the laneway. Letterboxes must be integrated into the dwelling, and not be standalone.
17.

A maximum of one driveway per dwelling is permitted.
18.

Driveways should have the minimum width required to facilitate vehicle access in order to maintain space between driveways, pathways and any bin storage areas for amenity planting.

Site Services and Bin Storage

19.

All dwellings are to be designed to ensure the following:

a)

There are no conflicts between services (electricity/ water) and pedestrian access to the front and rear of the lot, or vehicular access to the rear of the lot;

b)

A dedicated area is provided within the lot to accommodate refuse bin storage. This is preferred to be located outside of the garage on the laneway interface. Bin storage must be appropriately screened;

c)

Air-conditioners, hot water systems, clothes lines and other household services must be screened and/or located to minimise the visual impact to the street. Services may be visible from the lane, but must be screened.

Rainwater Tanks

20.

For lots <225m², no rainwater tank is required.

Facade Treatment, Passive Ventilation and Natural Light Provisions

21.

On all terrace lots the main façade (facing the primary street frontage) must comprise at least two different materials and at least one horizontal or vertical step in the façade.
22.

Each dwelling unit must not present the same façade design to the street as the immediate adjoining dwelling/s.
23.

Design of corner dwellings must visually 'wrap' around the corner, providing activation of the corner and passive surveillance of the secondary street through the form of porch / alfresco openings and/or glazing.
24.

All dwellings must include a clearly identifiable and addressed front door. Front door must be visible from the street. Front door access must not be via a lane. Sliding doors do not constitute a front door. Front door must be sufficiently sheltered from the elements, preferably utilising the structure of the first floor.
25.

Dwellings must include landscaping along the street frontage to reinforce the dwelling entry, and to positively contribute to the streetscape. Ground covers are preferred in lieu of turf. Vertical landscaping should also be considered to positively influence streetscape.
26.

The design of the dwelling must consider passive ventilation and natural light provision through a combination of the following elements:

-

inclusion of a mid-block courtyard space, light wells or recess that extends into the dwelling to allow multiple rooms to have windows/openings onto that space;

-

each living area that adjoins an external wall (not built to boundary) has an open-able window or door.

Definitions

Site Cover

The proportion of the site covered by buildings, including roof overhangs.

PLAN OF DEVELOPMENT TABLE

| | LANEWAY TERRACE ALLOTMENTS – 16.5M & 21M DEEP | |
|--|---|------------------|
| | 6.6m Wide + | |
| | Ground Floor | Upper Floor |
| Front / Primary Street Frontage (metres) | | |
| Primary Frontage | 1.5 ¹ | 1.0 ¹ |
| Garage | 0.5 mandatory | n/a |
| Rear | 0.5 | 0.0 |
| Side (metres) | | |
| Built to Boundary | 0.0 | 0.0 |
| Non Built to Boundary | 0.9 | 0.9 |
| Corner Lots — Secondary frontage | 1.2 ¹ | 1.2 ¹ |
| Garage and On-site Car Parking | | |
| On site parking requirements (minimum) | 1 space to be covered and enclosed. Single garages acceptable. Double garages are permitted. Garages are to be located along the built to boundary wall | |
| Site Cover (maximum) | 90% | |

¹0.0m to balconies and verandahs

TYPICAL TERRACE FRONTING PARK OR STREET (REAR LOADED)



LEGEND

- Lot boundary
- - -

Maximum building envelope (ground floor)
- - -

Protrusions for verandah / balconies
- 1.0m

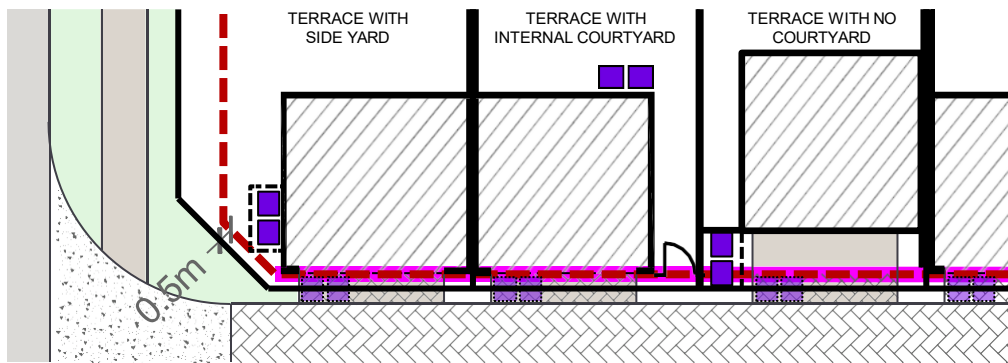
Building setback dimensions
- Mandatory built to boundary wall
- Garage setback from front boundary
- ▨

Preferred private open space location
- ▨

Alternate location for private open space (21m deep terraces only)
- ▨

Garage location
- Letterbox location (if primary frontage is park)
- Letterbox location (if primary frontage is a street)

TYPICAL LANEWAY INTERFACE



LEGEND

- Lot boundary
- 1.0m

Building setback dimensions
- ▨

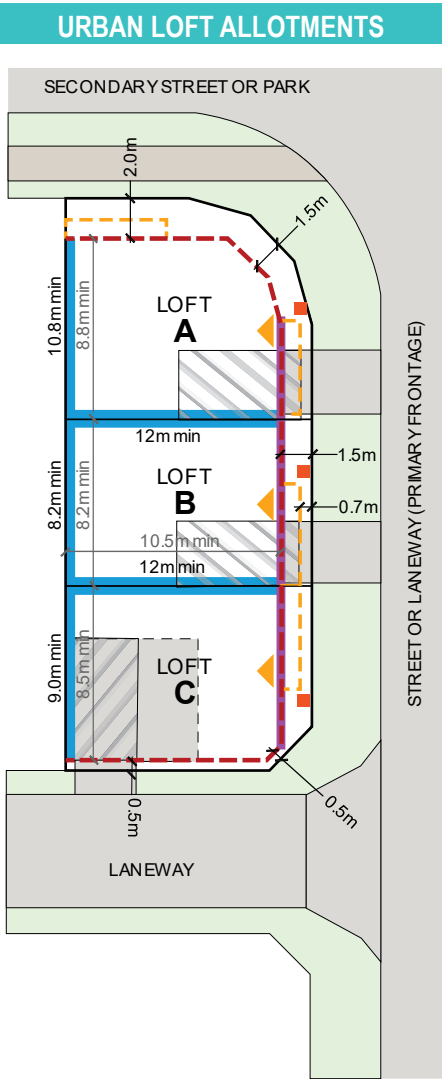
Indicative driveway location
- Preferred location of bin storage
- Preferred location of bin collection
- - -

Screened bin enclosure

1.1.5 URBAN LOFTS - A, B AND C

Development Controls

| General | | Privacy and Overlooking | Site Cover and Amenity |
|---|--|--|---|
| <div>1. All development is to be undertaken in accordance with the Development Approval.</div> <div>2. All building setbacks and built to boundary walls are subject to service easements existing and proposed.</div> <div>3. Maximum building height is 2 storeys (9m) for all dwellings or 3 storeys where identified on a Fixed Elements Plan or Plan of Development. Rooftop terraces are defined as a 'storey'.</div> | | <div>7. Buildings must be sited and designed to provide adequate visual privacy for neighbours:<div>- Where the distance separating a window or balcony of an adjoining dwelling from the side or rear boundary is less than 1.5m:<div>a) a permanent window and a balcony has a window/ balcony screen extending across the line of sight from the sill to at least 1.5m above the adjacent floor level; or</div><div>b) a window has a sill height more than 1.5m above the adjacent floor level, or</div><div>c) a window has obscure glazing below 1.5m; where: Window and Window/Balcony Screen has the same meaning as in the Queensland Development Code.</div></div></div> | <div>13. Site cover for each lot is not to exceed that shown in the Plan of Development table.</div> <div>14. Private open space must be provided and may be roofed and take the form of an upper floor balcony or rooftop terrace that is not less than 8m² with a minimum dimension of 2.5m and accessible from an upper floor living area.</div> |
| Setbacks | | Parking and Driveways | Fencing |
| <div>4. Setbacks are as per the Plan of Development Table, unless dimensioned otherwise on plan. Setbacks are measured to the wall of the building or structure. Eaves may extend into the front, side and rear setbacks up to 450mm from the property boundary except in the case of:<div>- to a laneway: up to the property boundary;</div><div>- where a wall is built to boundary.</div></div> <div>5. Upper floor setbacks must be in accordance with minimum ground floor setbacks.</div> <div>6. Built to Boundary Walls:<div>a) Built to boundary wall dimensions are limited by the boundary setbacks and building height limit. Building plans and details of appropriate development staging and construction methods must be submitted for approval;</div><div>b) where not adopted, the setbacks for non built to boundary walls in the Plan of Development Table apply.</div></div> | | <div>8. Doubles garages are permitted only on Loft type A and C lots and not on Loft type B. On Loft Type A lots a double garage may only front the secondary street and on Loft type C only the laneway.</div> <div>9. A maximum of one driveway is permitted per lot.</div> <div>10. Driveways are to have the following widths:<div>- single driveway: maximum 3.0m;</div><div>- double driveway: maximum 4.8m to a secondary street and to a laneway, the minimum width required to facilitate vehicle access.</div>Driveways are to avoid on-street works such as: dedicated parking bays, drainage inlets, service pillars, street planting beds and bio-retention areas.</div> <div>11. The minimum distance of a driveway from an intersection of one street with another street (not a laneway) shall be 6.0 metres.</div> <div>12. Parking spaces on driveways do not have to comply with gradients in AS2890.</div> | <div>15. Fencing along the street frontage may be up to 1.2m.</div> |
| | | Definitions | |
| | | Site Cover | |
| | | The proportion of the site covered by buildings, including roof overhangs. | |



PLAN OF DEVELOPMENT TABLE

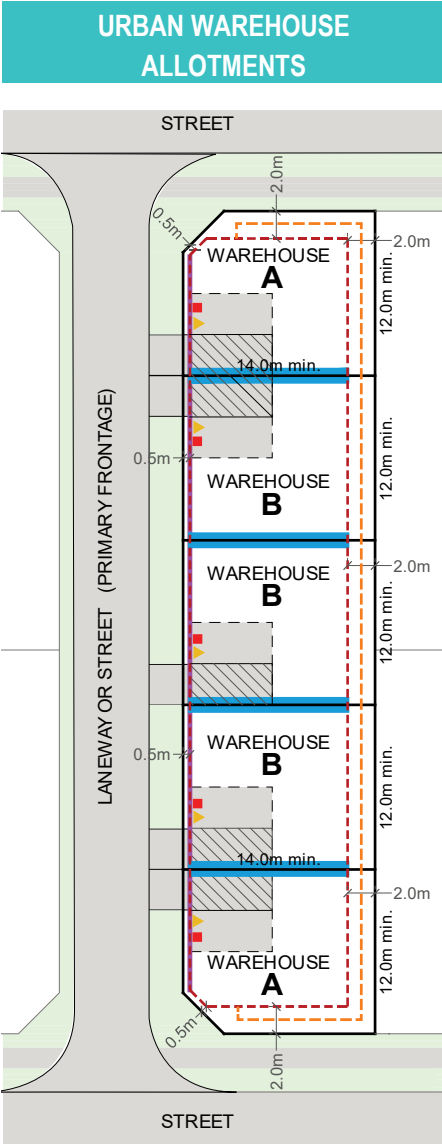
| | URBAN LOFT ALLOTMENTS | | | | | |
|--|--|------------------|---|------------------|---|------------------|
| | Type A | | Type B | | Type C | |
| | Ground Floor | Upper Floor | Ground Floor | Upper Floor | Ground Floor | Upper Floor |
| Front / Primary Street Frontage (metres) | | | | | | |
| Primary Frontage | 1.5 | 1.5 ¹ | 1.5 | 1.5 ¹ | 1.5 | 1.5 ¹ |
| Garage | 0.7 mandatory | n/a | 0.7 mandatory | n/a | 0.5 mandatory | n/a |
| Rear | 0.0 | 0.0 | 0.0 | 1.5 | 0.0 | 0.0 |
| Side (metres) | | | | | | |
| Built to Boundary | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 | 0.0 |
| Non Built to Boundary | n/a | n/a | n/a | n/a | n/a | n/a |
| Corner Lots — Secondary Street | 2.0 | 2.0 ¹ | n/a | n/a | 0.5 | 0.5 |
| Garage and On-site Car Parking | | | | | | |
| On site parking requirements (minimum) | 1 space to be covered and enclosed. Single garage acceptable. Double garages are permitted only where fronting the secondary street. Garages are to be located along the built to boundary wall. | | 1 space to be covered and enclosed. Single garage acceptable. Double garages are not permitted. Garages are to be located along the built to boundary wall. | | 1 space to be covered and enclosed. Single garage acceptable. Double garages are permitted only where fronting laneway. Garages are to be located along the built to boundary wall. | |
| Site Cover (maximum) | 90% | | 95% | | 95% | |

¹ Upper floor balconies and awnings may protrude up to 1.0m into the boundary setback

1.1.6 URBAN WAREHOUSE ALLOTMENTS

Development Controls

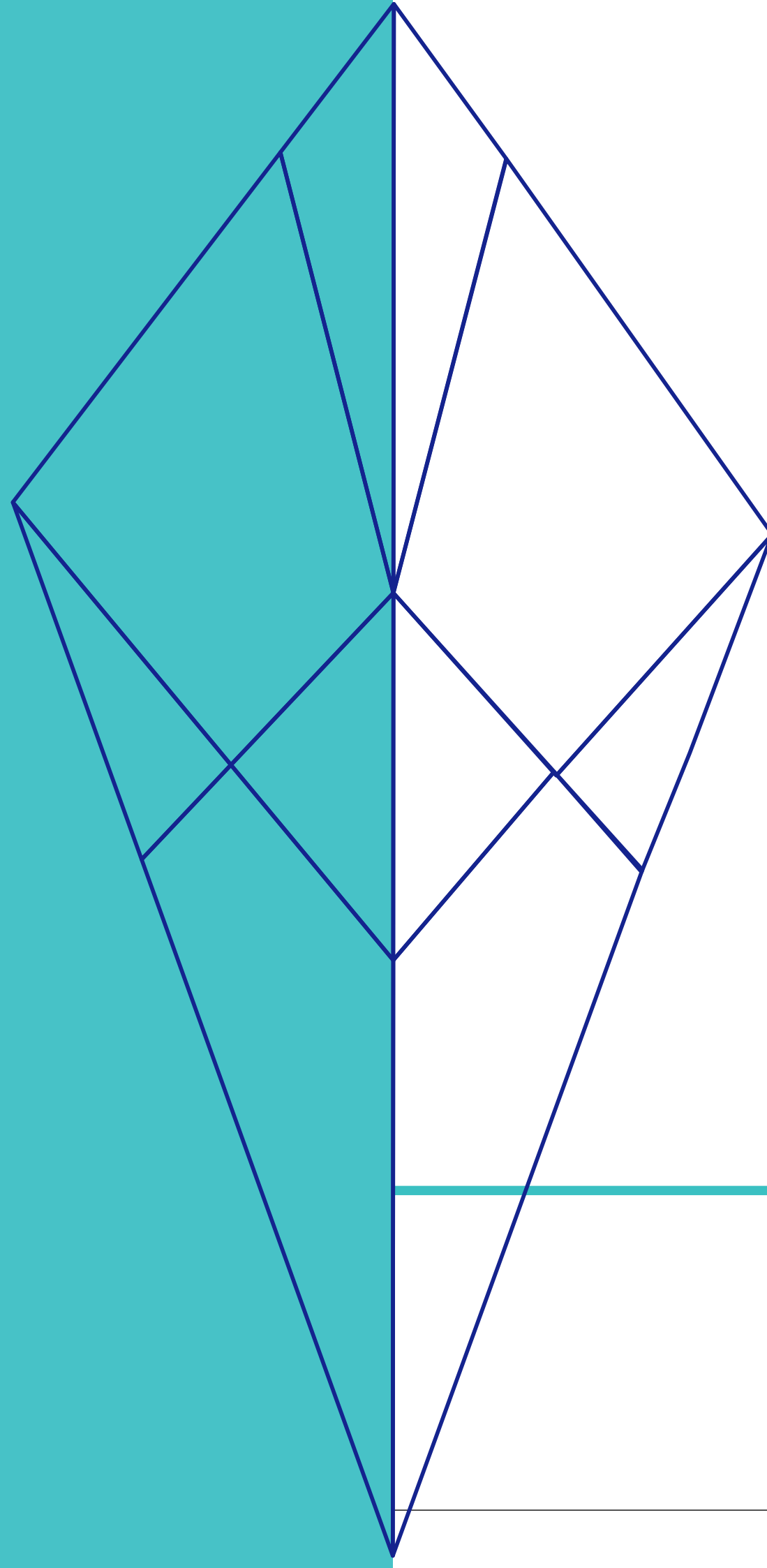
| General | | Privacy and Overlooking | Site Cover and Amenity |
|---|--|---|---|
| <div>1. All development is to be undertaken in accordance with the Development Approval.</div> <div>2. All building setbacks and built to boundary walls are subject to service easements existing and proposed.</div> <div>3. Maximum building height is 2 storeys (9m) for all dwellings or 3 storeys where identified on a Fixed Elements Plan or Plan of Development. Rooftop terraces are defined as a 'storey'.</div> | | <div>7. Buildings must be sited and designed to provide adequate visual privacy for neighbours:<div>- Where the distance separating a window or balcony of an adjoining dwelling from the side or rear boundary is less than 1.5m:<div>a) a permanent window and a balcony has a window/ balcony screen extending across the line of sight from the sill to at least 1.5m above the adjacent floor level; or</div><div>b) a window has a sill height more than 1.5m above the adjacent floor level, or</div><div>c) a window has obscure glazing below 1.5m; where: Window and Window/Balcony Screen has the same meaning as in the Queensland Development Code.</div></div></div> | <div>13. Site cover for each lot is not to exceed that shown in the Plan of Development table.</div> <div>14. Private open space must be provided and may be roofed and take the form of an upper floor balcony or rooftop terrace that is not less than 8m² with a minimum dimension of 2.5m and accessible from an upper floor living area.</div> |
| Setbacks | | Parking and Driveways | Fencing |
| <div>4. Setbacks are as per the Plan of Development Table, unless dimensioned otherwise on plan. Setbacks are measured to the wall of the building or structure. Eaves may extend into the front, side and rear setbacks up to 450mm from the property boundary except in the case of:<div>- to a laneway: up to the property boundary;</div><div>- where a wall is built to boundary.</div></div> <div>5. Upper floor setbacks must be in accordance with minimum ground floor setbacks.</div> <div>6. Built to Boundary Walls:<div>a) Built to boundary wall dimensions are limited by the boundary setbacks and building height limit. Building plans and details of appropriate development staging and construction methods must be submitted for approval;</div><div>b) where not adopted, the setbacks for non built to boundary walls in the Plan of Development Table apply.</div></div> | | <div>8. Doubles garages are permitted only on Warehouse Type A and B. A double garage may front the secondary street on Warehouse Type A lots.</div> <div>9. A maximum of one driveway is permitted per lot.</div> <div>10. Driveways are to have the following widths:<div>- single driveway: maximum 3.0m;</div><div>- double driveway: maximum 4.8m to a secondary street and to a laneway, the minimum width required to facilitate vehicle access.</div>Driveways are to avoid on-street works such as: dedicated parking bays, drainage inlets, service pillars, street planting beds and bio-retention areas.</div> <div>11. The minimum distance of a driveway from an intersection of one street with another street (not a laneway) shall be 6.0 metres.</div> <div>12. Parking spaces on driveways do not have to comply with gradients in AS2890.</div> | <div>15. Fencing along the street frontage may be up to 1.2m.</div> |
| | | Definitions | |
| | | Site Cover | |
| | | The proportion of the site covered by buildings, including roof overhangs. | |



PLAN OF DEVELOPMENT TABLE

| | URBAN WAREHOUSE ALLOTMENTS | | | |
|--|---|------------------|---|------------------|
| | Type A | | Type B | |
| | Ground Floor | Upper Floor | Ground Floor | Upper Floor |
| Front / Primary Street Frontage (metres) | | | | |
| Primary Frontage | 0.5 | 0.5 | 0.5 | 0.5 |
| Garage | 0.5 mandatory | n/a | 0.5 mandatory | n/a |
| Rear | 2.0 | 2.0 ¹ | 2.0 | 2.0 ¹ |
| Side (metres) | | | | |
| Built to Boundary | 0.0 | 0.0 | 0.0 | 0.0 |
| Non Built to Boundary | n/a | n/a | n/a | n/a |
| Corner Lots — Secondary Street | 2.0 | 2.0 ¹ | n/a | n/a |
| Garage and On-site Car Parking | | | | |
| On site parking requirements (minimum) | 1 space to be covered and enclosed. Single garage acceptable. Double garages are permitted. Garages are to be located along the built to boundary wall. | | 1 space to be covered and enclosed. Single garage acceptable. Double garages are permitted. Garages are to be located along the built to boundary wall. | |
| Site Cover (maximum) | 90% | | 95% | |

¹ Upper floor balconies and awnings may protrude up to 1.0m into the boundary setback



APPENDIX D ROAD HIERARCHY

LEGEND

Application Boundary

INTERSECTIONS

Signalised Intersection

SUB-ARTERIAL ROADS

Sub-Arterial 3A - 35.5m

TRUNK CONNECTOR

Trunk Connector C2 - 25.0m

Trunk Connector G - 25.9m

NEIGHBOURHOOD CONNECTOR

Neighbourhood Connector N1 - 25.0m

Neighbourhood Connector N2 - 25.0m

Neighbourhood Connector L - 19.0m

ACCESS STREETS

Urban Access A1 - 19.5m

Urban Access A2 - 17.5m

Access Street P - 15.5m

Access Street Esplanade - 13.5m

Laneway Link - 14.8m

Laneway - 10.5m

Laneway - 8.0m

Shared Vehicular / Pedestrian Access (Laneway) - 13.0m

Shared Vehicular / Pedestrian Access (Driveway) - 12.0m

INTERIM VEHICULAR ACCESS

Interim Vehicular Access

(Refer to Engineering Services Report for Further Detail on Interim Vehicular Access Staging)

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DATE: 19 JUNE 2024
CLIENT: STOCKLAND
DRAWN BY: JC/MD
CHECKED BY: MD



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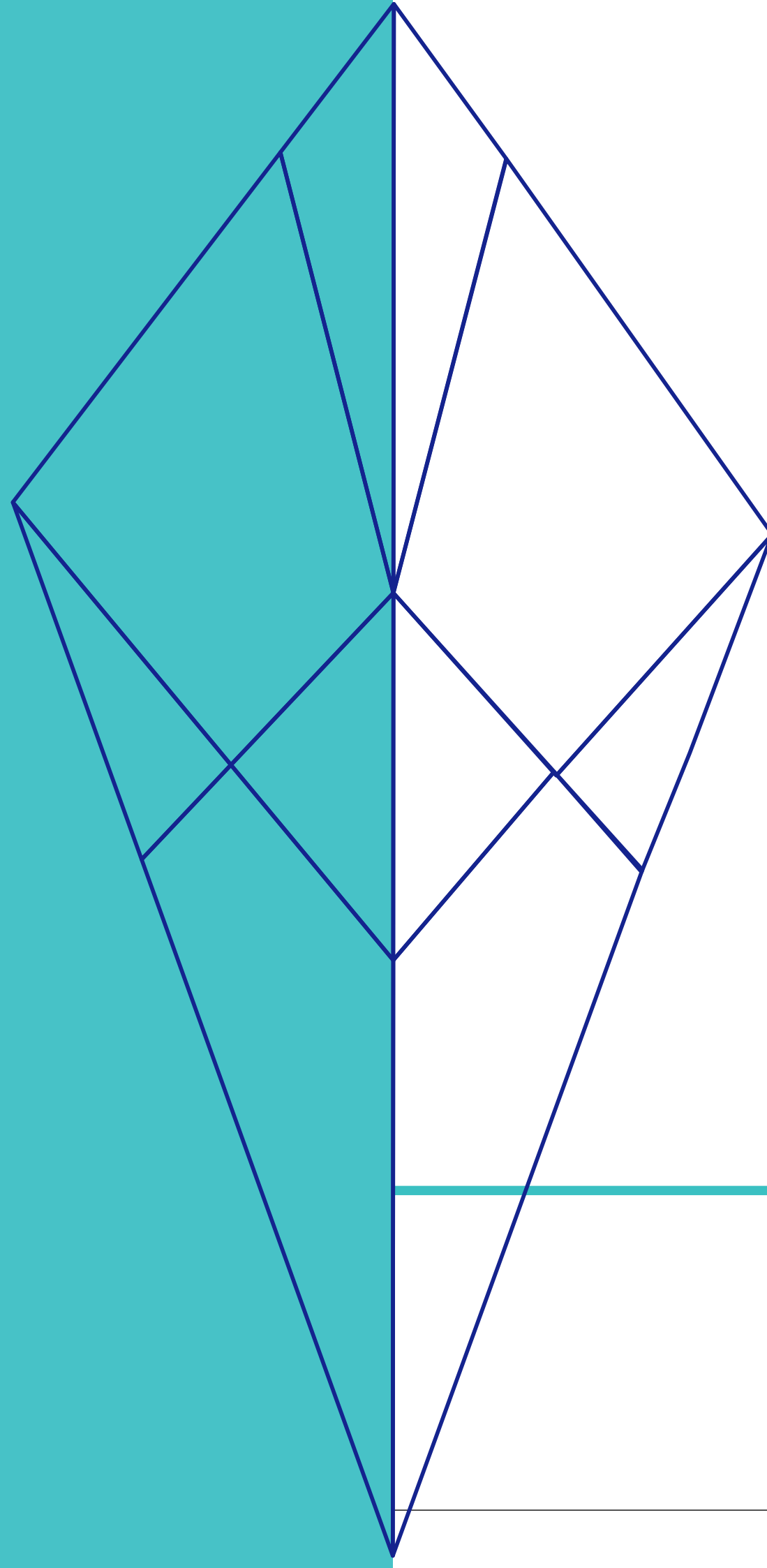
AURA LAKES - PRECINCT 18 (PART)
ROAD HIERARCHY PLAN

Not a signalised intersection

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APPENDIX E ROAD CROSS SECTIONS

SUB-ARTERIAL 3A

35.5m



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DATE: 18 JUNE 2024
CLIENT: STOCKLAND
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CHECKED BY: MD

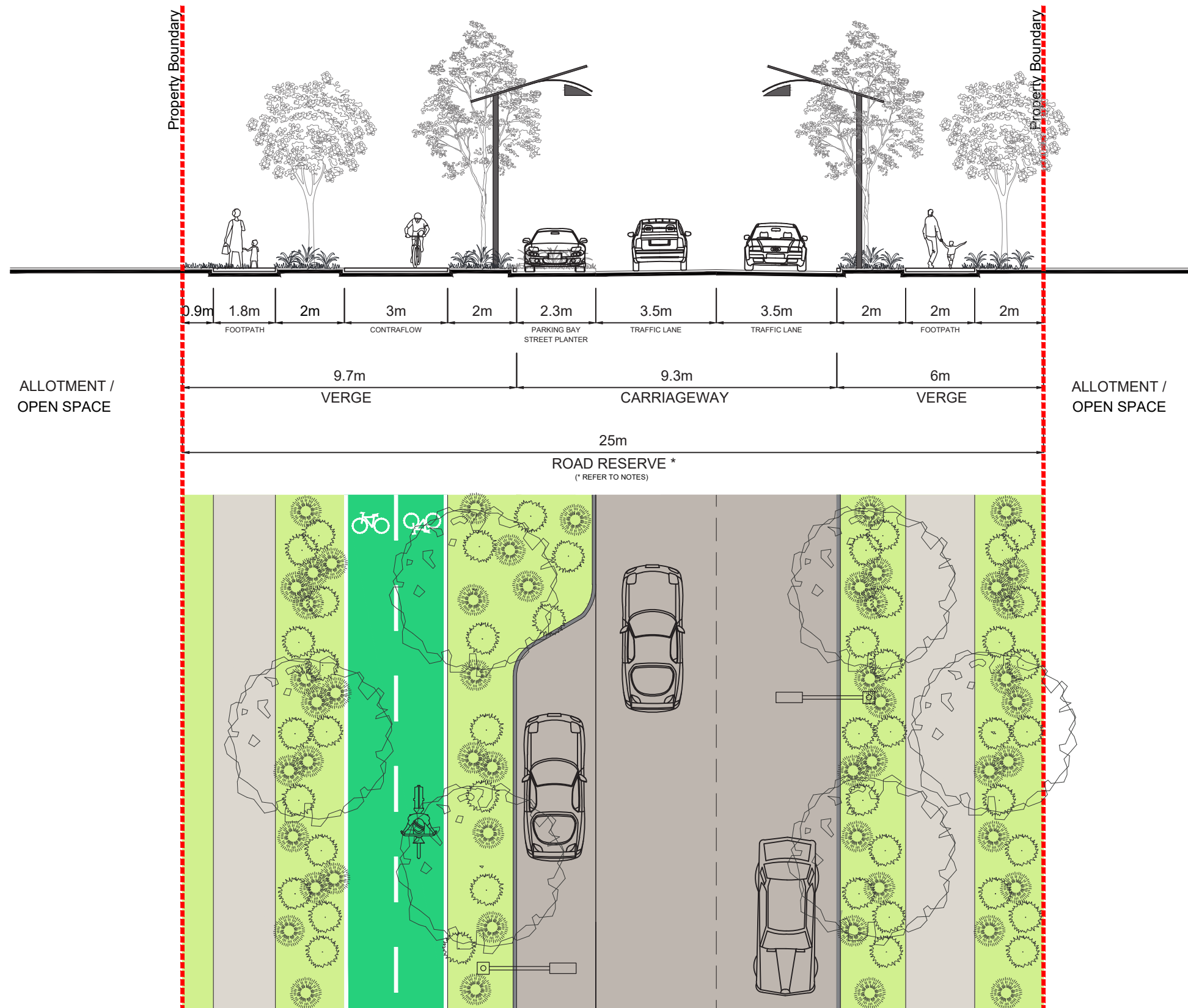
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TRUNK CONNECTOR C2

25m



NOTES

* Road cross section where road crosses culvert to be reviewed in detailed design.

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AURA LAKES - PRECINCT 18 (PART) ROAD CROSS SECTIONS

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DATE: 18 JUNE 2024
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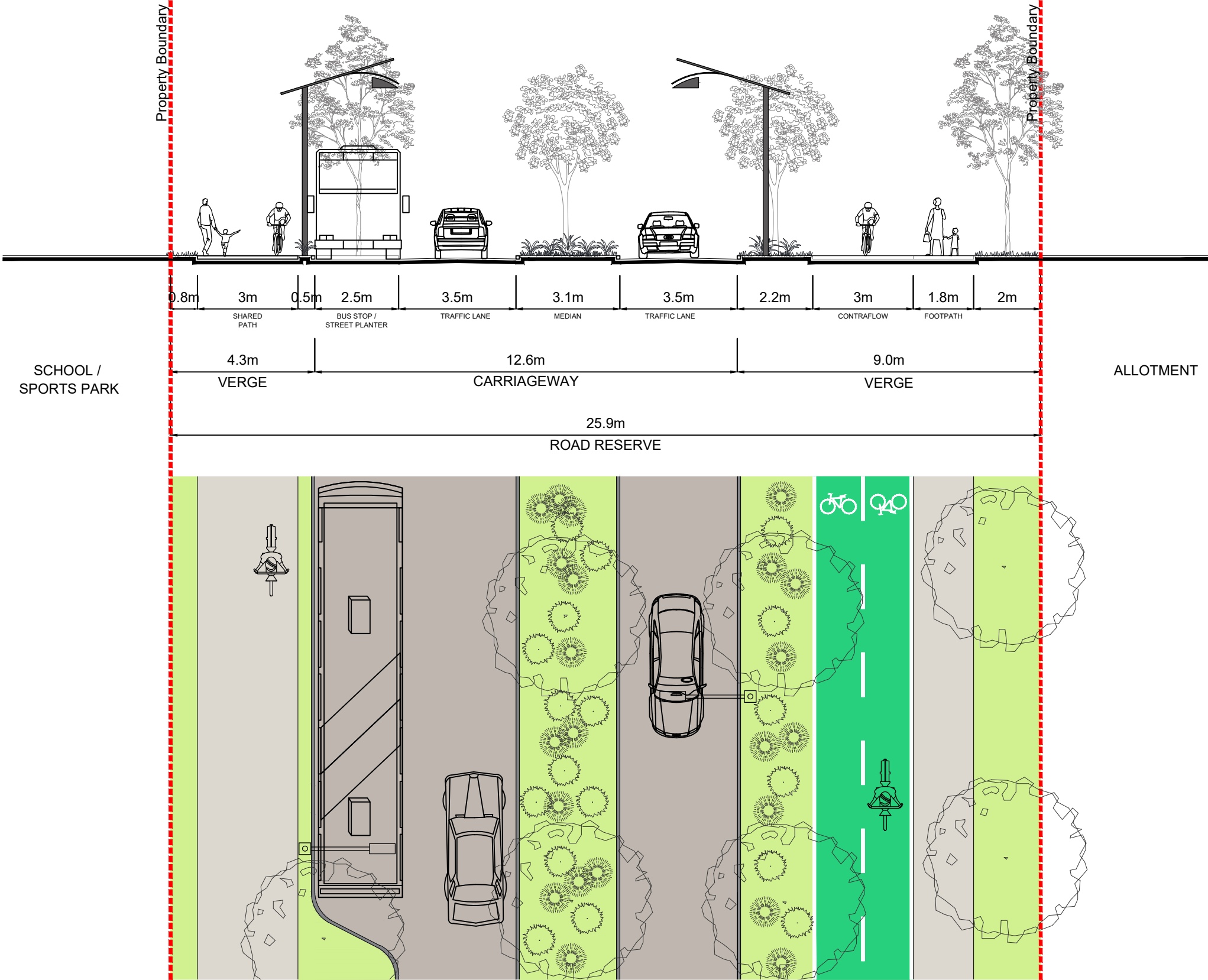
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TRUNK CONNECTOR G

25.9m



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AURA LAKES - PRECINCT 18 (PART)

ROAD CROSS SECTIONS

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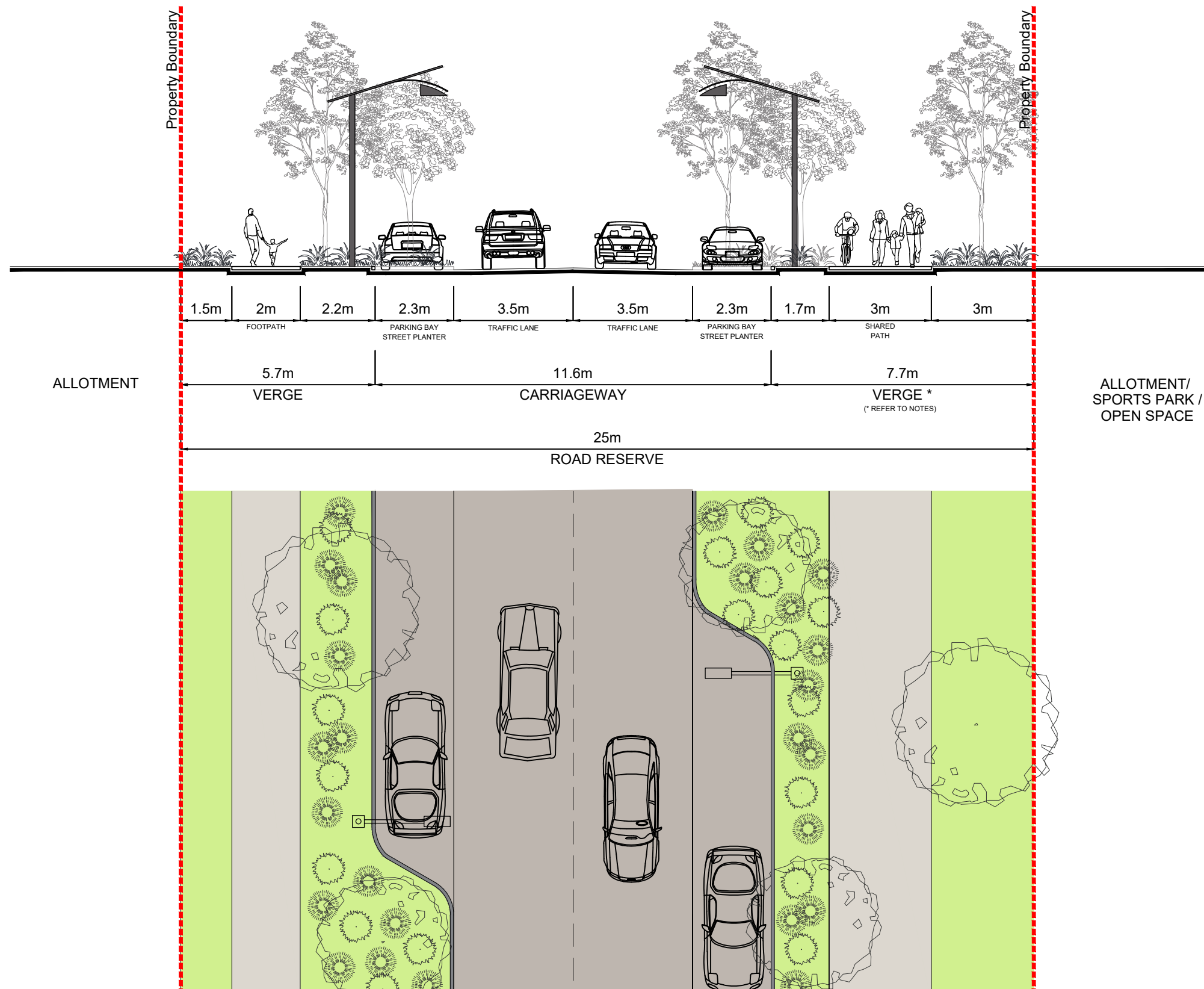


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NEIGHBOURHOOD CONNECTOR N1

25m



NOTES

* Where road cross section is adopted adjoining open space, verge width may be reduced / footpath may be located within open space.

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AURA LAKES - PRECINCT 18 (PART) ROAD CROSS SECTIONS

PLAN REF: **AU12885-17C**
DATE: 18 JUNE 2024
CLIENT: STOCKLAND
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CHECKED BY: MD

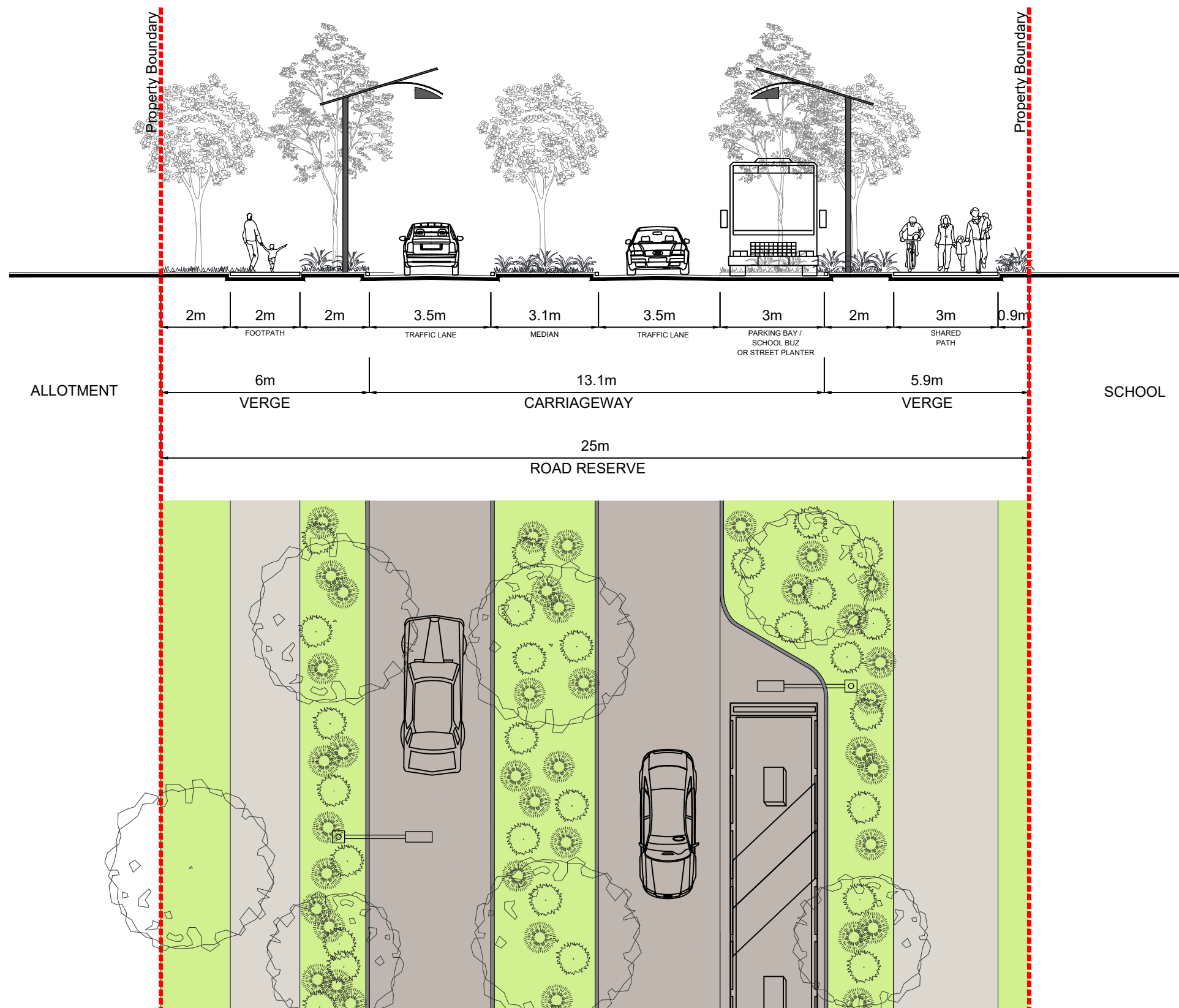
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NEIGHBOURHOOD CONNECTOR N2

25m



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AURA LAKES - PRECINCT 18 (PART)
ROAD CROSS SECTIONS

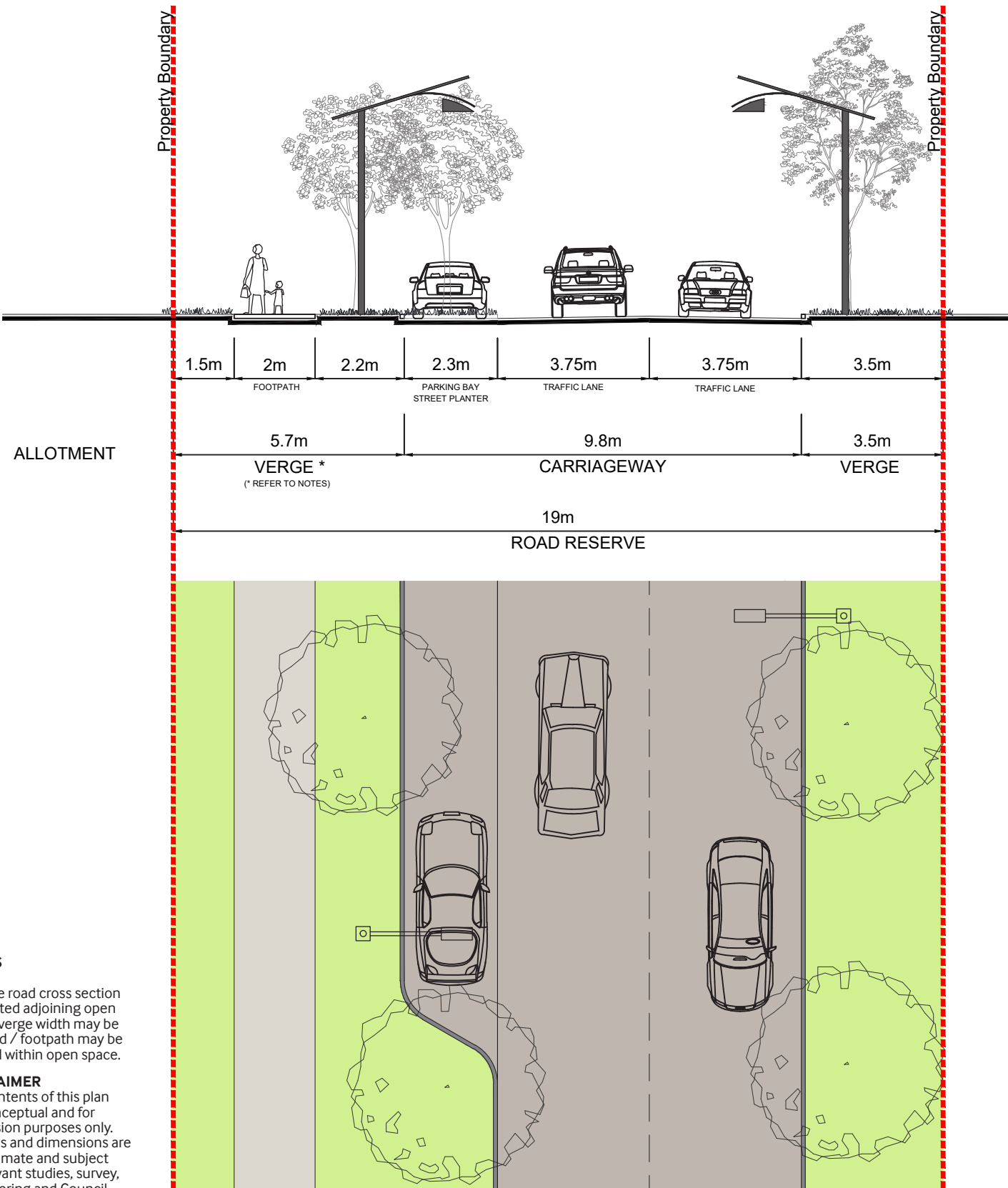
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NEIGHBOURHOOD CONNECTOR L

19m



NOTES

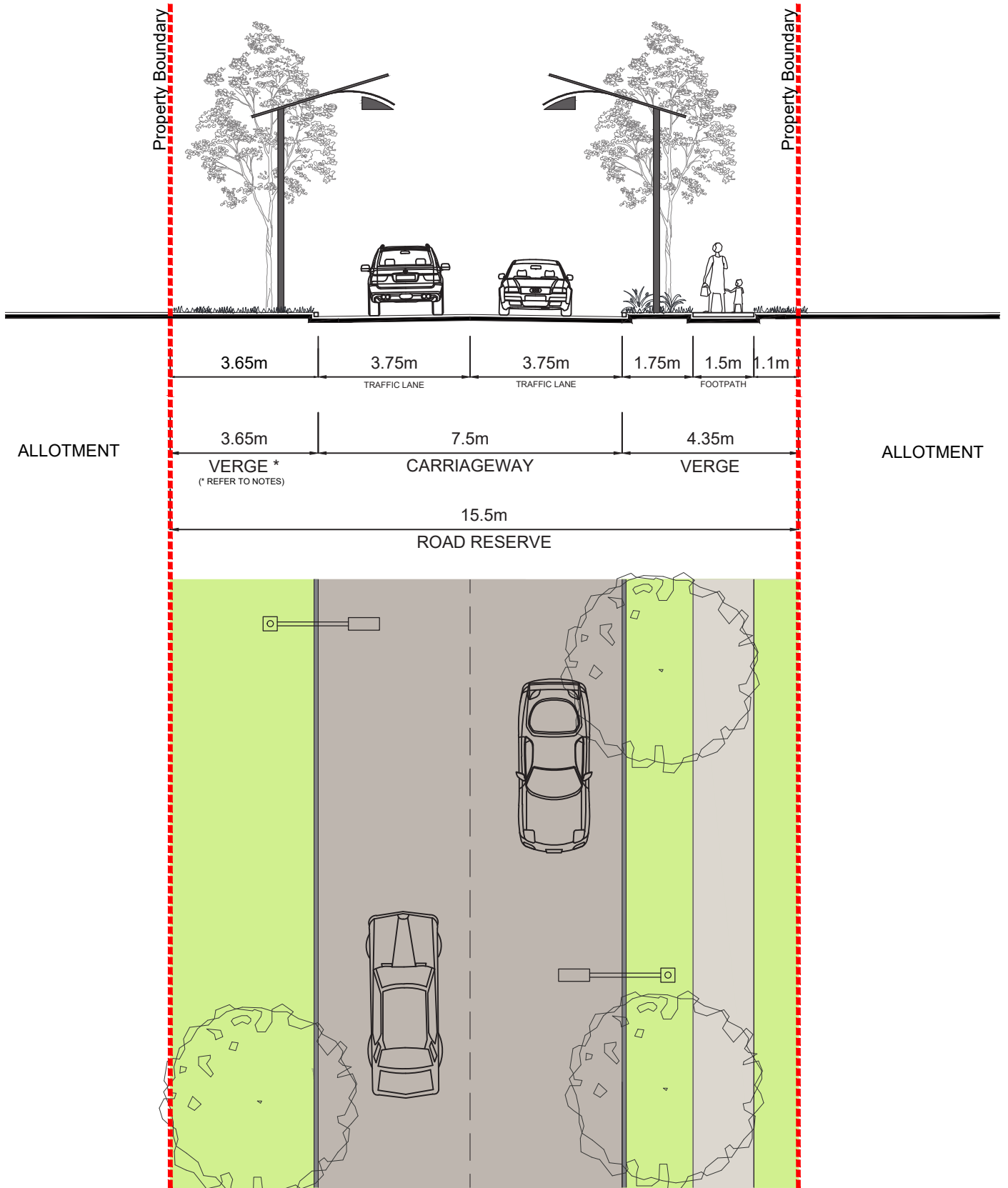
* Where road cross section is adopted adjoining open space, verge width may be reduced / footpath may be located within open space.

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ACCESS STREET P

15.5m



AURA LAKES - PRECINCT 18 (PART) ROAD CROSS SECTIONS

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DATE: 18 JUNE 2024
CLIENT: STOCKLAND
DRAWN BY: MM
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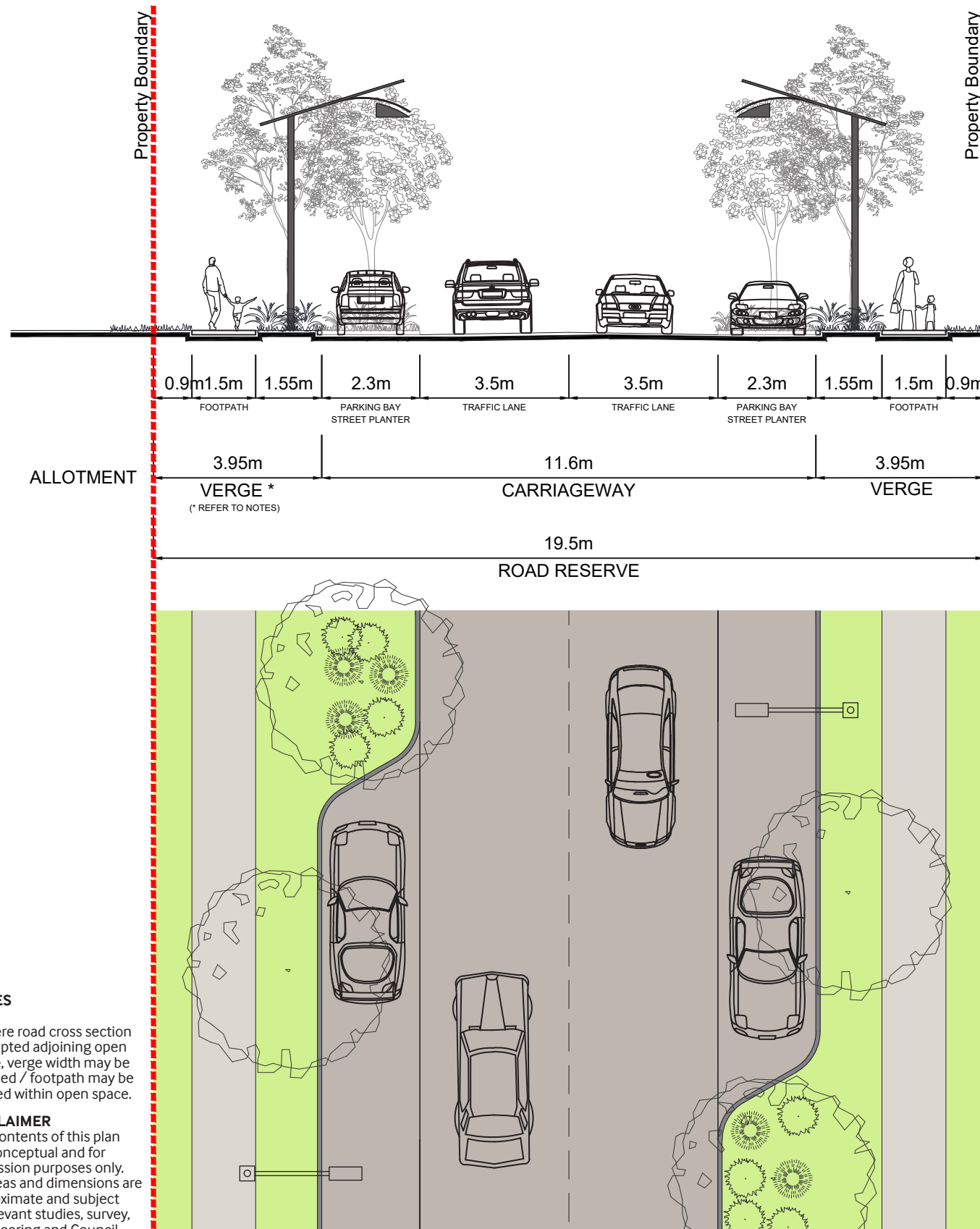
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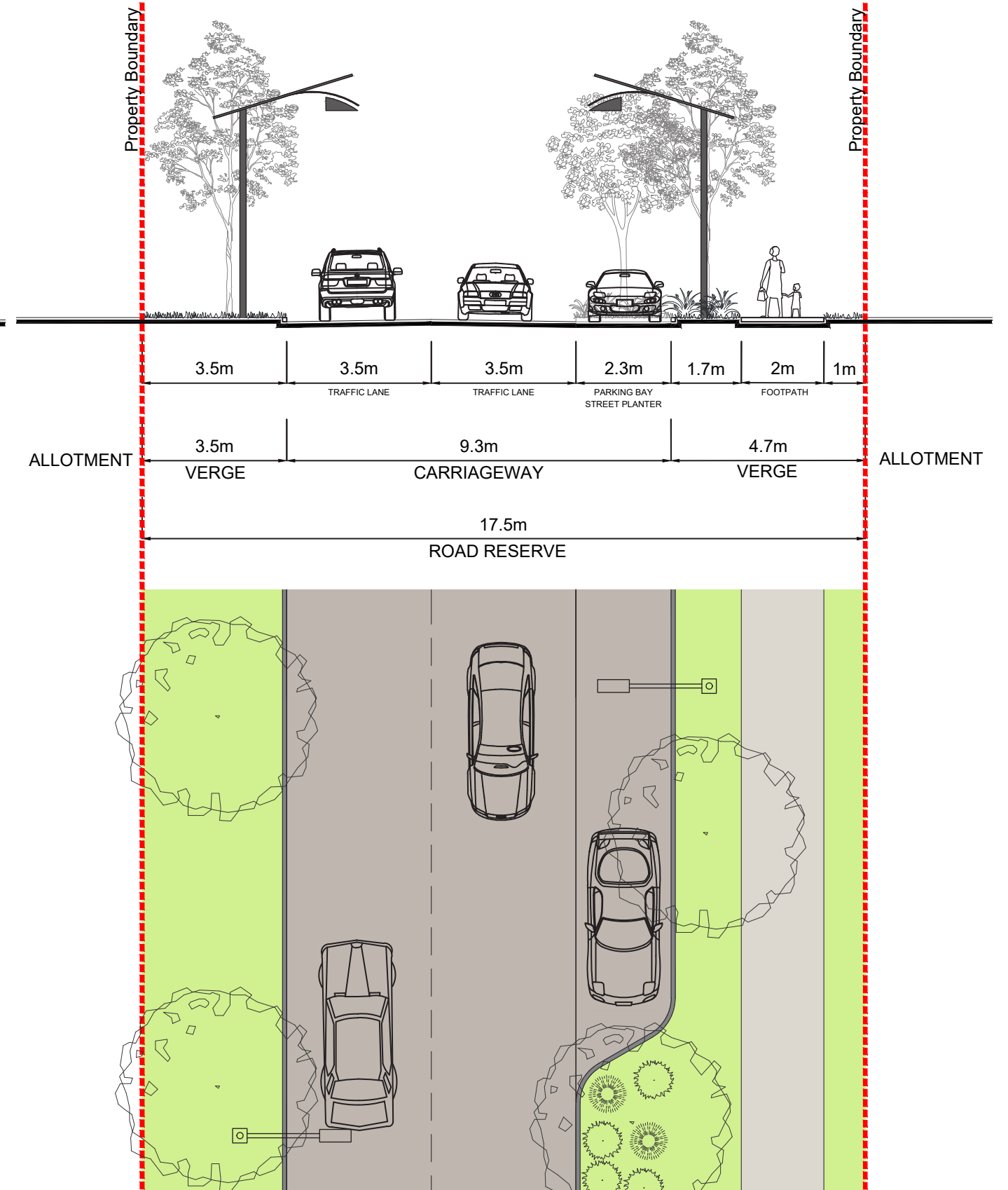
URBAN ACCESS A1

19.5m



URBAN ACCESS A2

17.5m



NOTES

* Where road cross section is adopted adjoining open space, verge width may be reduced / footpath may be located within open space.

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AURA LAKES - PRECINCT 18 (PART) ROAD CROSS SECTIONS

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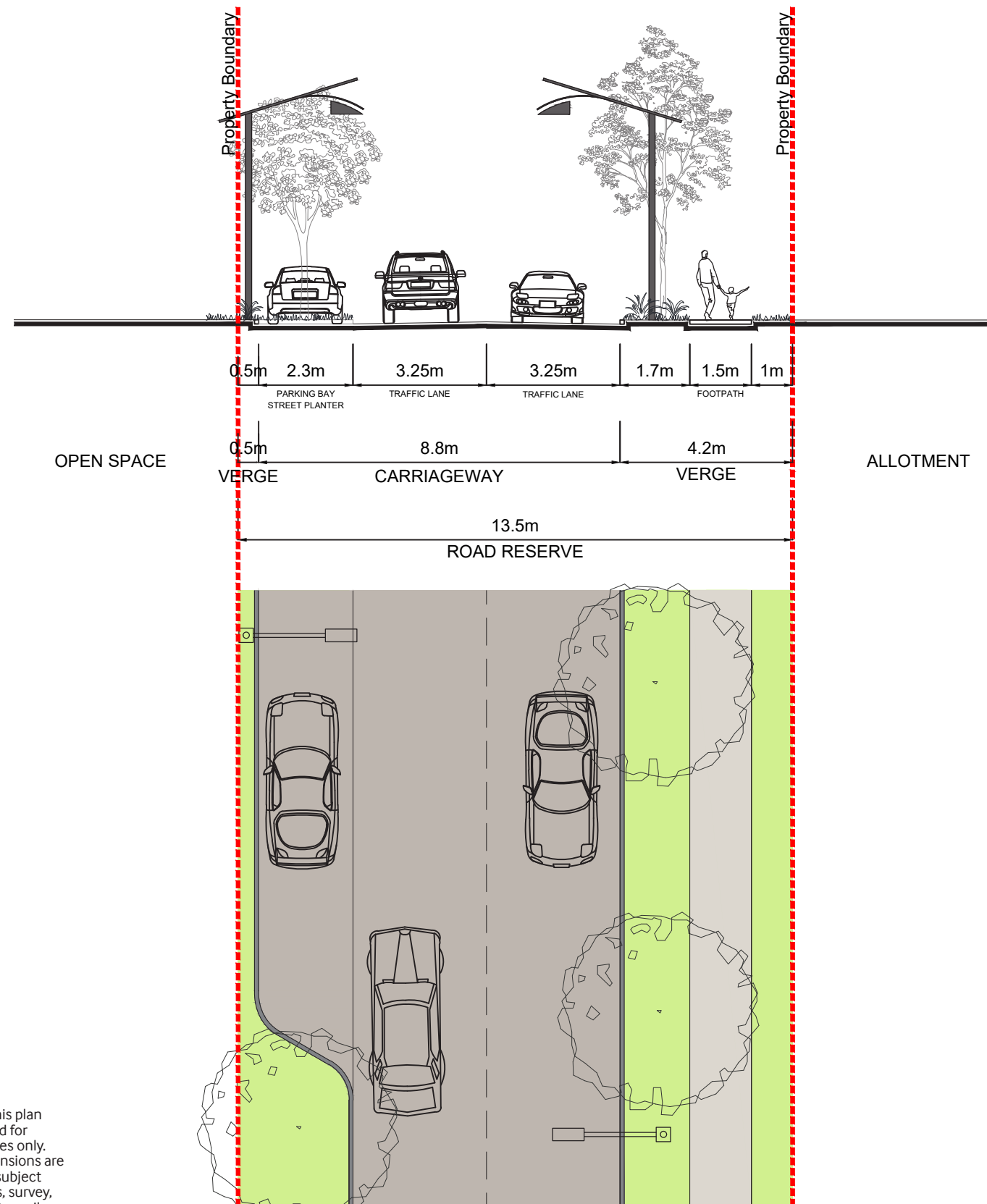
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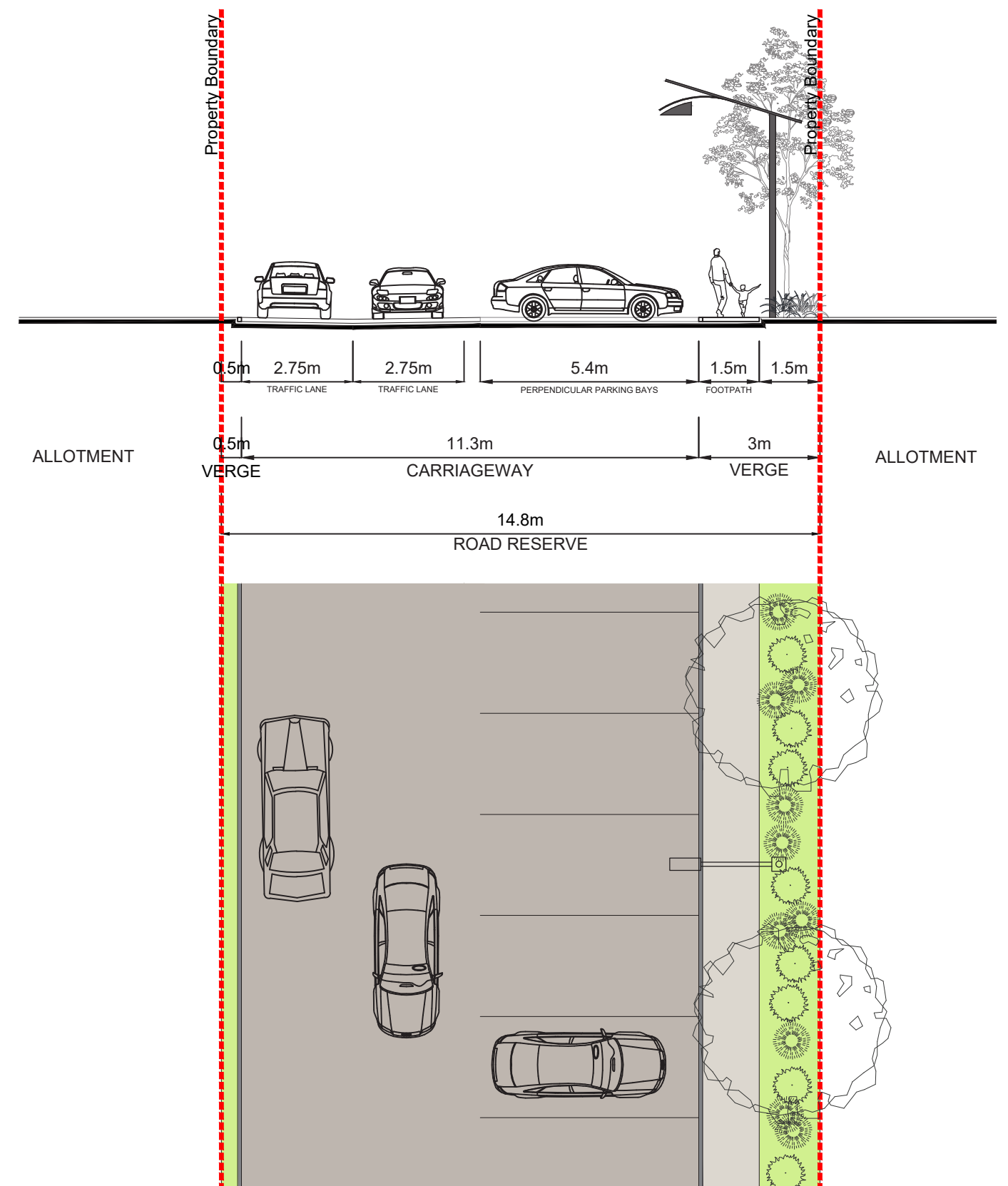
ACCESS STREET ESPLANADE

13.5m



LANEWAY LINK

14.8m



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AURA LAKES - PRECINCT 18 (PART) ROAD CROSS SECTIONS

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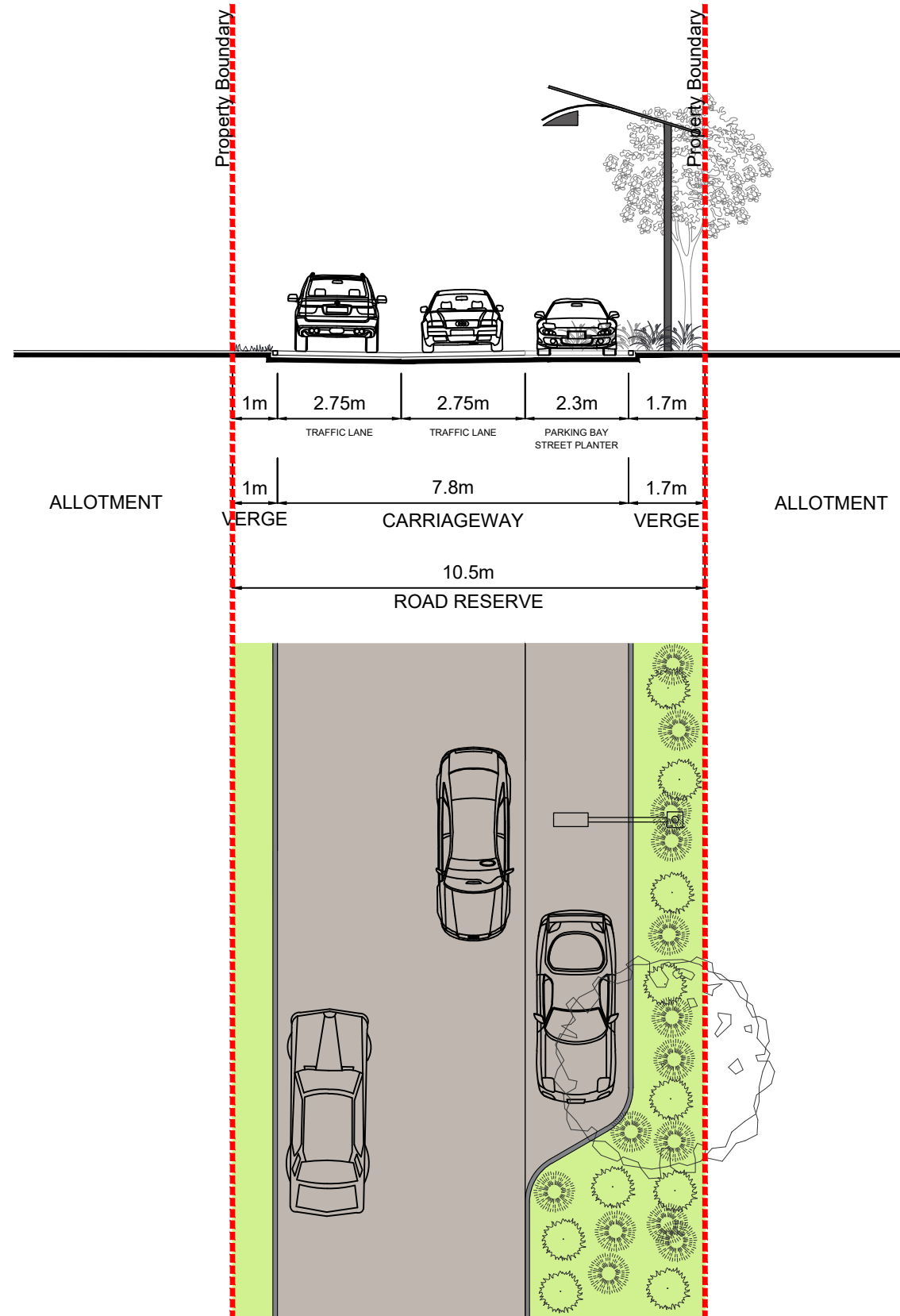
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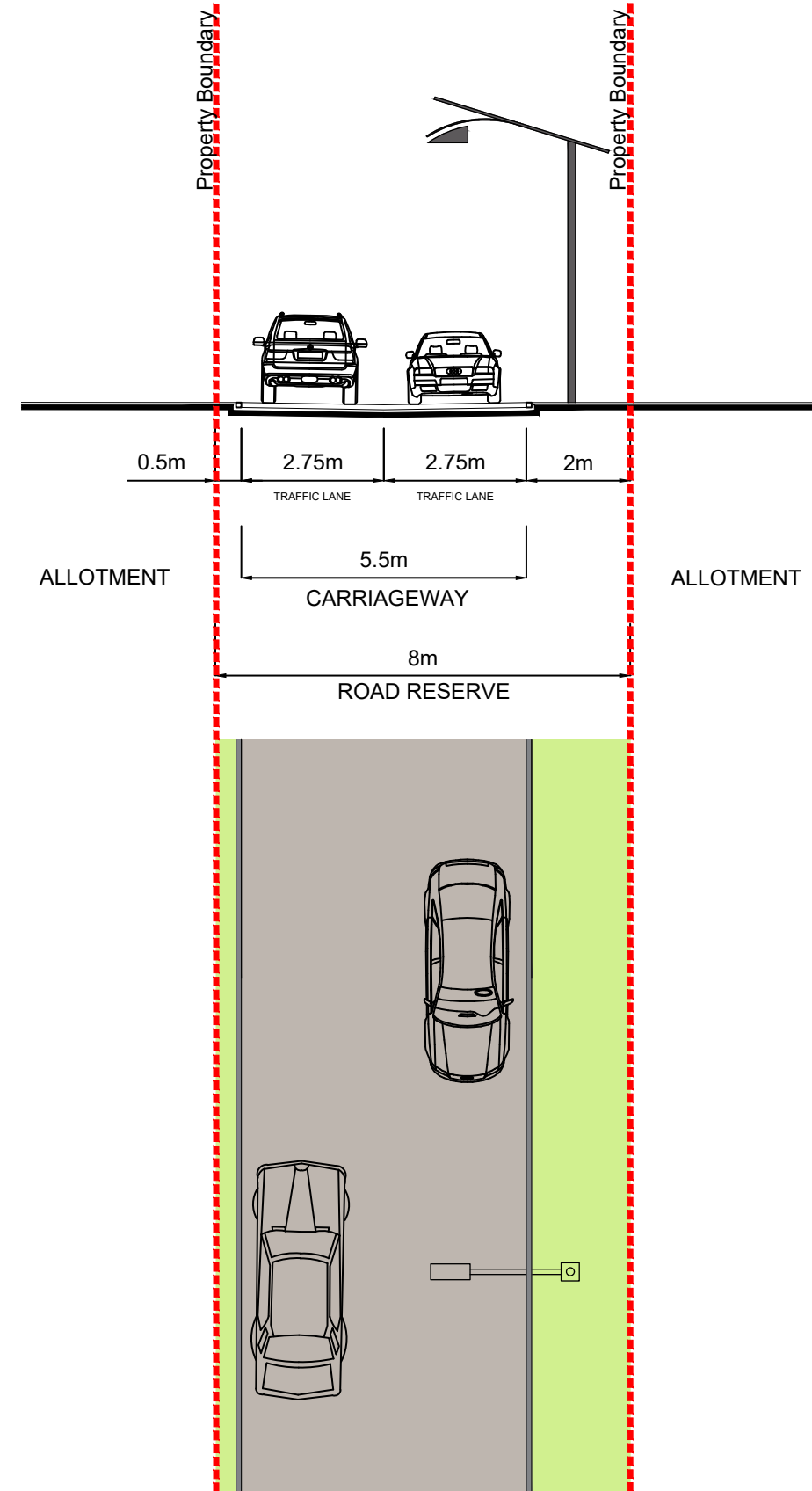
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0 2 4 6 8 10 1 : 150 @ A3

LANEWAY 10.5m



LANEWAY 8m



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AURA LAKES - PRECINCT 18 (PART) ROAD CROSS SECTIONS

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DATE: 18 JUNE 2024
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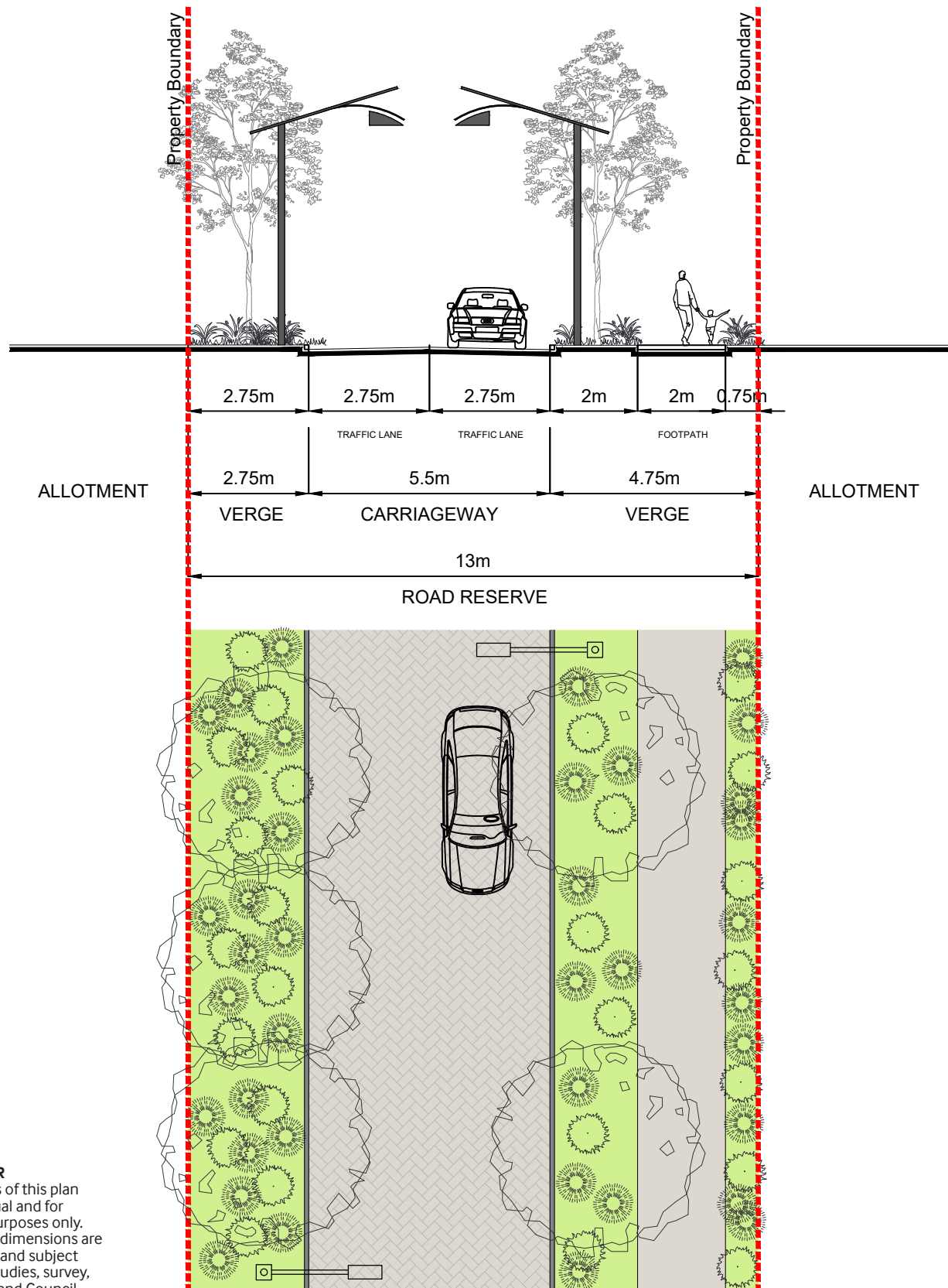
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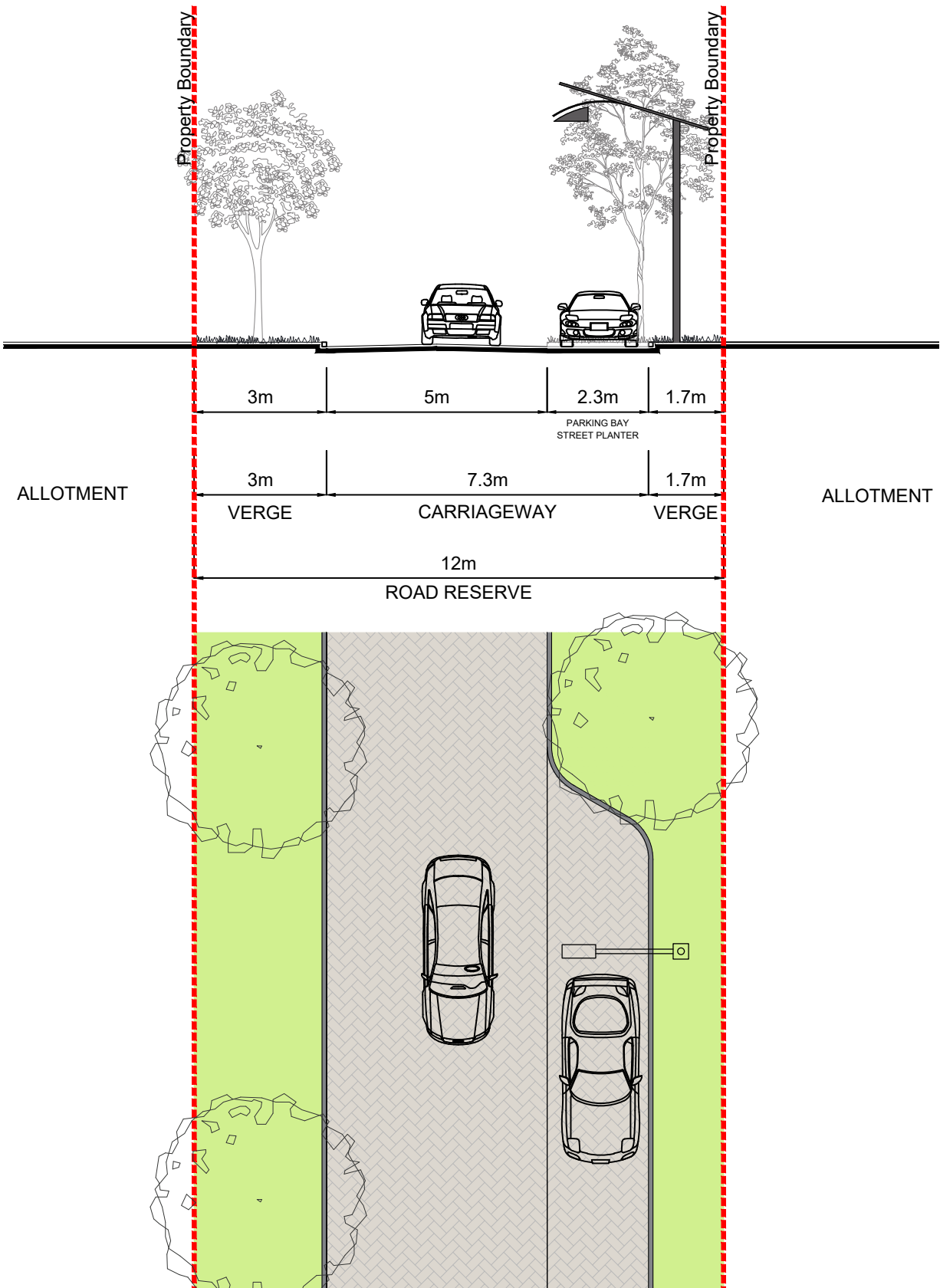
SHARED VEHICULAR / PEDESTRIAN ACCESS (LANEWAY)

13m



SHARED VEHICULAR / PEDESTRIAN ACCESS (DRIVEWAY)

12m



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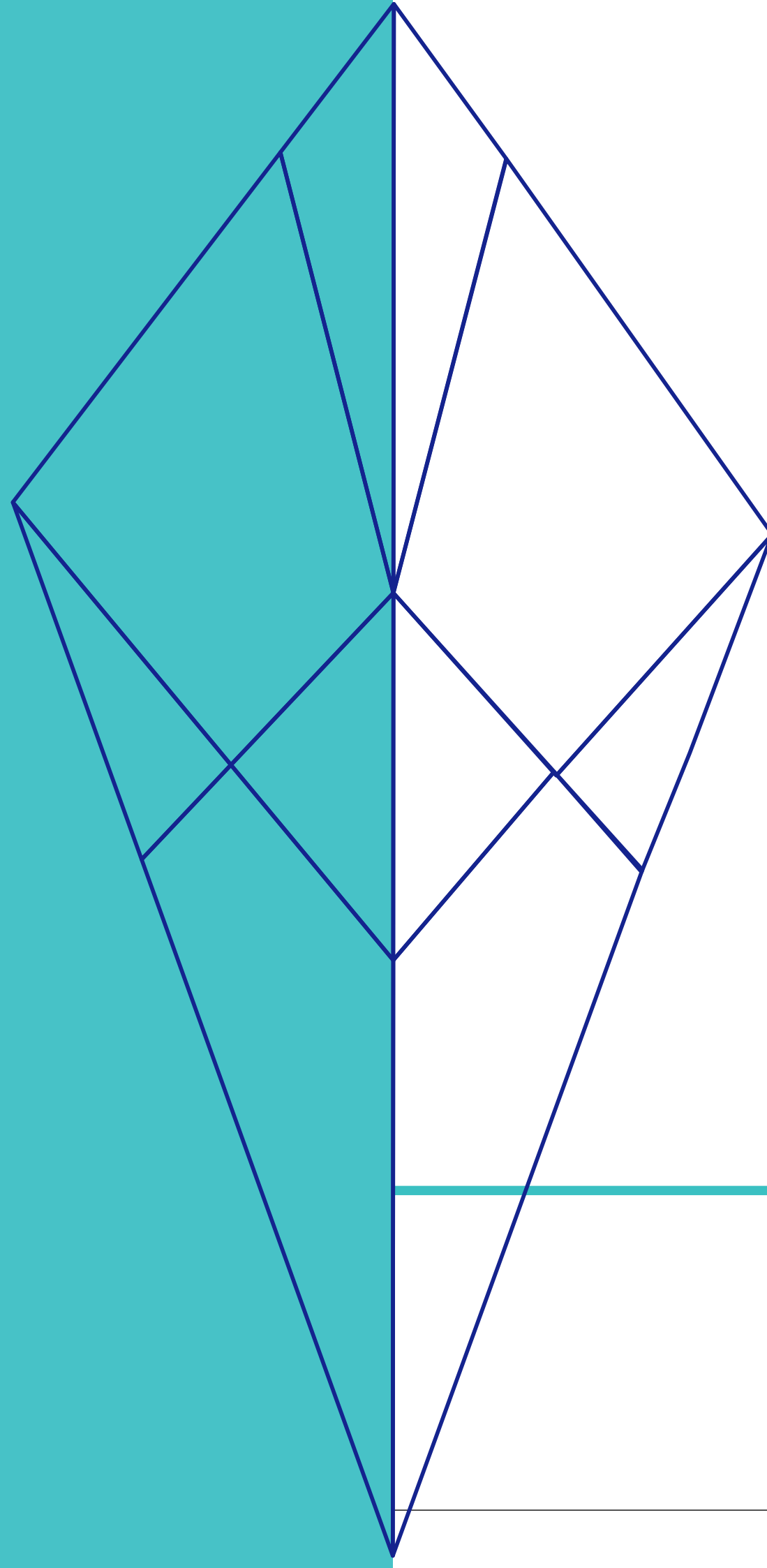
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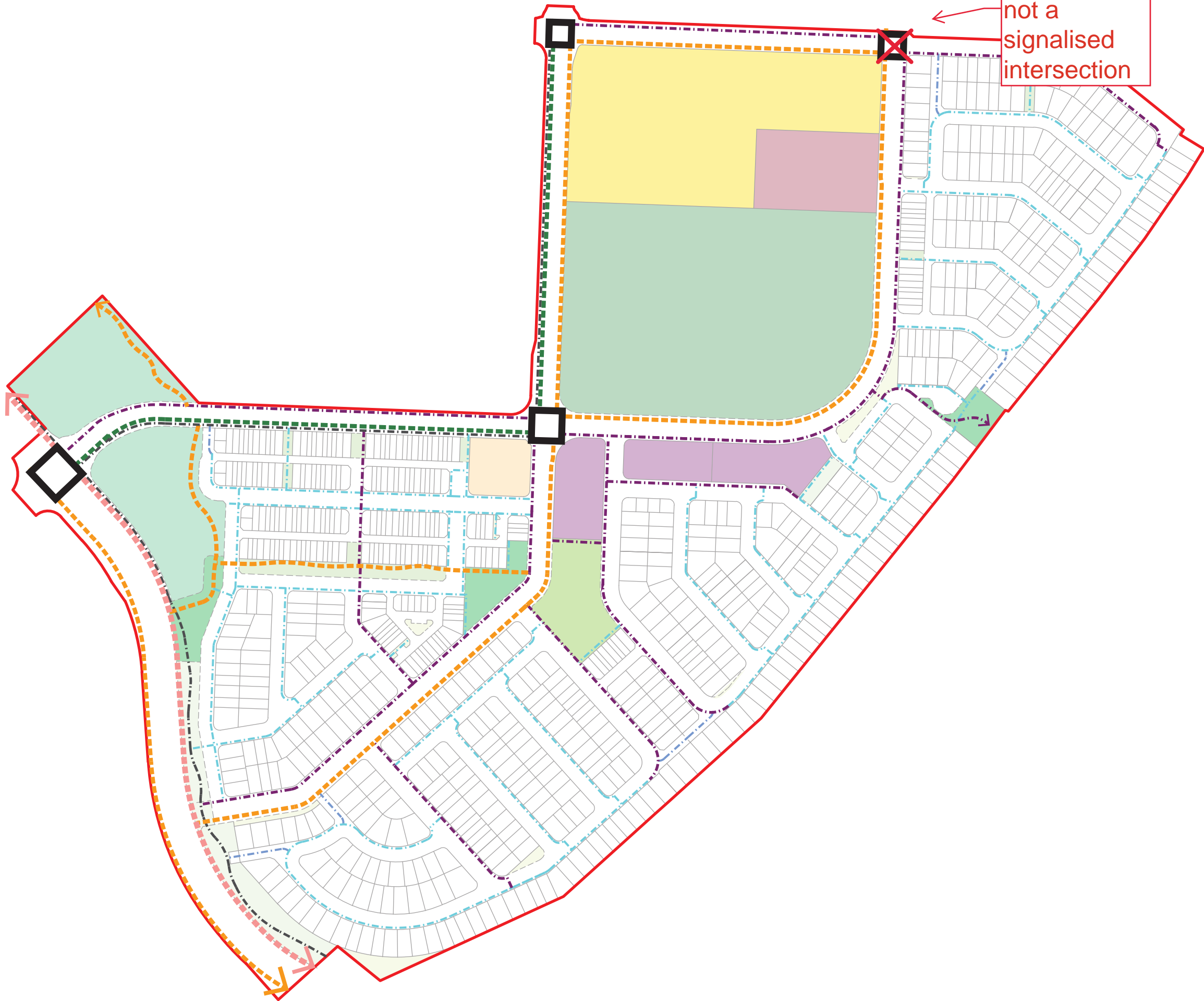


APPENDIX F CONNECTIVITY PLANS



LEGEND

- Application Boundary
- Signalised Intersection
- 4.0m Wide Contraflow / Dedicated Cycle Path
- 3.0m Wide Contraflow / Dedicated Cycle Path
- 3.0m Wide Shared Path
- 2.0m Wide Pedestrian Path
- 1.8m Wide Pedestrian Path
- 1.5m Wide Pedestrian Path
- Shared Driveway Pedestrian Path



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Pathway alignments through open space indicative only and subject to detailed design.

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DATE:

CLIENT:

DRAWN BY:

CHECKED BY:

AU12885-19D

19 JUNE 2024

STOCKLAND

JC/MD

MD

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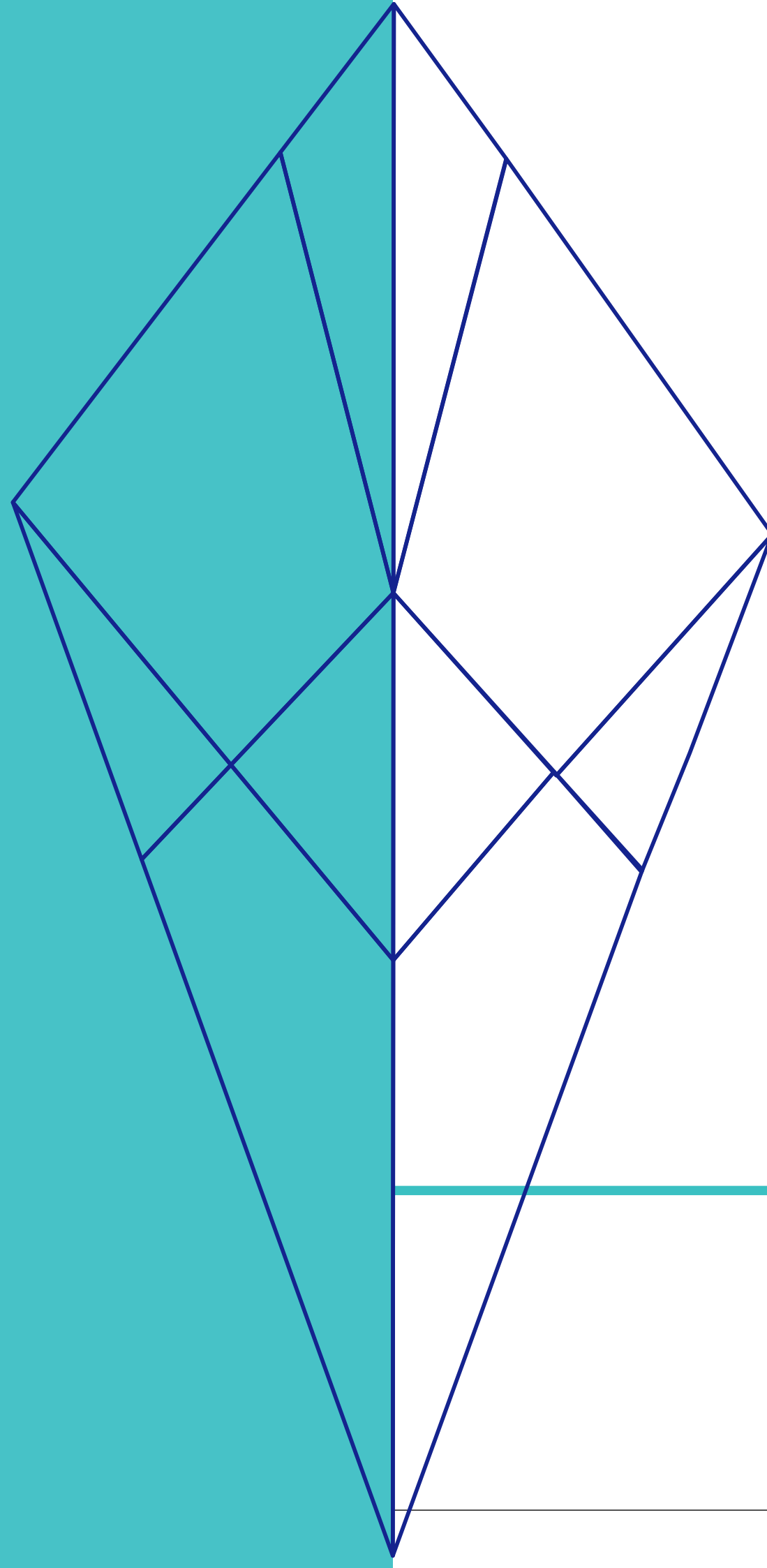
1 : 4,000 @ A3

AURA LAKES - PRECINCT 18 (PART)
CONNECTIVITY PLAN

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APPENDIX G PUBLIC TRANSPORT PLANS



LEGEND

- Application Boundary
- Signalised Intersection
- Public Bus Route
- School BUZ Route
- Indicative Bus Stop Location (Indented Bay)
- Indicative School Bus Stop Location (Within School)
- Indicative School BUZ Stop Location (Indented Bay)
- 400m Walking Catchment from Bus Stop

not a
signalised
intersection

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AU12885-20C

19 JUNE 2024

STOCKLAND

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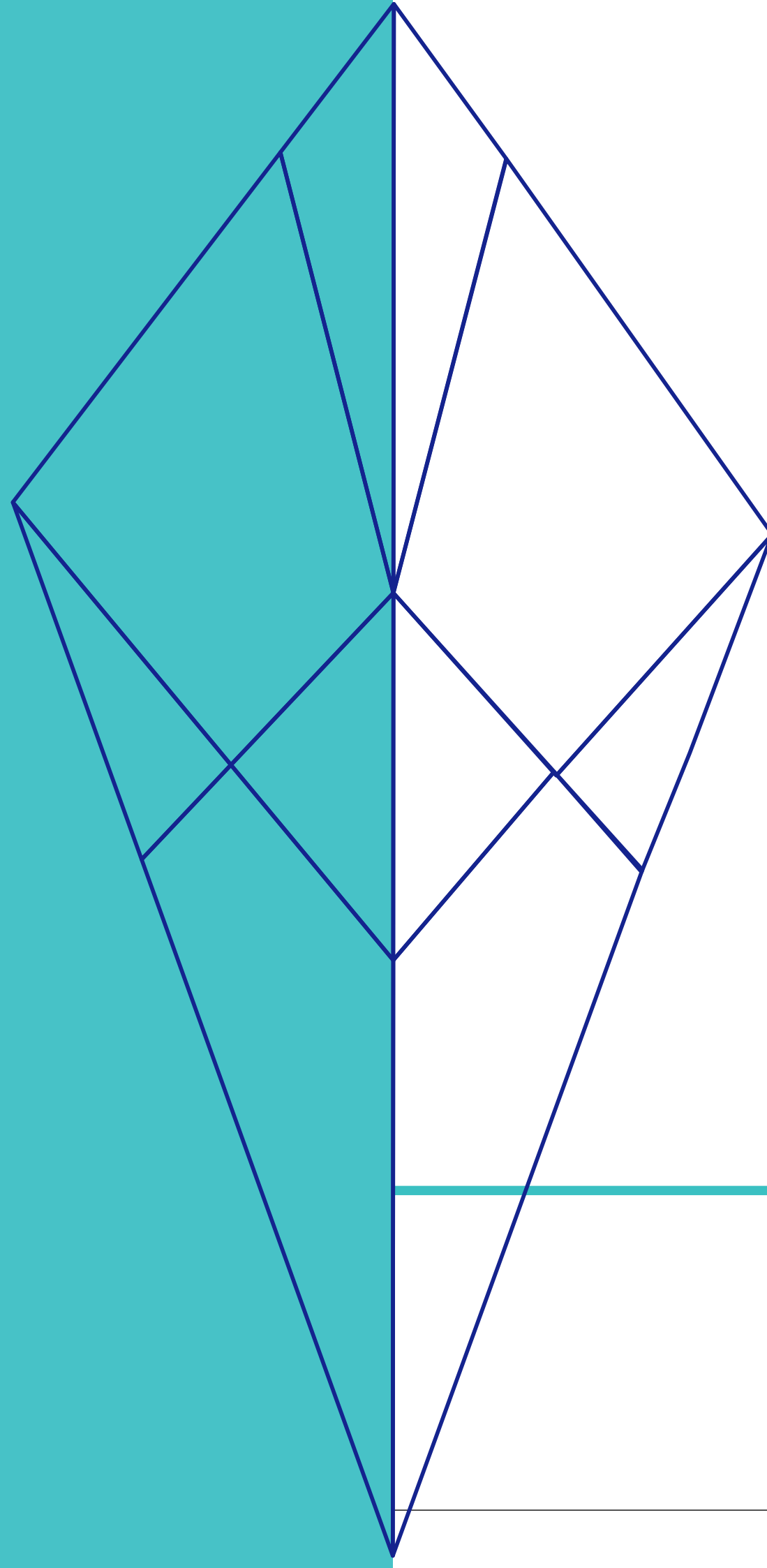
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AURA LAKES - PRECINCT 18 (PART)
PUBLIC TRANSPORT PLAN

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APPENDIX H APPROVED ADVERTISING DEVICES

NOTES

1.

Proposed Advertising Devices must be of materials and colours which complement the design theme of the residential estate and present a visually attractive appearance to the public.
2.

Unless otherwise agreed to in writing by the DSDMIP, the advertising devices nominated on this plan are to be considered temporary only and are not permitted to remain longer than:

a.

Fifteen (15) years from the date the approval is granted where a Billboard Sign; or

b.

Ten (10) years from the date the approval is granted or once the last residential lot is sold within the estate, whichever is the earliest, where a Pylon Identification Sign (10 year).

After these times, the Advertising Devices must be removed.
3.

Advertising Devices must be:

a.

Maintained to not cause disturbance to the occupants of nearby developments; and

b.

Located and designed to not create a nuisance or potential hazard to pedestrians.
4.

Construction of Advertising Devices is limited to 6:30am–6:30pm Monday to Saturday, and may not occur outside of these hours, on Sundays or on public holidays.
5.

Advertising Devices are to be consistent with the scale and design of the existing buildings and other works on the site and in the locality, and complement the local streetscape; and where appropriate, to reflect the character of the area.
6.

An Estate Entry Sign:

a.

Is placed at the entrance of an estate;

b.

Is set at or within 500mm of ground level;

c.

Is maintained as a freestanding structure in a landscaped environment;

d.

Does not obstruct pedestrian/cycle access to the estate;

e.

Signface area does not exceed 50m²;

f.

Is a maximum height of 6 metres.
7.

An Entry Statement:

a.

Is set at or within 500mm of ground level;

b.

No more than 2.0m high;

c.

Is mounted as either a free-standing structure or as part of a boundary fence wall in a landscaped environment;

d.

Does not exceed a maximum sign facing area of 10m².
8.

A Billboard Sign or Pylon Identification Sign is permitted where complying with the following Criteria:

a.

Is mounted as freestanding structure in a landscape environment;

b.

Is designed and treated in such a way that the supporting framework and the back of the signface area blend with the surrounding streetscape or field a view;

c.

Has a maximum thickness not exceeding 75mm per metre of height above ground level; and

d.

Is permitted up to a maximum height of 15 metres and a maximum signface area of 43m² per signface;
9.

A Third Party Sign is permitted to be erected on land owned or under the control of Stockland, and intended to advertise the emerging community of Caloundra South.
10.

H Frame Advertising Device;

a.

does not exceed 1200 x 2440mm in size;

b.

may be single or double sided;

c.

must consist of ACM face with a powder-coated ali fabricated structure finish;

d.

four (4) H frames located on Neighbourhood Connector N1 in Stage 2 to be removed upon closure of interim access to Aura Boulevard.

LEGEND

- Application Boundary
- Estate Entry Sign
- Entry Statement
- Pylon Identification Sign (10 year duration)
- Temporary Billboard (to be illuminated)
- Lot 8007 (Allotment for Temporary Billboard)
- H-Frames (10 year duration)



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AU12885-21C

DATE:

19 JUNE 2024

CLIENT:

STOCKLAND

DRAWN BY:

JC/MD

CHECKED BY:

MD

0255075100125150175200250

1 : 4,000 @ A3

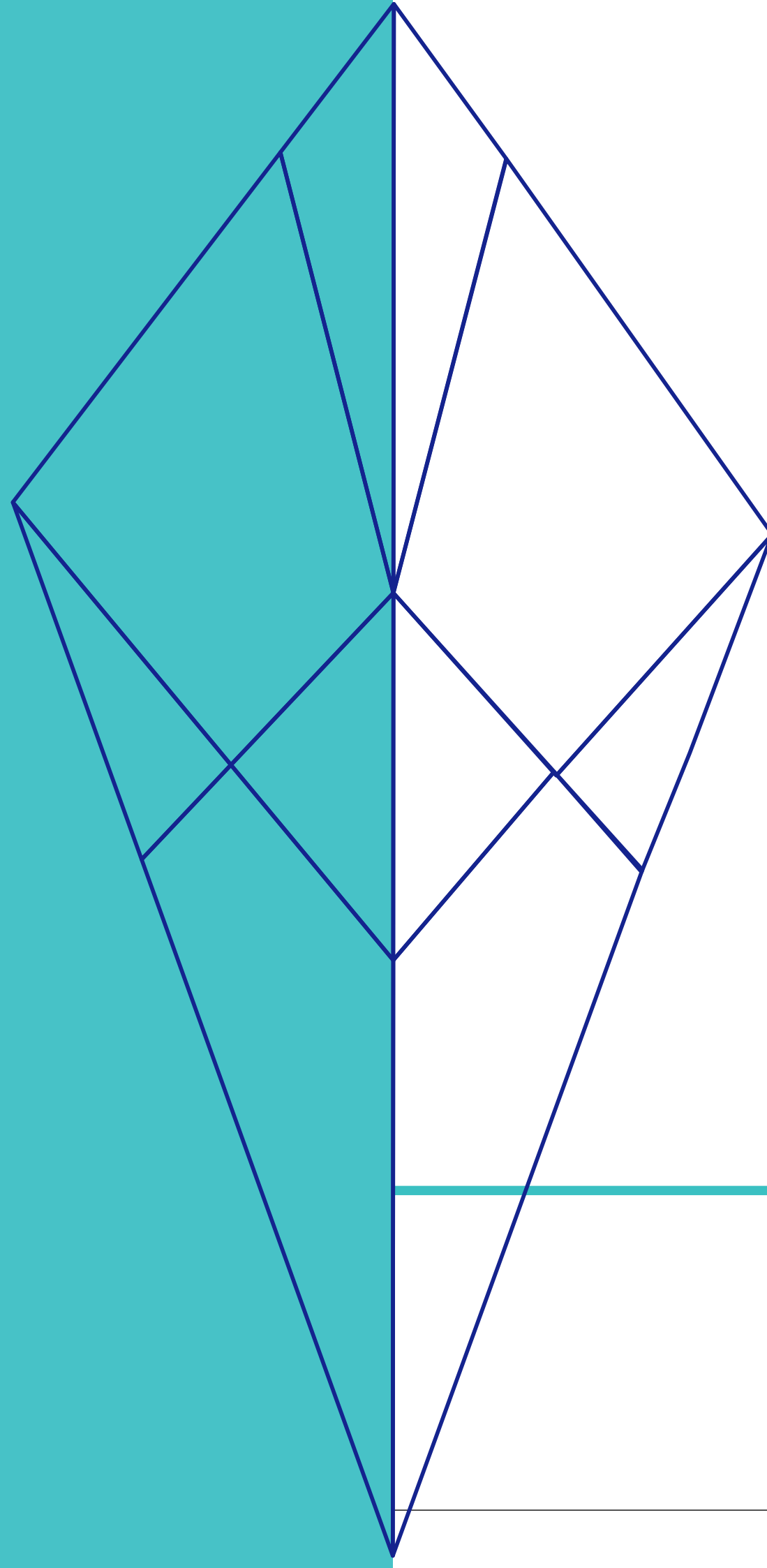
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INDICATIVE PROPOSED ADVERTISING DEVICES

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APPENDIX I OPEN SPACE NETWORK PLAN

| LOCAL CATEGORY PARK AREA REQUIREMENTS | | | |
|---|---|---------------|-----------|
| TYPE | POPULATION RATE (average people per household) | PRECINCT 18.1 | |
| House | 2.6 | 744 dw | 1,935 ppl |
| Duplex | 2.2 | 10 dw | 22 ppl |
| Attached Dwelling | 1.8 | 169 dw | 305 ppl |
| TOTAL | | 923 dw | 2,262 ppl |
| TOTAL LOCAL OPEN SPACE REQUIREMENT (1.3ha PER 1,000 PERSONS) | | 2.941 ha | |

| LOCAL CATEGORY PARK AREA PROVISIONS | |
|---|---------------|
| PARK TYPE | PRECINCT 18.1 |
| <div></div> Local Linear Park | 1.494ha |
| <div></div> Local Recreation Park | 0.822ha |
| <div></div> Neighbourhood Recreation Park | 0.663ha |
| TOTAL | 2.979ha |

| OTHER OPEN SPACE AREA PROVISIONS | |
|---------------------------------------|---------------|
| PARK TYPE | PRECINCT 18.1 |
| <div></div> Neighbourhood Sports Park | 6.5ha |
| TOTAL | 6.5ha |

LOCAL PARK ALLOCATION NOTES

1. The local park provision is calculated at 1.3 hectares per 1,000 population.
2. All residential precincts comply with the minimum Neighbourhood Recreation Park provision requirement - 90 per cent of dwellings within a 400m catchment.

LEGEND

- Application Boundary
- 400m Park Catchment
- Future Neighbourhood Recreation Park



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19 JUNE 2024

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1 : 4,000 @ A3

AURA LAKES - PRECINCT 18 (PART)
OPEN SPACE NETWORK

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Fortitude Valley QLD 4006

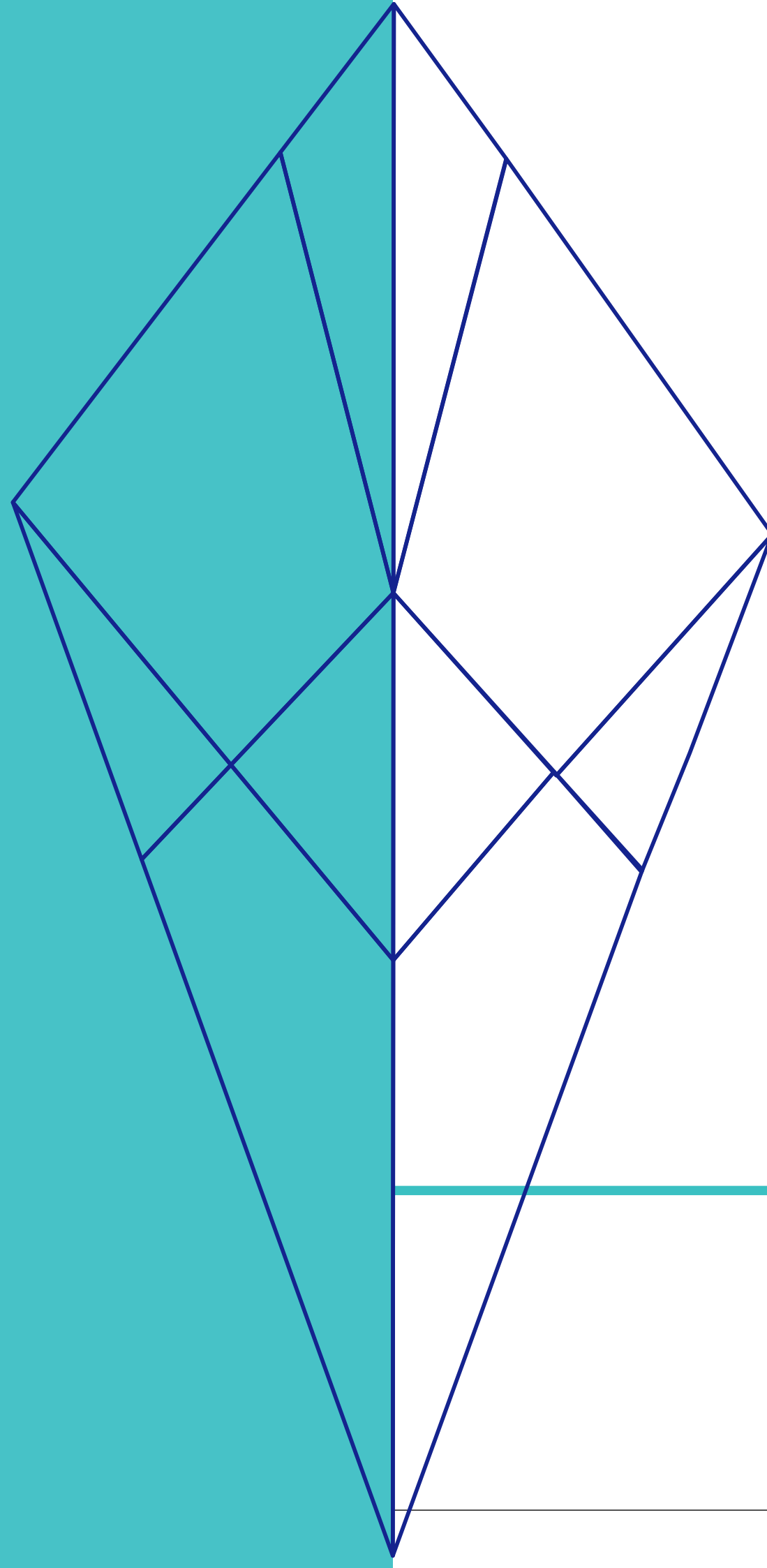
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APPENDIX J FIXED ELEMENTS PLAN

LEGEND

Application Boundary

LAND USES

- Residential
- School
- Emerging Community
- Child Care Centre
- Multiple Residential Sites
- Local Recreation Park
- Neighbourhood Recreation Park
- Future Neighbourhood Recreation Park
- Neighbourhood Sports Park
- Local Linear Park
- Drainage
- Indicative WSUD Basin

BUILDING HEIGHTS

- Maximum Number of Storeys

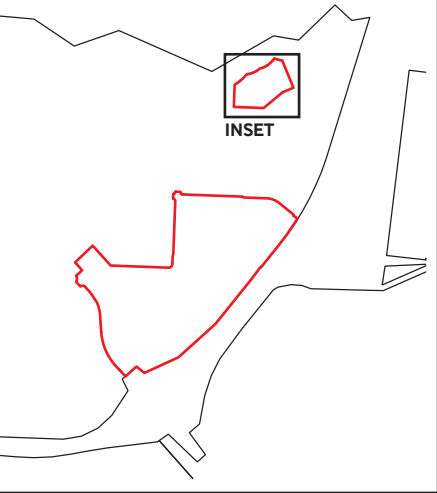
ROAD NETWORK HIERARCHY

- Signalised Intersection
- Sub-Arterial 3A - 35.5m
- Trunk Connector C2 - 25.0m
- Trunk Connector G - 25.9m
- Neighbourhood Connector N1 - 25.0m
- Neighbourhood Connector N2 - 25.0m
- Interim Vehicular Access

PUBLIC TRANSPORT

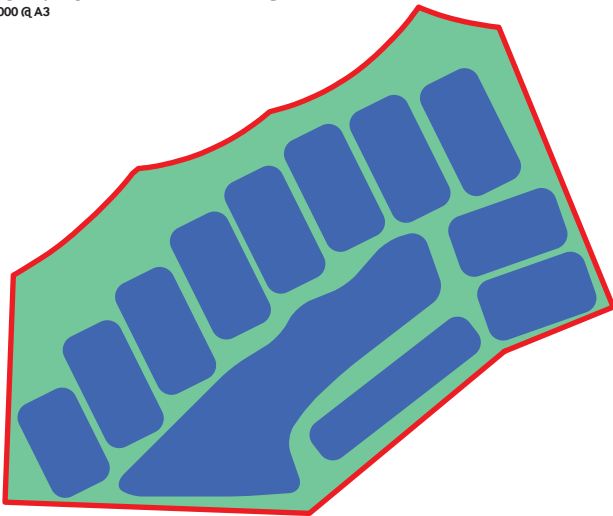
- Indicative Bus Stop Location (Indented Bay)
- Indicative School Bus Stop Location (Within School)
- Indicative School BUZ Stop Location (Indented Bay)

KEY MAP
(NOT TO SCALE)



INTERIM
VEHICULAR
ACCESS

INSET: NORTHERN DRAINAGE
1:4000 @ A3



PLAN REF: **AU12885-23B**
DATE: 21 JUNE 2024
CLIENT: STOCKLAND
DRAWN BY: JC/MD
CHECKED BY: MD



AURA LAKES - PRECINCT 18 (PART)
FIXED ELEMENTS PLAN

0 25 50 75 100 125 150 175 200 250 1 : 4,000 @ A3

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