

Rename as: William Street and Queens Wharf Road Retaining Walls and Victoria Bridge Abutment (Northern) Conservation Management Plan

POD VOLUME 3: ATTACHMENT D.9: WILLIAM STREET AND QUEENS WHARF ROAD RETAINING WALLS CONSERVATION MANAGEMENT PLAN

PREPARED BY: URBIS PTY LTD



QUEEN'S WHARF BRISBANE

Any items struck out
are not approved.

AMENDED IN RED

By: K McGill

Date: 20 December 2017



PLANS AND DOCUMENTS
referred to in the PDA
DEVELOPMENT APPROVAL

Approval no: DEV2017/846

Date: 21 December 2017



DATE OF ISSUE: 24.11.2017

REVISION: 13

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TABLE OF CONTENTS

1.	Introduction	1
1.1.	Background	1
1.2.	Queen’s Wharf Brisbane	1
1.3.	Purpose	2
1.3.1.	Former Victoria Bridge Abutment (Northern)	2
1.4.	Site Location	3
1.5.	Methodology	3
1.6.	Limitations	3
2.	Historical Context	6
2.1.	William Street and Queens Wharf Road Retaining Walls	6
2.1.1.	William Street Retaining Wall and Queens Wharf Road Retaining Walls	6
2.1.2.	Abutment of the Second Victoria Bridge	8
2.2.	Chronological Overview	9
2.3.	Historical Themes	10
3.	Physical Context	11
3.1.	Views, Landscape and Seeting	11
3.2.	William Street Retaining Wall	11
3.3.	Queens Wharf Road Retaining Wall	12
3.4.	WWII Air Raid Shelter	12
3.5.	Abutment of the Second Victoria Bridge	13
3.6.	Curtilage	13
3.7.	Condition and Integrity	14
3.7.1.	Condition	14
3.7.2.	Integrity	14
3.8.	Historical Archaeological Potential	14
3.8.1.	Summary	14
4.	Cultural Significance	16
4.1.	About Cultural Significance	16
4.2.	Significance Assessment	16
4.3.	Statement of Significance	21
4.4.	Levels of Significance	21
4.5.	Schedule of Significant Elements	22
5.	Key Issues	27
5.1.	Heritage Listings	27
5.2.	Statutory Obligations	27
5.2.1.	Economic Development Act 2012	27
5.2.2.	Queensland Heritage Act 1992	28
5.2.3.	Building Code of Australia	28
5.2.4.	Disability Discrimination Act 1992	28
5.3.	Non-Statutory Considerations	28
5.3.1.	National Trust of Australia (Queensland)	28
5.4.	Current Proposal	29
5.5.	Management Framework	29
5.5.1.	Management Plans and Guidelines	29
5.5.2.	External Stakeholders	29
5.6.	Opportunities	29

5.7.	Visitor and Public Access	29
5.8.	Threats	29
6.	Conservation Policy	30
6.1.	General Approach	30
6.2.	Understanding Significance	31
6.3.	Settings and View	31
6.4.	Conservation of Fabric	32
6.4.1.	Retaining Significant Elements	32
6.4.2.	Elements of Little Significance	32
6.4.3.	Neutral Elements	32
6.4.4.	Intrusive Elements	32
6.5.	Repairs and Maintenance	32
6.5.1.	Repairing Significant Elements	33
6.6.	Future Use	33
6.6.1.	Adaptive Reuse	33
6.6.2.	Construction Management	33
6.7.	Alterations and New Works	34
6.7.1.	Signage	34
6.8.	Statutory Requirements	34
6.9.	Historical Archaeology	35
6.10.	Interpretation	35
6.11.	Archival Recording	35
6.12.	Endorsement and Review	36
7.	Implementation	37
7.1.	Implementation Strategies	37
8.	References	38
9.	Abbreviations and Definitions	39

FIGURES:

Figure 1 – Queen’s Wharf Brisbane Precinct Plan	1
Figure 2 –Heritage Curtilage and place components	4
Figure 3 – Heritage Curtilage of the Former Victoria Bridge Abutments	5
Figure 4 – William Street c.1873	6
Figure 5 – Early timber fence along Queens Wharf Road c.1878	6
Figure 6 – Bus Shelter in 1950	7
Figure 7 – William Street in 1963	7
Figure 8 – During the 1974 flood	8
Figure 9 – Victoria Bridge, 1897. Abutment remnants of that at left of bridge.	8
Figure 10 – Lift adjacent to Bridge Abutment, December 2015	9
Figure 11 – Views and visual connectivity	11
Figure 12 – William Street retaining wall	11
Figure 13 – Queens Wharf Road retaining wall	12
Figure 14 – Looking Towards WWII Air Raid Shelter along Queens Wharf Road	13
Figure 15 – Top of WWII Air Raid Shelter Along William Street.....	13
Figure 16 – Victoria Bridge abutment from Bicentennial Bikeway	13
Figure 17 – Victoria Bridge abutment from stairs	13
Figure 18 – Historical Archaeological Potential	15

TABLES:

Table 1 – Chronological Overview.....	9
Table 2 – Historical Themes.....	10
Table 3 – Assessment of Heritage Significance.....	16
Table 4 – Levels of Significance.....	21

1. INTRODUCTION

1.1. BACKGROUND

Urbis has been engaged by Destination Brisbane Consortium (DBC) to prepare a Conservation Management Plan (CMP) for the William Street and Queens Wharf Road Retaining Walls, and the Former Victoria Bridge Abutment (northern), Brisbane.

The William Street and Queens Wharf Road Retaining Walls are recognised as a State Heritage Place and are entered in the Queensland Heritage Register (QHR; Place ID 600135). The Former Victoria Bridge Abutments (northern and southern) are also entered in the QHR (Place ID 600303).

The places are also included within the Brisbane City Council Local Heritage Overlay (William Street and Queens Wharf Road Retaining Walls and Victoria Bridge Abutment).

The places are sited within the Queen's Wharf Brisbane (QWB) Priority Development Area (PDA) (**Figure 1**). The QWB PDA comprises land bounded by the Brisbane River to the south, west and north-west; Queen Street to the north; George Street to the east and north-east; and Alice Street and the Riverside Expressway to the south-east. The area of the QWB PDA is approximately 26.8ha (13.5ha over land; 13.3ha over the river, excluding the bridge and landing area).

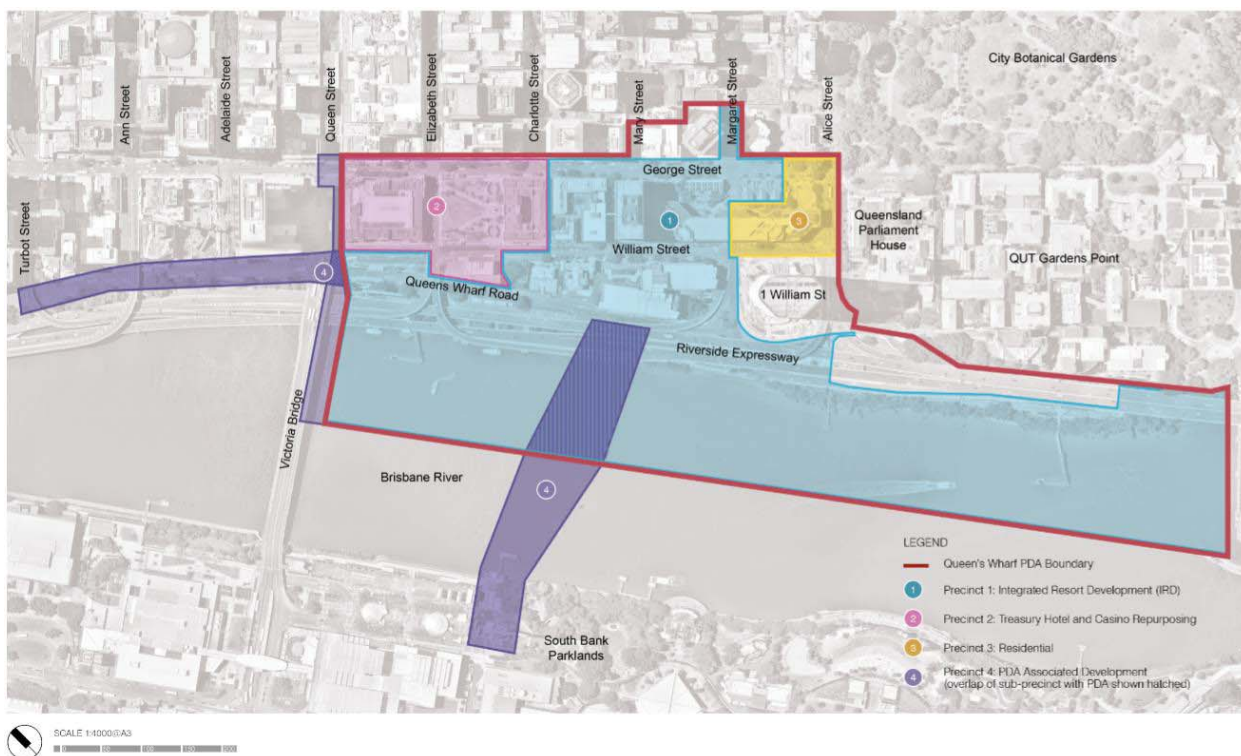


Figure 1 – Queen's Wharf Brisbane Precinct Plan

Source: Urbis 2017

The QWB PDA was declared by Economic Development Queensland (EDQ) on 28 November 2014, to facilitate the planning and delivery of the QWB Integrated Resort Development (IRD) and establish the necessary policy environment to support the intended development outcome for the site.

1.2. QUEEN'S WHARF BRISBANE

QWB is recognised as the birthplace of Brisbane, with European settlement taking place in this location almost 200 years ago. Much evidence of that history still survives in the precinct, in the streets, the various buildings and places, in the archaeological record, and the collective memory of generations of Queenslanders.

The cultural heritage significance of the QWB PDA is a result of the major government presence in the area, which was first established at this location in the mid-1820s when the Moreton Bay penal settlement was formed on the north bank of the Brisbane River. The PDA contains a range of fine government and private buildings from the early nineteenth century, the mid to late nineteenth century and the early twentieth century, and more modern buildings that demonstrate the evolution of the various generations of the government presence at this location in the centre of Brisbane.

The QWB PDA contains 13 Heritage Places and items that are entered in the QHR under the *Queensland Heritage Act 1992* (QHA). These places are well known and appreciated, with a high level of community and public support for their retention and continued use. They are as follows:

- Early Streets of Brisbane (Place ID 645611);
- The former Treasury Building (Place ID 600143);
- The former Land Administration Building (Place ID 600123), including the First World War Honour Board (Place ID 600117);
- The former State Library (Place ID 600177);
- ~~Queen's~~ **Queens** Gardens (Place ID 600112);
- The William Street and Queens Wharf Road retaining walls (Place ID 600135);
- The Commissariat Store, including the adjacent Miller Park (Place ID 600176);
- The former Victoria Bridge abutment (northern) (Place ID 600303);
- The former Government Printing Office (Place ID 600114);
- The former Department of Primary Industries (DPI) Building (National Trust House) (Place ID 601093);
- Harris Terrace (Place ID 600121); and
- The Mansions (Place ID 600119).

1.3. PURPOSE

This CMP has been prepared to comply with the QWB PDA Development Scheme, which requires CMPs to be prepared to provide guidance for the development and ensure the ongoing care and maintenance of heritage places.

The purpose of this CMP is to provide an understanding of the history and cultural significance of the places and provide guidance for the future conservation of the built form and site. The CMP provides a careful analysis of why the William Street and Queens Wharf Road retaining walls, and the Former Victoria Bridge Abutment (northern) are significant, policies on how to retain their significance, and conservation strategies to ensure their long-term viability.

Future development of the William Street and Queens Wharf Road retaining walls, and the former Victoria Bridge Abutment (northern), should be generally in accordance with this CMP.

1.3.1. Former Victoria Bridge Abutment (Northern)

It is noted that at the time of preparation of the QWB Development Scheme (January 2016), and commissioning of this CMP, the northern abutment was included as a place component in the heritage entry for the William Street and Queens Wharf Road Retaining Walls. As such, information about the item and policies for its management were included in this CMP.

As of April 2017, reference to this element has been removed from the entry, and the northern abutment was included in the QHR entry 600303 *Former Victoria Bridge Abutments*. However, given that the northern abutment lies within the QWB PDA, to manage the heritage values of the item it is still referenced in this CMP, where appropriate.

1.4. SITE LOCATION

The William Street and Queens Wharf Road retaining walls are located between Queens Wharf Road and William Street along the north bank of the Brisbane River (**Figure 2**). The majority of the place is included within Lot 209 SP289465.

The QHR entry comprises the following elements, which are identified within **Figure 2**:

- William Street Retaining Wall (1889);
- Queens Wharf Road Retaining Wall (1936);
- Men's Toilets (1920s-1930s);
- WWII Air Raid Shelter (c1941);
- Bus Shelter (1944) (now demolished); and
- Women's Toilets (1970s) (now demolished).

The former Victoria Bridge Abutment (northern) is in Lot 13 SP180753 (refer **Figure 3**).

1.5. METHODOLOGY

This CMP has been prepared in accordance with:

- Guideline: Conservation Management Plans (DEHP 2015);
- Australia ICOMOS Charter for Places of Cultural Significance, The Burra Charter, 2013 (Australia ICOMOS 2013) (the Burra Charter); and
- The Conservation Plan (Kerr 2000).

Conservation policies have been summarised at the end of **Section 6** to assist with future assessment.

Existing literature about the place, including previous heritage studies and the Department of Environment and Heritage Protection (DEHP) entry in the heritage register for the site have been reviewed. The information in these documents has been utilised and incorporated into this report where appropriate.

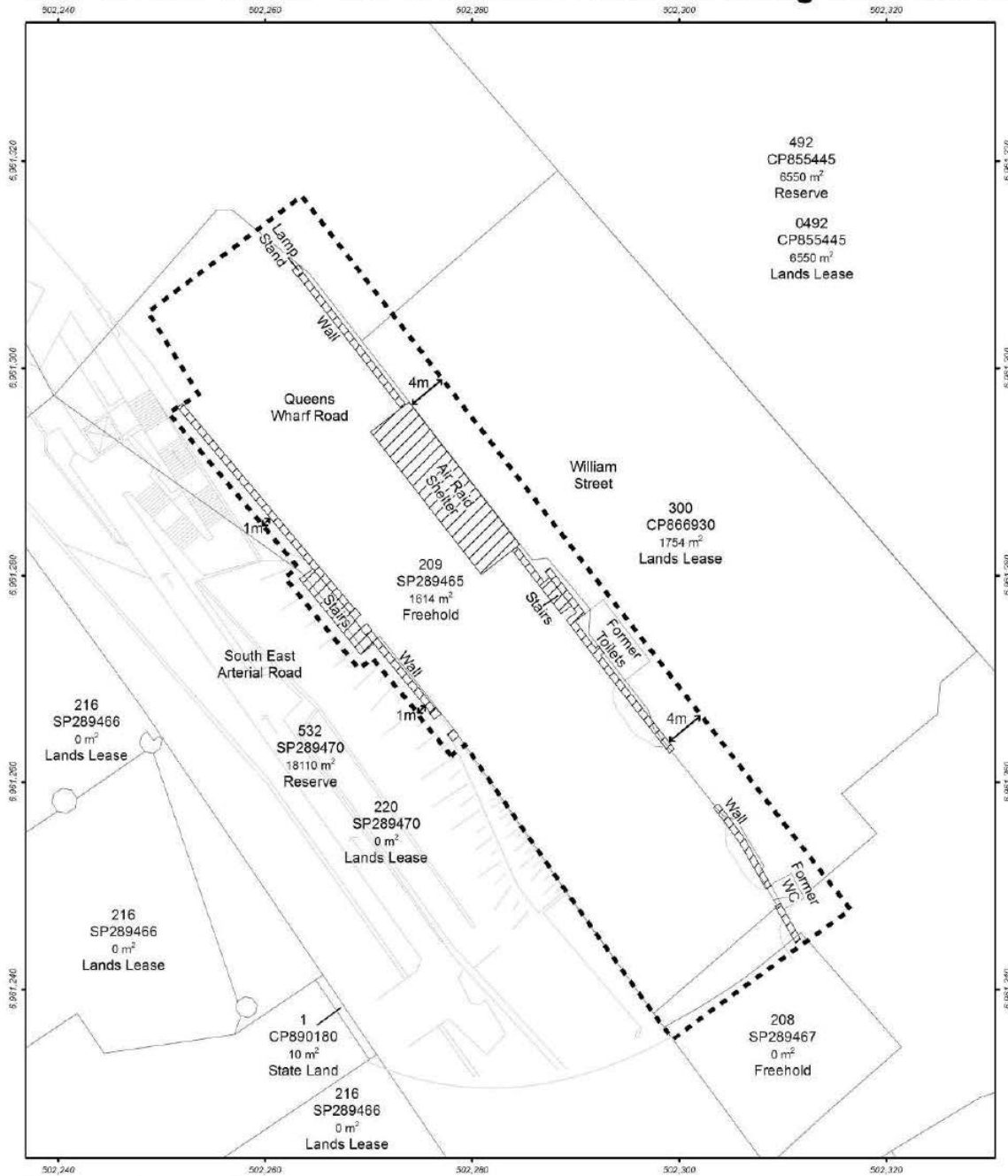
Consultation has occurred with DBC and its consultants, and with the Queensland Heritage Council (QHC), DEHP and the National Trust of Australia (Queensland) (NTAQ) during the preparation of this report.

The site of the William Street and Queens Wharf Road retaining walls and the Former Victoria Bridge Abutment (northern) was physically inspected as part of this study.

1.6. LIMITATIONS

At the time of inspection the Air Raid Shelter was in use as storage. Access to the interior was not possible.

William Street and Queens Wharf Road retaining walls 600135



0 5 10 20 Meters 1:500

Map Projection: Transverse Mercator (MGA) zone 56
Horizontal datum: Geodetic Datum of Australia 1994 (GDA94)
Map Production: Heritage, Department of Environment and Heritage Protection 4 May 2017
Accuracy Statement: Due to varying source, accuracy or currency of data layers used in this map, the spatial locations of features may not coincide when overlaid.
Disclaimer: This map has been produced for the purposes of information and discussion. While the map has been prepared with care, neither the Queensland Government nor the Department of Environment and Heritage Protection or its officers or its staff, accept any liability for any decisions or actions taken by individuals or organisations on the basis of this map.



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Map 2 (with site plan)

Heritage boundary overview

Legend

- Heritage register boundary
- Significant elements
- Cadastral**
- Parcel
- Easement



Figure 2 –Heritage Curtilage and place components

Source: Google Earth

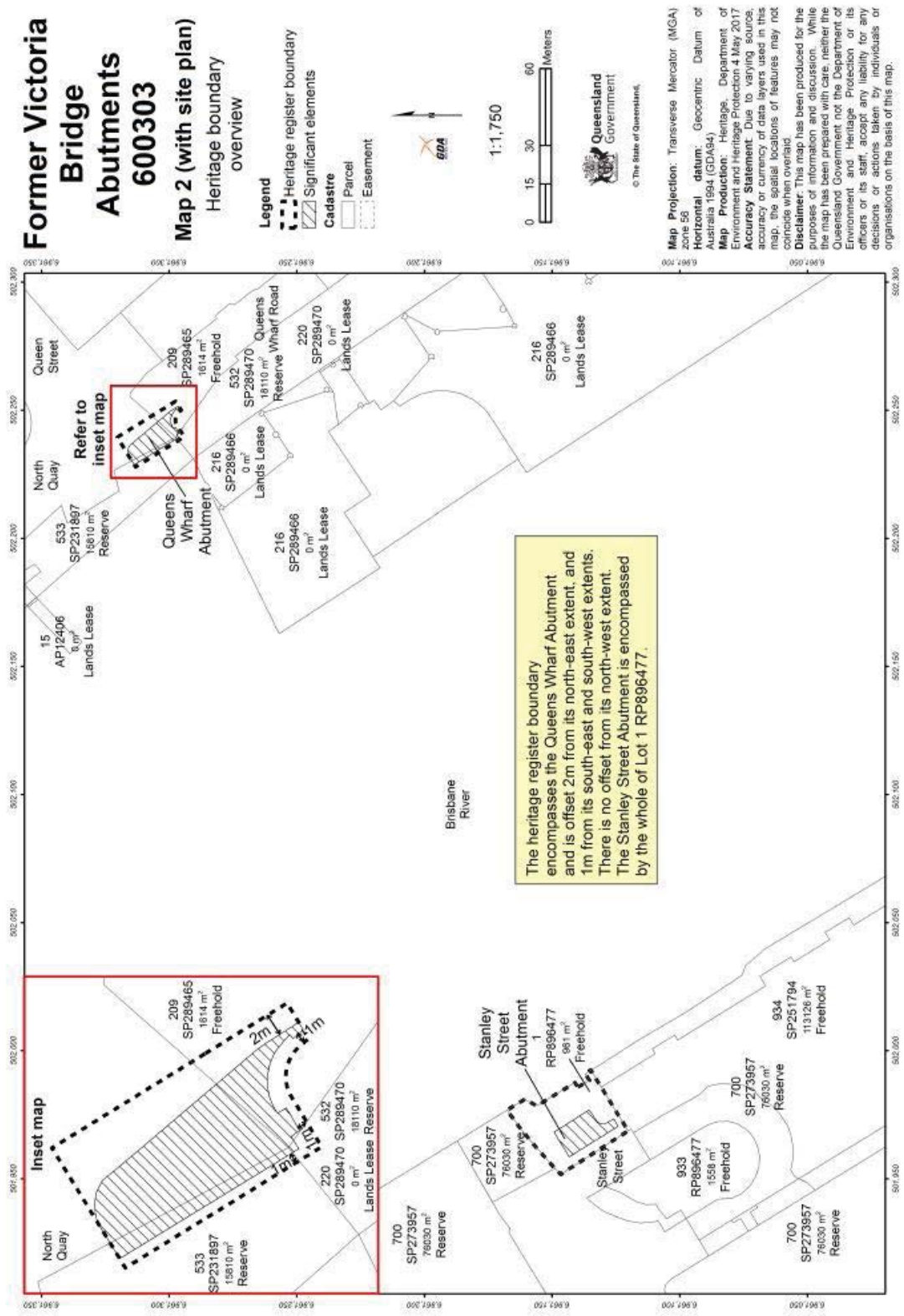


Figure 3 – Heritage Curtilage of the Former Victoria Bridge Abutments
 Source: DEHP QHR Entry 600303

2. HISTORICAL CONTEXT

2.1. WILLIAM STREET AND QUEENS WHARF ROAD RETAINING WALLS

The following history is derived from the QHR citation.

2.1.1. William Street Retaining Wall and Queens Wharf Road Retaining Walls

Prior to the construction of the 1880s William Street Retaining Wall, a timber picket fence lined William Street in this location (**Figure 4**) and a timber post and rail fence was positioned along the south-western side of Queens Wharf Road (**Figure 5**).

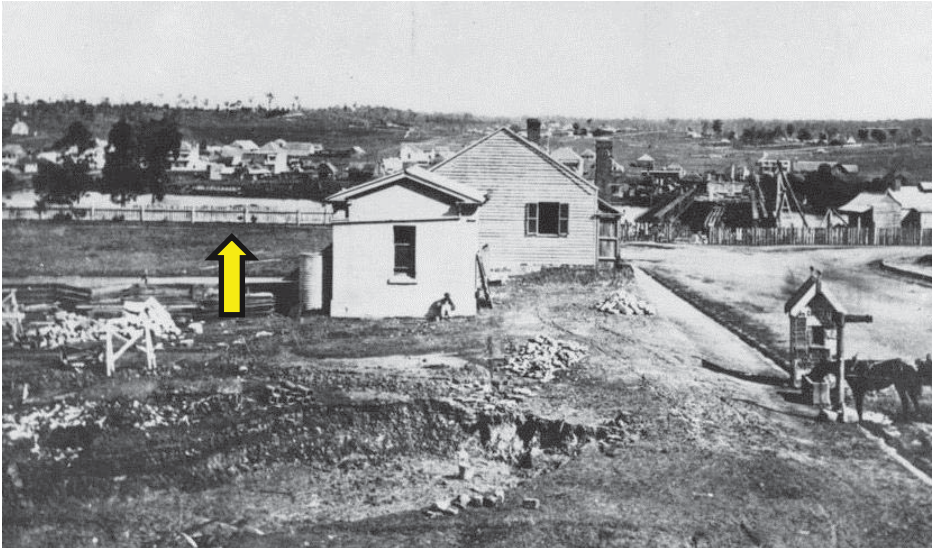


Figure 4 – William Street c. 1873

Source: SLQ ID 17269

The William Street Retaining Wall was planned in 1888 to end William Street with some aesthetic distinction to compliment the new Treasury Building being erected opposite the site. The Victoria Bridge end of an old rubble wall along William Street was to be replaced by a tuff and sandstone wall with an elaborate cast iron railing and a small pissoir built into the wall at the Elizabeth Street end.



Figure 5 – Early timber fence along Queens Wharf Road c. 1878

Source: SLQ ID 17646

In November 1888 the contract was awarded to EJ O'Connor, who in the previous year had stabilised the large tuff retaining wall at Queen Street, Petrie Bight (QHR 600159). Within five months O'Connor was

contracted to excavate and reform William Street and build the retaining wall for £1431. By the time Cabinet first met in the new Treasury Building in 1889, the wall, railing and gas lamp in William Street were completed.

The marble end post to the William Street Retaining Wall (extant) was a gift to Mayor Galloway from the mayor of Adelaide, who had visited Brisbane whilst the wall was being completed. The extant railing and the elaborate gas lantern were made by local firm Smith Faulkner & Co (formerly Smith Forrester & Co), who also had provided the cast iron railing for the large retaining wall at Petrie Bight.

By 1890 a cab stand had been established at the Victoria Bridge end of the William Street Retaining Wall. By the turn of the century the cabs had been supplanted by horse-drawn buses, and a small timber and iron shelter for the convenience of waiting passengers had been erected on the William Street pavement abutting the end railing. This was removed by about 1910.

Until 1928 when the Queen Street frontage of the Treasury Building was completed, the William Street frontage was an important focus for official occasions. In January 1901, large crowds congregated in front of the William Street Retaining Wall to hear Lord Lamington proclaim the Commonwealth of Australia from a balcony of the Treasury Building opposite.

In the decades that followed, the retaining wall served more practical purposes. In the twentieth century motorbuses replaced the horse-drawn vehicles lined along William Street, and by the early 1930s a large timber bus timetable sign had been erected against the railing of the retaining wall.

From the 1920s to 1930s, fill under the William Street footpath was excavated to accommodate male toilets built into the retaining wall at the Queens Wharf Road level. Apparently at the same time, a three-metre section of the retaining wall and railing was realigned to accommodate a stairway leading from William Street to the new public conveniences. The entrance to the original pissoir was bricked over during this period.

The decorative masonry and iron Queens Wharf Road Retaining Wall, on the river side of Queens Wharf Road, was erected in the 1930s. Plans were prepared in 1934, but the wall was not erected until 1936. It appears to have been intended to, and still does, demark the entrance to the ramp leading to Hayles Wharf, which is leased from the Council. Sections of the retaining wall here were extant before this period.

As a wartime precaution, the Brisbane City Council erected an air raid shelter on the Queens Wharf Road frontage of the William Street Retaining Wall, abutting the Victoria Bridge end, circa early 1940s. Above this a bus shelter, equipped with public telephones and a drinking fountain was erected on the William Street footpath in 1944 (**Figure 6**). The configuration of the bus shelter, retaining wall alignment and Second Victoria Bridge abutment are shown in a 1963 postcard view of Brisbane (**Figure 7**).



Figure 6 – Bus Shelter in 1950

Source: BCC-B54-721

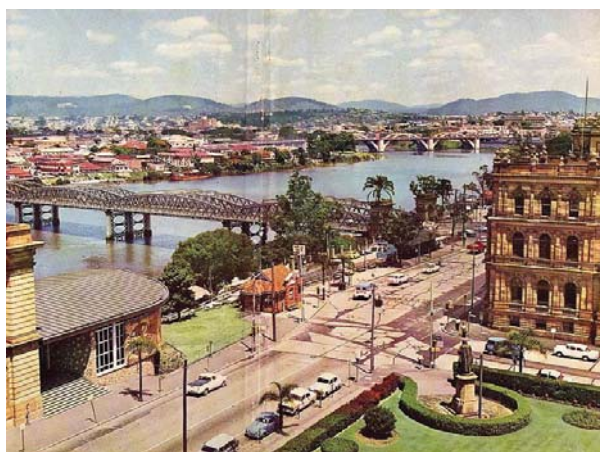


Figure 7 – William Street in 1963

Source: BCC CD25-05

In the early 1970s female toilet accommodation was provided abutting the William Street Retaining Wall on Queens Wharf Road, and 20 metres of the wall was demolished to make way for the Elizabeth Street off-ramp for the Riverside Expressway. **Figure 8** shows the area during the 1974 flood and construction of the Riverside Expressway showing a number of structures along William Street and Queens Wharf Road.



Figure 8 – During the 1974 flood

Source: BCC T54-919

2.1.2. Abutment of the Second Victoria Bridge

The Abutment of the Second Victoria Bridge was constructed in 1896 for the second Victoria Bridge, which was designed by John Smith Murdoch and AB Brady. Much of the first Victoria Bridge, completed in 1874, was destroyed during the 1893 flood. The second Victoria Bridge remained in use until 1969, when it was demolished and replaced by the third Victoria Bridge, which currently spans the Brisbane River.

Following the 2011 flood, works were undertaken to upgrade the North Quay ferry terminal. As part of these works, a lift, stairs and new access ramps were constructed adjacent to the bridge abutment in 2015 (Figures 9 and 10), and conservation works were undertaken to the Queens Wharf Road Retaining Wall.



Figure 9 – Victoria Bridge, 1897. Abutment remnants of that at left of bridge.

Source: State Library of Queensland Image Number 63659



Figure 10 – Lift adjacent to Bridge Abutment, December 2015

Source: Google Street View

2.2. CHRONOLOGICAL OVERVIEW

Table 1 provides a summary of the key dates and events in the history and development of the William Street and Queens Wharf Road retaining walls.

Table 1 – Chronological Overview

Date	Event
1888	Construction of William Street Retaining Wall planned to complement the Treasury Building being constructed.
1889	Wall, railing and gas lamp for the William Street Retaining Wall had been completed.
1896	Second Victoria Bridge constructed.
1920s-early 1930s	Male toilets constructed beneath William Street Retaining Wall.
1936	Queens Wharf Road Retaining Wall constructed along the river side of Queens Wharf Road.
1941	WWII Air Raid Shelter built adjacent to male toilets on Queens Wharf Road.
1944	Bus shelter constructed above WWII Air Raid Shelter along William Street.
1969	Second Victoria Bridge replaced with new bridge.
1970s	Female toilets constructed along Queens Wharf Road. 20m of William Street Retaining Wall demolished for Riverside Expressway construction.
1990s	Treasury Casino redevelopment saw excavation of area behind the William Street Retaining Wall for the construction of a loading dock.
2000s	Removal of bus shelter.
2015	Construction of new lift, stairs and ramp from Queens Wharf Road to Bicentennial Bikeway.

2.3. HISTORICAL THEMES

Historical themes can be used to understand the context of a place, such as what influences have shaped that place over time. The Queensland thematic framework (Blake 1996) includes ten principal themes and 64 sub-themes in Queensland's history. These themes correlate with national and local historical themes.

Historical themes at each level that are relevant to the William Street and Queens Wharf Road retaining walls are provided in **Table 2**.

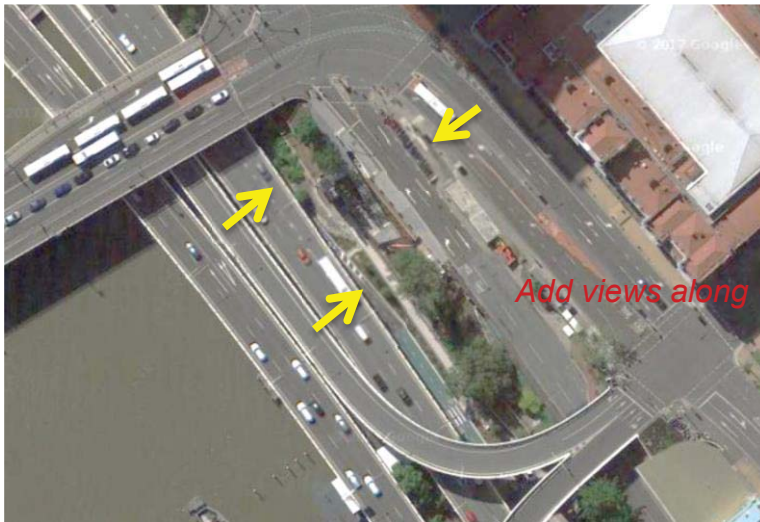
Table 2 – Historical Themes

QLD Theme	Sub-Theme	Example of Place
6. Building settlements, towns, cities and dwellings	Developing urban services and amenities	<ul style="list-style-type: none">• Retaining walls• Bridges
7. Maintaining order	Defending the country	<ul style="list-style-type: none">• WWII Air Raid Shelter
8. Creating social and cultural institutions	Commemorating significant events	<ul style="list-style-type: none">• Monument

3. PHYSICAL CONTEXT

3.1. VIEWS, LANDSCAPE AND SEETING

The retaining walls are located in close proximity to the heritage-listed former Treasury Building and former State Library. The fabric of the structures contributes to the streetscape of Heritage Places in the William Street area and the structures have strong visual connectivity with the former Treasury Building. Views and visual connectivity are shown in **Figure 11**.



Add views along Queens Wharf Road from both directions

Figure 11 – Views and visual connectivity

Source: Urbis 2016

3.2. WILLIAM STREET RETAINING WALL

The William Street Retaining Wall is constructed of rough-coursed stone (Brisbane Tuff), with a parapet, sandstone coping and decorative cast-iron railing. It extends approximately 70 metres along William Street between Queen and Elizabeth Streets, and reaches a maximum height of 4.5 metres to the Queens Wharf Road frontage. A pedestal terminating the wall at the Victoria Bridge end is inscribed “WM Galloway Mayor 1889”, and supports an elaborate gas lamp stand bearing the words “Smith, Faulkner & Co. 1889”.

Visually this wall forms a plinth to the former Treasury Building when viewed from the direction of the river (see **Figure 12**).



Figure 12 – William Street retaining wall

Source: Urbis 2016

Original stonework and some new stone face both sides of an interwar stairwell, which has a contemporary metal railing. This leads to a men's toilet block that has been built into the retaining wall, with windows and a door opening off Queens Wharf Road. The letters "BCC" are emblazoned in raised cement render in the arch above the central window.

Openings in this part of the wall have been made to provide access to the Treasury Casino loading dock area. Another section of original wall remains between the former female toilets and the Elizabeth Street freeway exit ramp. The entrance to the original pissoir, built into the wall in this section, is bricked over.

Two lengths of stone kerbing adjacent to the wall survive on top of the cutting made when Queens Wharf Road was lowered in the 1950s.

3.3. QUEENS WHARF ROAD RETAINING WALL

On the river side of Queens Wharf Road the stone retaining wall has been recently subject to conservation works and sits on a base wall of rough laid pebbles set in concrete (**Figure 13**).

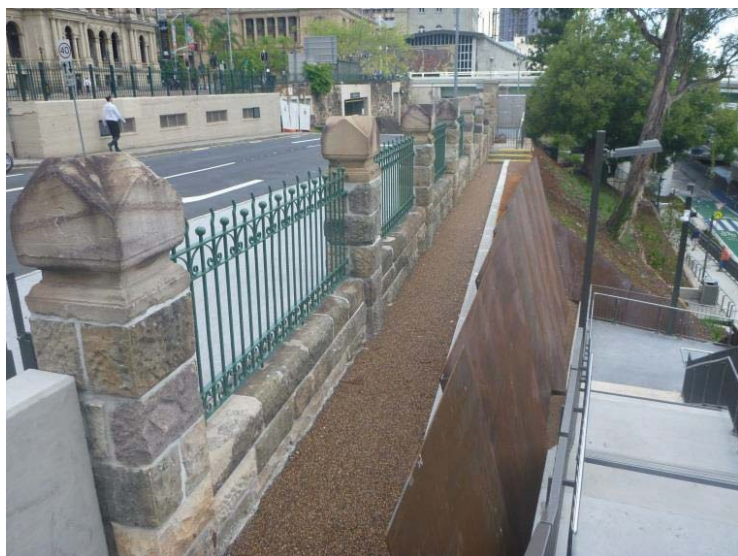


Figure 13 – Queens Wharf Road retaining wall

Source: Urbis 2016

Ornamental capping stones on the pillars are cut at the corners so that they form gables in elevation. Two former gateways in the wall are marked by taller pillars with flat capping stones which until recently led to concrete steps and bitumen surfaced ramps.

New stairs and lift access have been constructed further north towards the Victoria Bridge Abutment between Queens Wharf Road and the Bicentennial Bikeway.

3.4. WWII AIR RAID SHELTER

A Second World War concrete air raid shelter is contiguous to the William Street Retaining Wall on the Queens Wharf Road level (**Figures 14** and **15**). Above this about 15 metres of the parapet was removed to accommodate a 1944 bus shelter at the William Street level.

A further 30 metres of the parapet has been removed at the Elizabeth Street end of the William Street Retaining Wall for the building of a female toilet block (1970s) (not extant) so that the William Street footpath now extends onto its roof. Box steel railings have been used in this area.

The bus shelter and women's toilets have since been removed. The WWII Air Raid Shelter is currently used by Brisbane City Council for storage purposes.

The current floor plan and elevation (to Queens Wharf Road) are provided in **Appendix A**.

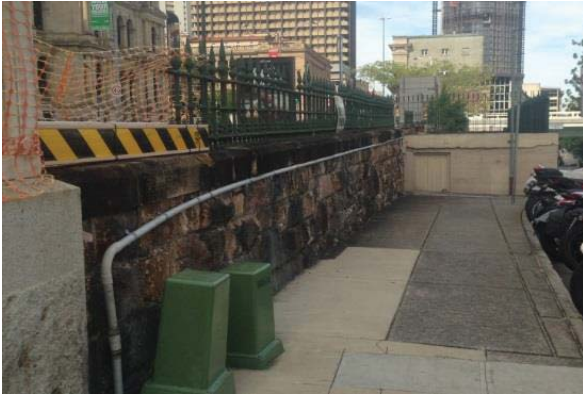


Figure 14 – Looking Towards WWII Air Raid Shelter along Queens Wharf Road

Source: Urbis 2016



Figure 15 – Top of WWII Air Raid Shelter Along William Street

Source: Urbis 2016

3.5. ABUTMENT OF THE SECOND VICTORIA BRIDGE

Part of the northern abutment of the second permanent Victoria Bridge (1897) survives at the western end of the Queens Wharf Road Retaining Wall (**Figures 16 and 17**). Located below Queens Wharf Road and adjacent to the current Victoria Bridge, it is a substantial rock-faced tuff structure with a smooth sloped sill stone at the junction between the base and the wall above. Adjoining the abutment is a curved battered wall, constructed of loose rough-hewn tuff blocks, that sits on a concrete base.



Figure 16 – Victoria Bridge abutment from Bicentennial Bikeway

Source: Urbis 2016

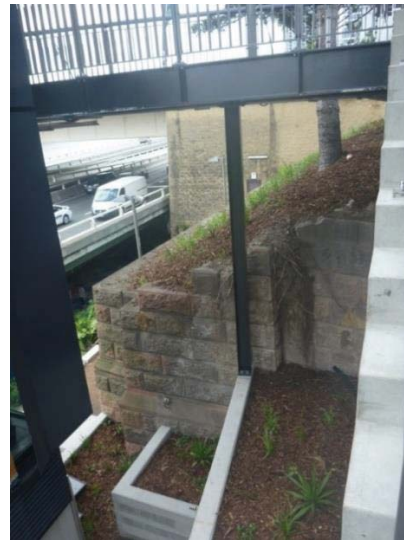


Figure 17 – Victoria Bridge abutment from stairs

Source: Urbis 2016

3.6. CURTILAGE

A heritage boundary, or curtilage, has been prepared for the William Street and Queens Wharf Road retaining walls by officers of the DEHP and approved by the Queensland Heritage Council as an appropriate curtilage for the place. The majority of the place is included within Lot 209 SP289465.

The bridge abutment is in 13SP180753, forming the northern portion of the QHR Entry 600303.

3.7. CONDITION AND INTEGRITY

3.7.1. Condition

As part of recent redevelopment of the site associated with the installation of a lift and construction of new stairs, the Queens Wharf Road Retaining Wall has been conserved. This, and the former Victoria Bridge abutment (northern) are currently in good condition.

The pedestal at the northern end of the William Street Retaining Wall is also currently being repaired. The remainder of the wall is in need of conservation works, and it is not known if this will occur once the works to the pedestal is complete.

The use of a cement rich mortar in the William Street Retaining Wall has resulted in deterioration of the stone in some locations. Mortar is also missing or damaged in several locations.

Overall the stone is stained and has vegetation growing in it. The William Street Retaining Wall also has a number of redundant fixings that could result in damage to the stone.

3.7.2. Integrity

The walls have been altered from their original configuration and are considered to have a moderate degree of integrity.

3.8. HISTORICAL ARCHAEOLOGICAL POTENTIAL

Historical archaeology is the study of the past using physical evidence in conjunction with historical sources. It focuses on the objects used by people in the past and the places where they lived and worked. It can tell us about the way things were made and used and how people lived their daily lives. Archaeology is not just about objects and remains; it is also about landscapes and links between sites.

~~Archaeological Potential is defined as (Heritage Office & Department of Urban Affairs and Planning 1996):~~

~~The degree of physical evidence present on an archaeological site, usually assessed on the basis of physical evaluation and historical research. Common units for describing archaeological potential are:~~

- ~~• Known archaeological features/sites (high archaeological potential);~~ *Replace with text on next page*
- ~~• Potential archaeological features/sites (medium archaeological potential);~~
- ~~• No archaeological features/sites (low archaeological potential).~~

It is essential to identify areas of archaeological potential in order to prepare measures to protect and conserve the archaeological record, and to assist in providing recommendations for further investigation.

3.8.1. Summary

The historical archaeological potential of the William Street and Queens Wharf Road retaining walls, and Former Victoria Bridge Abutment, is shown in **Figure 18**.

The potential archaeological resources of the place can be divided into two categories, as follows:

- **Low (blue)**

The area has historically contained pathways, roads and landscaping and due to the level of past disturbance the level of archaeological potential is considered to be low.

- **High (red)**

Part of the area falls within the Early Streets of Brisbane heritage listing and has the potential to contain archaeological evidence from the initial European occupation of Brisbane through to the present day.

There is also potential for earlier walls to be present, and recent excavation of the retaining wall at the Commissariat Store also indicates potential for domestic artefacts to be located in the fill behind these existing retaining walls.

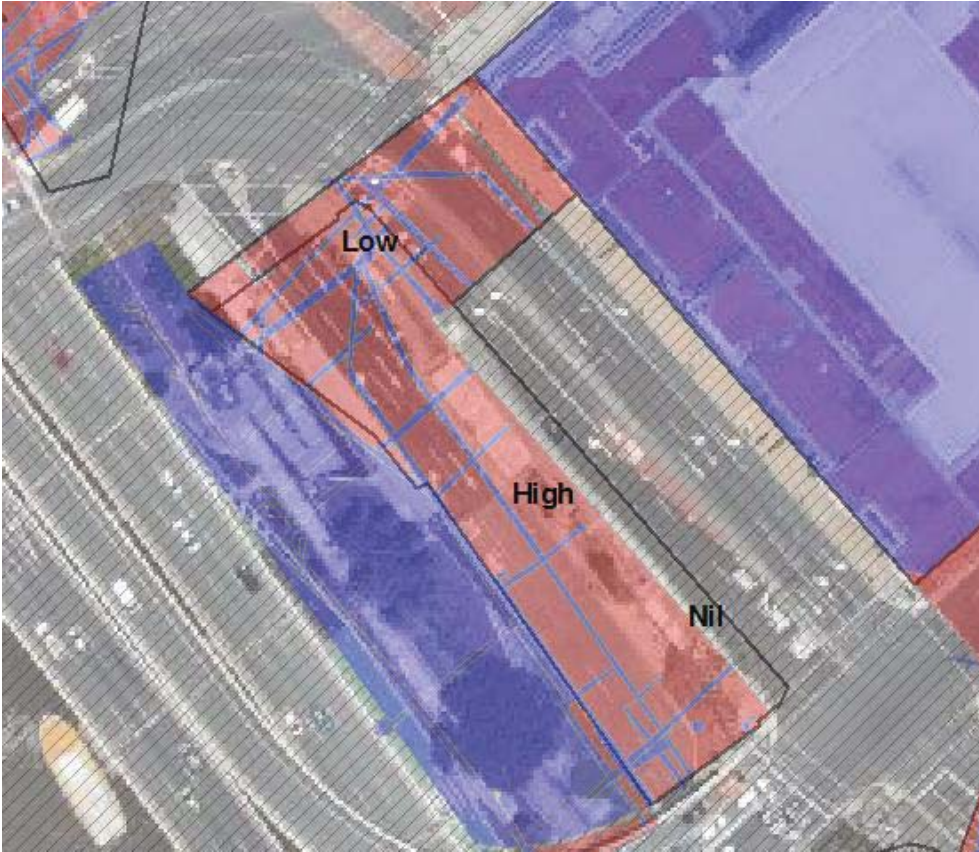


Figure 18 – Historical Archaeological Potential

Source: Urbis 2016

Replace strikethrough under 3.8 with:

High archaeological potential: Where archaeological features are known to exist, and the level of ground disturbance is understood and has not impacted on the archaeological record.

Moderate archaeological potential: Where historic features are known to have existed in the past, and the level of disturbance is either unquantified or has not extended beyond the surface layers.

Low archaeological potential: Where historic features are known to have existed in the past, but historical evidence indicates that the area below the surface has been disturbed. Where no documented features have existed in the past, but the level of ground disturbance is understood and has not impacted on the archaeological record.

No archaeological potential: No archaeological remains possible due to prior substantial disturbance that has been recorded such as excavation.

4. CULTURAL SIGNIFICANCE

Before making decisions to change a heritage item, an item within a heritage conservation area, or an item located in proximity to a heritage listed item, it is important to understand its values and the values of its context. This leads to decisions that will retain these values in the future. Statements of heritage significance summarise a place's heritage values – why it is important, and why a statutory listing was made to protect these values.

4.1. ABOUT CULTURAL SIGNIFICANCE

Cultural significance is the term used to embrace the range of qualities that make some places especially important to the community, over and above their basic utilitarian function. These places are usually those that help understand the past, enrich the present, and will be of value to future generations.

The Burra Charter defines cultural significance as aesthetic, historic, scientific or social value for past, present, or future generations.

It is a simple concept, helping to identify and assess the attributes that make a place of value to people and society. An understanding of it is therefore basic to any planning process involving historic buildings or places. Once the significance of a place is understood, informed policy decisions can be made which will enable that significance to be retained or revealed. A clear understanding of the nature and level of the significance of a place not only suggests constraints on future action, it also introduces flexibility into the process by identifying areas which can be adapted or developed with greater freedom.

4.2. SIGNIFICANCE ASSESSMENT

Table 3 outlines the current significance assessment for William Street and Queens Wharf Road retaining walls as outlined in the QHR entry. Relevant detail regarding the former Victoria Bridge Abutment (Northern) (taken from QHR Entry 600303) is included in the table in *italics*.

Table 3 – Assessment of Heritage Significance

Criterion		Significance Assessment	
<p>A – Historical Significance /Scientific Significance</p> <p>The place is important in demonstrating the evolution or pattern of Queensland's history</p> <p>Significance Indicators</p> <ul style="list-style-type: none"> Product, result or outcome of an event, phase, movement, process, activity or way of life that has made a strong, noticeable or influential contribution to the evolution or pattern of our development of our society or of our environment. 	☒	<p>The William Street and Queens Wharf Road retaining walls are important in demonstrating the historical development of Queensland's civic infrastructure and urban facilities from the late 19th to the mid 20th Century. Construction of the William Street wall (1889) was closely associated with the Treasury Building, making a functional and aesthetic contribution to a site that was an expression of Queensland's economic aspirations and a focus for official occasions.</p>	
	<ul style="list-style-type: none"> Example of a process or activity that has made a strong, noticeable or influential contribution to the evolution or pattern of our development of our society or of our environment. 	☐	<p>The various sanitation elements incorporated into the William Street retaining wall, from a simple closet recess to an interwar toilet block with ventilation shaft, illustrate the development of urban public health facilities.</p>

<ul style="list-style-type: none"> Influenced by an event, phase, movement, process, activity or way of life that has made a strong, noticeable or influential contribution to the evolution or pattern of our development of our society or of our environment. 	<input type="checkbox"/>	<p>Designed to afford protection in the event of air raid attacks or other emergencies, the air raid shelter is important in demonstrating the impact of WWII on the civilian population of Brisbane and as part of the Ari Raid Precaution activities that were implemented for the defence of Brisbane.</p>	
<ul style="list-style-type: none"> Has influenced an event, phase, movement, process, activity or way of life that has made a strong, noticeable or influential contribution to the evolution or pattern of our development of our society or of our environment. 	<input type="checkbox"/>	<p>The Queens Wharf Road wall (1936) is important in demonstrating interwar beautification initiatives undertaken along the north bank of the Brisbane River.</p> <p><i>The Victoria Bridge Abutments stand as a memorial of the first permanent bridge to be constructed across the Brisbane River which did not succumb to the forces of nature. The opening of the bridge in 1897 provided a major boost to the continued development of the City of South Brisbane, which has first received its development impetus from the introduction of cross-river transport provided by the previous bridges at this location.</i></p>	
<ul style="list-style-type: none"> Site of or associated with an event, or activity that has made a strong, noticeable or influential contribution to the evolution or development of our society or of our environment. 	<input type="checkbox"/>		
<ul style="list-style-type: none"> Symbolic association with an event, or activity that has made a strong, noticeable or influential contribution to the evolution or development of our society or of our environment. 	<input type="checkbox"/>	<p>State threshold indicators</p> <ul style="list-style-type: none"> Regional importance Earliness Representativeness Distinctiveness/Exceptionality Rarity 	<input checked="" type="checkbox"/> <input checked="" type="checkbox"/> <input checked="" type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/>
<p>B – All Aspects of Heritage Significance The place has rare, uncommon or endangered aspects of Queensland's cultural heritage.</p>		Does not meet this criterion.	
<p>C – Scientific Significance/Historical Significance</p> <p>The place has potential to yield information that will contribute to an understanding of Queensland's history.</p> <p>Significance Indicators</p> <p>Potential to contribute new knowledge about Queensland's history</p>	<input checked="" type="checkbox"/>	<p>The place has the potential to contain archaeological artefacts that are an important source of information about Queensland's history. Despite disturbance in some areas, there is the potential for subsurface material to survive relating to the establishment, evolution and pattern of settlement of early Brisbane as a penal colony, as well as accumulated material</p>	

<ul style="list-style-type: none"> • A function that has been an important part of the pattern of Queensland's history 	<input type="checkbox"/>	<ul style="list-style-type: none"> • Rarity/uncommonness • Exceptionality 	<input type="checkbox"/>
<ul style="list-style-type: none"> • The work of a designer who made an important contribution to Queensland's built environment 	<input type="checkbox"/>		
<ul style="list-style-type: none"> • An architectural style or form that has made an influential or noticeable contribution to the pattern or evolution of Queensland's built environment 	<input type="checkbox"/>		
<ul style="list-style-type: none"> • A construction technique or particular use of materials that has made a conspicuous or early contribution to the evolution of Queensland's built environment 	<input checked="" type="checkbox"/>		
<ul style="list-style-type: none"> • Variations within, or the evolution of or the transition of, the principal characteristics of a class of cultural places 	<input type="checkbox"/>		
<p>E – Aesthetic Significance/Architectural Significance</p> <p>The place is important because of its aesthetic significance.</p> <p>Significance Indicators</p> <p>Demonstrates or possesses:</p> <ul style="list-style-type: none"> • Beautiful attributes • Natural beauty or natural aesthetic quality • Picturesque attributes • Evocative qualities • Expressive attributes • Landmark quality • Streetscape contribution • Symbolic meaning 	<input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input checked="" type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input checked="" type="checkbox"/> <input type="checkbox"/>	<p>The William Street and Queens Wharf Road retaining walls are of aesthetic significance for their contribution to the definition of an important ceremonial and civic space associated with the river front and the Treasury Building. The textured surface of the stone, its varied colouration and the light and open effect of the railings endow both walls with an attractive streetscape presence.</p> <p><i>Designed by Government Architect, AB Brady, the abutments' 19th Century design characteristics, which include classical stylistic references, monumental form and rusticated stonework, combined with their siting on either side of the river, form an evocative reminder of previous efforts to span the Brisbane River and make an important contribution to the urban riverscape of Queensland's capital city.</i></p> <p>State threshold indicators</p> <ul style="list-style-type: none"> • Intactness 	<input type="checkbox"/> <input type="checkbox"/> <input type="checkbox"/> <input checked="" type="checkbox"/>

		<ul style="list-style-type: none"> • Integrity • Degree of deterioration • Setting and location context • Demonstrated representation 	<input type="checkbox"/>
F – Aesthetic Significance/Architectural Significance/Other Significance The place is important in demonstrating a high degree of creative or technical achievement at a particular period.		Does not meet this criterion.	
G – Social Significance The place has a strong or special association with the life or work of a particular person, group or organisation of importance in Queensland's history		The site has had a strong and continuing association with Brisbane passenger transport since 1890.	
Significance Indicators		State threshold indicators	
<ul style="list-style-type: none"> • Important to the community as a landmark, marker or signature 	<input type="checkbox"/>	<ul style="list-style-type: none"> • Length of association 	<input checked="" type="checkbox"/>
<ul style="list-style-type: none"> • A place which offers a valued, customary experience 	<input checked="" type="checkbox"/>	<ul style="list-style-type: none"> • Demonstrated extent and degree of community association 	<input type="checkbox"/>
<ul style="list-style-type: none"> • A popular meeting or gathering space 	<input type="checkbox"/>	<ul style="list-style-type: none"> • Significant former association 	<input type="checkbox"/>
<ul style="list-style-type: none"> • Associated with events having a profound effect on a particular community or cultural group 	<input type="checkbox"/>		
<ul style="list-style-type: none"> • A place of ritual or ceremony 	<input type="checkbox"/>		
<ul style="list-style-type: none"> • Symbolically representing the past in the present 	<input type="checkbox"/>		
<ul style="list-style-type: none"> • A place of essential community function leading to a special attachment 	<input type="checkbox"/>		
H – Historical Significance The place has a strong or special association with a particular community or cultural group for social, cultural or spiritual reasons		Does not meet this criterion.	

4.3. STATEMENT OF SIGNIFICANCE

The following statement of significance is derived from the Queensland Heritage Register citation. Recommended changes to the statement of significance are shown in **bold**. The (amended) statement of cultural significance below is adopted as the statement of cultural significance in this CMP for the William Street and Queens Wharf Road retaining walls.

*The retaining walls are important in demonstrating the evolution and pattern of Queensland's history, in particular the William Street wall, railing, lamp stand, pissoir entry, interwar toilet block and 1940s air-raid shelter, provide evidence of the civic function of the site. The retaining walls also offer surviving evidence of the 1897 Victoria Bridge, which **provides important evidence of the second permanent crossing of the Brisbane River, and has an association with Queensland Government architects, John Smith Murdoch and Alfred Barton Brady.***

The iron railings and lamp stand on the William Street retaining wall are important in demonstrating the principal characteristics of the decorative work of leading 19th century Brisbane ironmongers Smith Faulkner & Co.

The retaining walls exhibit particular aesthetic characteristics valued by the community, namely the contribution of both the William Street and Queens Wharf Road retaining walls to the definition of an important ceremonial and civic space associated with the river front and the Treasury Building. The textured surface of the stone, its varied colouration and the light and open effect of the railings endow both walls with special aesthetic appeal.

The site has had a strong and continuing association with Brisbane passenger transport since 1890.

4.4. LEVELS OF SIGNIFICANCE

The level of heritage significance of a place and its components also needs to be considered. Different components of a place may make up different relative contributions to its heritage value, and integrity and condition also need to be taken into account.

Table 4 describes levels of significance and justification, adapted from the New South Wales Heritage Office guideline Heritage Manual 2: Assessing Heritage Significance, considered to be best practice in heritage conservation across Australia (NSW Heritage Office 2001).

Table 4 – Levels of Significance

Level of Significance	Justification
Exceptional	<ul style="list-style-type: none"> Rare or outstanding elements that directly contribute to the place's overall heritage significance. Retains a high degree of integrity and intactness in fabric or use. Any change should be minimal and retain significant values or fabric.
High	<ul style="list-style-type: none"> Element demonstrates a key aspect of the place's overall heritage significance. which directly contributes to the place's overall significance. Possesses a high degree of original fabric or retains the original use. Any change should be minimal and retain significant values or fabric.
Moderate	<ul style="list-style-type: none"> Element contributes to the place's overall heritage significance. May have been altered but they still have the ability to demonstrate a function or use particular to the site. Reconstructed Reproduced elements which are not original but have contributory significance in contributing to the character of the place.

Level of Significance	Justification
	<ul style="list-style-type: none"> Changes may be appropriate so long as it does not adversely affect the place's overall heritage significance.
Little	<ul style="list-style-type: none"> Element may be difficult to interpret or may have been substantially modified which detracts from its heritage significance. Changes may be appropriate long as it does not adversely affect the place's overall heritage significance.
Neutral	<ul style="list-style-type: none"> Elements do not add or detract from the site's overall heritage significance. Changes may be appropriate long as it does not adversely affect the place's overall heritage significance.
Intrusive	<ul style="list-style-type: none"> Elements are damaging to the place's overall heritage significance. May be considered for removal or alteration.


4.5. SCHEDULE OF SIGNIFICANT ELEMENTS



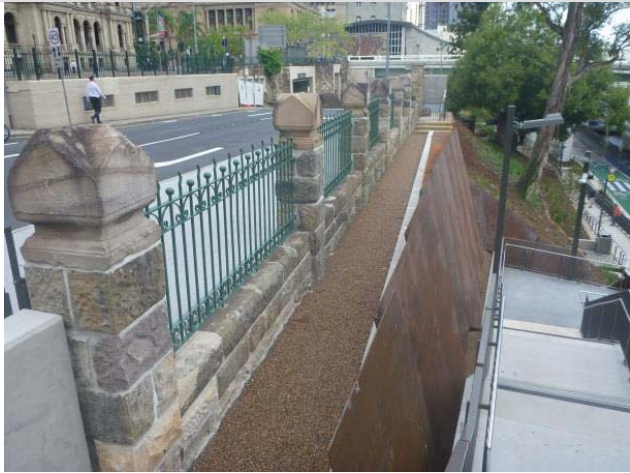
Victoria Bridge Abutment (northern)



While the William Street and Queen's Wharf Road retaining walls are considered to be of cultural heritage significance, not all of their component parts are equally significant. Various elements of the William Street and Queen's Wharf Road retaining walls have been graded below in relation to their contribution to the site's overall heritage significance. Elements including monuments, landscape and equipment that are located within the site's curtilage are outlined below in **Table 5** along with their level of significance in accordance with the assessment table at **Table 4**.



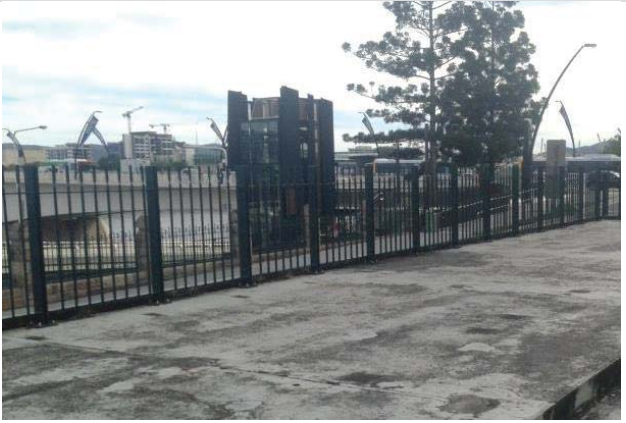
If as a result of further investigation, earlier or additional fabric revealed, this schedule is to be reviewed.




Table 5 – Gradings of Heritage Significance

Structure, Space or Element	Location	Photograph	Level
William Street Retaining Wall (1890s)	William Street Retaining Wall		High

<p>Pedestal with inscription, and portion of gas lamp stand</p>	<p>William Street Retaining Wall</p>		<p>High</p>
<p>Abutment to the Second Victoria Bridge (QHR 600303)</p>	<p>Adjacent to current bridge and above the Bicentennial Bikeway</p>		<p>High</p>
<p>Queens Wharf Road Retaining Wall</p>	<p>Queens Wharf Road, opposite the Treasury Building</p>		<p>High</p>

<p>WWII Air Raid Shelter</p>	<p>Between William Street and Queens Wharf Road, opposite the Treasury Building</p>		<p>High</p>
<p>Iron fencing</p>	<p>William Street Retaining Wall</p>		<p>High</p>
<p>Stairs</p>	<p>From William Street down to Queens Wharf Road</p>		<p>Moderate</p>

<p>Entrance to Men's Toilets</p>	<p>Queens Wharf Road at base of stairs</p>		<p>Moderate</p>
<p>Men's Toilet Block</p>	<p>Queens Wharf Road</p>		<p>Moderate</p>
<p>Modern balustrading</p>	<p>William Street Retaining Wall</p>		<p>Neutral</p>

<p>Storage area</p>	<p>Adjacent to WWII Air Raid Shelter, Queens Wharf Road</p>		<p>Intrusive</p>
<p>General and Service openings in wall</p>	<p>Queens Wharf Road</p>		<p>Intrusive</p>
<p>Fencing along Queens Wharf Road</p>	<p>Queens Wharf Road</p>		<p>Intrusive</p>

5. KEY ISSUES

This section outlines all the factors likely to present opportunities and constraints for the William Street and Queens Wharf Road retaining walls.

5.1. HERITAGE LISTINGS

The heritage listings identified in **Table 6** apply to the subject site.

Table 6 – Heritage Listings

Type of Listing	Name of Item	Details
STATUTORY LISTINGS		
Queensland Heritage Register under the <i>Queensland Heritage Act 1992</i> (items of state significance)	William Street and Queens Wharf Road retaining walls Former Victoria Bridge Abutment (Northern)	QHR Place ID 600135 QHR 600303
Brisbane City Plan 2014 City Plan Heritage Register Local Heritage Overlay (items of local significance)	William Street and Queens Wharf Road Retaining Walls – North Quay porphyry wall Victoria Bridge Abutment	Registered Registered

5.2. STATUTORY OBLIGATIONS

5.2.1. Economic Development Act 2012

The Queen's Wharf Brisbane Priority Development Area was declared on 28 November 2015.

The governing legislation for a PDA is the *Economic Development Act 2012* (Qld) (ED Act) and the Minister for Economic Development Queensland (MEDQ) is the assessing authority for PDA development applications. The MEDQ has the power under the ED Act to nominate an assessing authority for a PDA development application.

On 28 January 2016, the Queen's Wharf Brisbane PDA Development Scheme was approved by the Queensland Government and all development proposed within the PDA will be assessed against the Development Scheme. It is an offence to carry out PDA assessable development without a PDA development permit.

In relation to development of a Heritage Place, the Development Scheme provides that it is assessable development and that development should demonstrate practical conformance with the requirements, standards and guidance identified in the guideline *Developing heritage places: Using the development criteria* (DEHP 2013a), and the guiding principles of the Burra Charter.

Operational Work to this Heritage Place will be exempt development where it is consistent with a General Exemption Certificate issued under the QHA.

Similarly Minor Building Work will be exempt development where it is consistent with a General Exemption Certificate issued under the QHA.

The MEDQ may give weight to the QHA and it is likely that DEHP will play a role in the assessment of development of the Heritage Place.

5.2.2. Queensland Heritage Act 1992

The QHA makes provision for the conservation of Queensland's cultural heritage by protecting all places and areas listed on the QHR.

A place may be entered in the Heritage Register if it is of cultural heritage significance and satisfies one or more of the following criteria:

- (a) The place is important in demonstrating the evolution or pattern of Queensland's history.
- (b) The place demonstrates rare, uncommon or endangered aspects of Queensland's cultural heritage.
- (c) The place has potential to yield information that will contribute to an understanding of Queensland's history.
- (d) The place is important in demonstrating the principal characteristics of a particular class of cultural places.
- (e) the place is important in exhibiting particular aesthetic characteristics valued by the community or a particular cultural group.
- (f) The place is important in demonstrating a high degree of creative or technical achievement at a particular period.
- (g) The place has a strong or special association with a particular community or cultural group for social, cultural or spiritual reasons.
- (h) The place has a special association with the life or work of a particular person, group or organisation of importance in Queensland's history.

The QHA is administered by DEHP.

5.2.3. Building Code of Australia

The *Building Code of Australia* (BCA) guides all construction work in Australia. Under the *Local Government (Approvals) Regulation 1993* the consent authority has the discretionary power to require that existing structures comply with current building standards, as a condition of approval for proposed works to the building. The BCA provisions relate to fire safety, access and egress, and services and equipment.

Any strategies or solutions to ensure that components of the William Street and Queens Wharf Road retaining walls comply with the BCA should be driven by the cultural significance of the place. Where necessary, alternative solutions and performance based outcomes should be pursued to ensure the intent of the code is met without adversely impacting on significant fabric. Professional advice should always be obtained.

5.2.4. Disability Discrimination Act 1992

Under Section 23 of the *Disability Discrimination Act 1992* (DDA) it is unlawful to discriminate against a person on the basis of a disability, and therefore all buildings, including heritage buildings are subject to the requirements of the DDA. This includes the provision of equitable access ramps and or lifts to buildings and accessible car parking spaces and toilets (Heritage Victoria 2008).

5.3. NON-STATUTORY CONSIDERATIONS

The following outlines non-statutory considerations relevant to the place including non-Statutory listings in registers.

5.3.1. National Trust of Australia (Queensland)

The NTAQ is a community based organisation that promotes and seeks to protect important natural and cultural Heritage Places across the state.

The NTAQ maintains two heritage registers:

1. **NTA Qld Heritage Register** – In the 1960s the National Trust began compiling a list of places of cultural heritage significance in Queensland, which was subsequently formalised into the Register of National Trust of Queensland which contains individual buildings, precincts, natural environment places or culturally significant artefacts.

2. **NTA Qld Tree Register** – In the 1980s the Register of Significant Trees was introduced as an independent register. Since 2011, NTAQ has been conducting an audit of the trees in its Register. Significant trees in Queensland are steadily being added to the National Register of Significant Trees.

5.4. CURRENT PROPOSAL

A comprehensive overview of the proposed development, including timeframes for each of the precincts can be found within the Plan of Development (PoD) (refer to **Volume 2: Plan of Development** prepared by Urbis).

5.5. MANAGEMENT FRAMEWORK

The area containing the William Street and Queens Wharf Road retaining walls is generally maintained by Brisbane City Council.

5.5.1. Management Plans and Guidelines

Any proposed work to the site must be approved by the MEDQ in accordance with the ED Act.

There are currently no Management Plans or guidelines relevant to the place.

5.5.2. External Stakeholders

The following key stakeholders have an interest in the William Street and Queens Wharf Road retaining walls and should be consulted about any changes in the future:

- Queensland Heritage Council;
- DEHP;
- BCC; and
- NTAQ.

5.6. OPPORTUNITIES

The proposed redevelopment presents a number of opportunities to conserve and promote the important heritage values of Queen's Wharf Brisbane. Redevelopment of this area provides an immense opportunity to bring more people into this underutilised part of the Brisbane CBD.

5.7. VISITOR AND PUBLIC ACCESS

The site is currently publicly accessible and will continue as such in the future. The current grade of Queens Wharf Road is unlikely to be DDA compliant for pedestrians. This will need to be considered where level changes are proposed in the site's redevelopment in terms of the interface with the surviving fabric of both walls.

5.8. THREATS

No immediate threats to the heritage values of the place are apparent; however, as a nineteenth century structure it does possess a degree of fragility therefore any new works in close proximity to the place may pose a threat to its condition.

6. CONSERVATION POLICY

A conservation policy explains the principles to be followed to retain or reveal a place's heritage significance, and how the significance can be enhanced and maintained. This relies on a full understanding of the item's significance and a review of the constraints and opportunities arising out of that significance.

The following conservation policies have been guided by the significance of the place and are provided to provide clear guidance on the future management of the William Street and Queens Wharf Road retaining walls and the former Victoria Bridge Abutment.

6.1. GENERAL APPROACH

Heritage conservation in Australia is guided by the principles of the Burra Charter. The Burra Charter is the accepted standard for heritage conservation in Australia and provides the general philosophies and approaches to conservation analysis for those who make decisions regarding historic buildings or places. A number of fundamental principles provide the basic framework of the charter:

- The place itself is important;
- The aim of conservation is to retain the cultural significance of a place;
- Cultural significance is demonstrated and reflected in its physical fabric, but also in its use, associations and meanings;
- Significance should guide decisions;
- Do as much as necessary, as little as possible;
- Keep records; and
- Do everything in a logical order.

This conservation management plan is a part of this overall process, and is a component in the understanding, investigation and intervention that may be proposed as part of any future use and development of the William Street and Queens Wharf Road retaining walls

The Burra Charter contains a number of terms that are used in conservation that are pertinent to this report, and the conservation of the William Street and Queens Wharf Road retaining walls. These terms and their Burra Charter definitions are as follows:

- **Place** means site, area, land, landscape, building or other work, group of buildings or other works, and may include components, contents, spaces and views;
- **Cultural significance** means aesthetic, historic, scientific, social or spiritual value for past, present or future generations. Cultural significance is embodied in the place itself, its fabric, setting, use, associations, meanings, records, related places and related objects. Places may have a range of values for different individuals or groups;
- **Fabric** means all the physical material including components, fixtures, contents, and objects;
- **Conservation** means all the processes of looking after a place so as to retain its cultural significance;
- **Maintenance** means the continuous protective care of the fabric and setting of a place, and is to be distinguished from repair. Repair involves restoration and reconstruction;
- **Restoration** means returning the existing fabric of a place to a known earlier state by removing accretions or by reassembling existing components without the introduction of new material;
- **Reconstruction** means returning a place to a known earlier state and is distinguished from restoration by the introduction of new material into the fabric;
- **Adaptation** means modifying a place to suit the existing use or a proposed use;
- **Use** means the functions of the place, as well as the activities and practices that may occur at the place;

- **Compatible use** means a use which respects the cultural significance of a place. Such a use involves no, or minimal impact on cultural significance;
- **Setting** means the area around a place, which may include the visual catchment;
- **Interpretation** means all the ways of presenting the cultural significance of a place; and
- **Related place** means a place that contributes to the cultural significance of another place.

Other terms used in this document and their definitions include:

- **Sympathetic** means complementary to the style and character of a heritage item or place;
- **Intactness** means the degree of the early fabric that survives;
- **Integrity** means the degree to which the values of the place are evident and can be understood and appreciated;
- **Must** means a required and reasonably expected outcome; and
- **Should** means the desired outcome and all reasonable steps are to be taken to achieve the outcome. Where an outcome is specified as being 'should' there will be some flexibility to consider options as to how the outcome can be achieved. Options for achieving the outcome are to be tested against the overall significance of the place to ensure the heritage values are not unduly compromised in delivering the outcome.

Policy 1. Conservation of the place must be carried out in accordance with best heritage conservation practice, and within the accepted principles and standards of the Burra Charter and associated guidelines, and this CMP.

Policy 2. Prior to any works being undertaken (including conservation, repair or new works), a suitably qualified and experienced heritage consultant must be engaged to provide expert heritage conservation advice and to oversee the appropriate implementation of the works.

Policy 3. A Heritage Impact Assessment (HIA) must be prepared for all works that may have an impact on the significance of the place. The HIA must be prepared by a suitably-qualified heritage consultant. The HIA must have regard to the full suite of policies contained within this CMP, be in accordance with State Government guidelines and must be approved by the relevant approving authority prior to commencement of works.

Policy 4. Detailed records should be made of any new works, including repairs and conservation works.

6.2. UNDERSTANDING SIGNIFICANCE

In accordance with Article 6 of the Burra Charter, an understanding of the cultural significance of a place is required in order to maintain, undertake work and plan for future works.

As outlined in **Section 4.5**, the area contains a number of contributory elements with differing levels of significance associated with their date of construction, condition, integrity and associational value. These levels of significance should be taken into account when changes are proposed to these features, and the level of significance used to guide future decisions.

Policy 5. The cultural heritage significance of the William Street and Queens Wharf Road retaining walls must be understood prior to proposing works which will alter the key heritage values of the place. The significance schedule at Section 4.5 of this CMP must be used to guide decisions about the future management of these items.

and Victoria Bridge
Abutment

6.3. SETTINGS AND VIEW

The Burra Charter recognises that the setting and visual amenity of a place can contribute to a place's cultural significance. The curtilage of each place has been established to aid in protecting the setting.

Part of the significance of this area related to its setting and proximity to the Brisbane River. Although the relationship with the river has been somewhat diminished by the construction of the REX, the redevelopment provides opportunities to reinvigorate this part of QWB and the setting.

Policy 6. Views across the Riverside Expressway to the Brisbane River and Victoria Bridge should be maintained and improved where opportunity exists.

Policy 7. To preserve the setting and relationship with the Brisbane River, no new permanent structures should be constructed within the heritage curtilage of the place, or between the Heritage Place and the Brisbane River.

Policy 8. Views from Victoria Bridge to the retaining walls and former Bridge Abutment should be maintained.

6.4. CONSERVATION OF FABRIC

The following sections provide detailed on advice on conservation of the building fabric of the William Street and Queens Wharf Road retaining walls, including recommendations on the retention of significant fabric and removal of elements which are considered to be detrimental to the significance of the place.

6.4.1. Retaining Significant Elements

The William Street and Queens Wharf Road retaining walls contain a number of elements of moderate and high significance, and a number of elements that individually possess a lower degree of significance, but contribute to the historical value of the site as a whole.

Policy 9. Features identified as being of high and moderate significance in the significance schedule at Section 4.5 of this CMP should be retained and conserved, with minimal changes to these elements.

Add new Policy 10. Elements of moderate significance as presented in Section 4.5 of this CMP contribute to the significance of the place and should be retained and conserved. Some adaptation of these elements may be acceptable as long as it does not adversely affect the place's overall heritage significance.

6.4.2. Elements of Little Significance

Elements identified as being of little significance (**Section 4.5**) are not original or are reconstructed fabric. These elements include later stairs and fabric associated with the Men's Toilet.

Policy 10. Elements of little significance as presented in Section 4.5 of this CMP may be replaced in future with more accurate copies based on new evidence, or replaced with a modern, sympathetic alternative, as long as it does not adversely affect the place's overall heritage significance.

6.4.3. Neutral Elements

Neutral features comprise elements such as balustrading that do not necessarily contribute to the significance of the place.

Policy 11. Neutral elements are generally modern works which may be removed or modified to suit new development proposals, as long as it does not adversely affect the place's overall heritage significance.

6.4.4. Intrusive Elements

Intrusive elements detract, or have the potential to detract from the significance of the place. This includes recently installed fencing along Queens Wharf Road that is painted white. It does not blend into the setting, but stands out and detracts from adjacent heritage features.

Policy 12. Intrusive fabric is encouraged to be removed where it allows greater appreciation of the heritage values of the place.

Policy 13. Recently installed fencing should be painted black or green to blend in with the surroundings.

Policy 14. The area adjacent to the WWII Air Raid Shelter should cease to be used as storage.

6.5. REPAIRS AND MAINTENANCE

Maintenance is all the processes by which a place is kept viable for the benefit of its users, visitors and future generations. The desirable standard of maintenance depends on the intensity of use and climatic conditions and should follow the Burra Charter policy of "do as much as is necessary, as little as possible". Maintenance and repairs conducted on historic buildings should be sympathetic to the existing architectural and aesthetic heritage values of the place.

Policy 15. A regular maintenance program must be implemented to ensure the retaining walls and bridge abutments are maintained and conserved for the future.

6.5.1. Repairing Significant Elements

As described in **Section 3.5**, conservation works are currently underway to the pedestal of the William Street Retaining Wall. It is recommended that works continue to include the remainder of the wall.

Policy 16. Where replacement stone or indents are required to retaining walls, the new stone should be sourced from the same quarry match existing. Where this is not possible (e.g. Brisbane Tuff), a stone with similar properties to the existing is to be selected. ~~Delete stikethrough~~

Policy 17. Opportunities to reconstruct the gas lamp at the William Street Retaining Wall should be investigated.

Policy 18. Vegetation and redundant fixings should be removed from wall structures to assist their future preservation.

Policy 19. Cleaning of the historic features is to be undertaken using non-abrasive methods such as low pressure water. Chemicals and sandblasting should not be used.

Add new policy: Repairs to the stone walls, including repointing, should be undertaken by a suitably qualified tradesperson with experience in stone and motar repairs to heritage places.

6.6. FUTURE USE

With the exception of the WWII Air Raid Shelter, the identified heritage features within the heritage curtilage are important aesthetically and for their functional use.

Renumber policies

Policy 20. The heritage features within the precinct should not be concealed by new features, and are to continue their functional use.

6.6.1. Adaptive Reuse

The one building within the heritage curtilage of the William Street and Queens Wharf Road retaining walls is the WWII Air Raid Shelter built into the William Street Retaining Wall along Queens Wharf Road. As the building is currently used for storage purposes only, its adaptive reuse to make the place accessible to the public and therefore promoting its important heritage values, is highly encouraged. There are likely to be some issues regarding internal heights and provision of services, and it is recommended that an appropriately qualified heritage professional be involved in any future design to minimise potential impact on heritage values.

Policy 21. Adaptive reuse of the WWII Air Raid Shelter is encouraged, and specialist heritage advice must be sought to guide any future design. Minimal alterations to the original walls to facilitate ongoing use are acceptable providing they do not significantly impact on heritage values.

Policy 22. Adaptive reuse of the WWII Air Raid Shelter and pissoirs should ensure original fabric is retained. Where new openings or alterations are to be made, these should be limited to areas where fabric has previously been disturbed.

6.6.2. Construction Management

There is potential for construction impacts on the structural integrity of the place, and the archaeological resource, associated with future earthworks planned in close proximity to the William Street and Queens Wharf Road retaining walls.

Policy 23. Before any excavation works or earthworks are undertaken in close proximity to the retaining walls or bridge abutment, the condition of the place is to be assessed by a registered structural engineer with experience working with heritage structures in conjunction with a suitably qualified heritage professional and a detailed dilapidation report prepared.

Policy 24. A Construction Management Plan must be prepared to ensure any excavation works and earthworks do not cause any damage to the place or have a detrimental impact on the cultural significance of the place. The Construction Management Plan should include: a demonstrated understanding of the heritage issues, including archaeological potential and the need for vibration monitoring to minimise the risk of damage to the building; a

dilapidation report; defined limits for vibration and movement to avoid damage; details of monitoring; and details of protective measures required including bracing

Policy 25. All excavation works must be undertaken in accordance with an approved Construction Management Plan.

6.7. ALTERATIONS AND NEW WORKS

In accordance with Burra Charter Article 22 and the associated practice note, new work is acceptable if it is easily identifiable, but must respect and have minimal impact on the cultural heritage significance of the place. New work should also comply with the Burra Charter as a whole including the following:

- Not adversely affect the setting of the place (Article 8);
- Have minimal impact on the cultural heritage significance (Article 21.1);
- Not distort or obscure the cultural significance of the place, or detract from its appreciation or interpretation (Article 22.1); and
- Respect and have minimal impact on the cultural significance of the place (Article 22.1).

Policy 26. Any new works within the heritage curtilage of the place must respect the significance of the place. New structures adjacent to the retaining walls, bridge abutment or Air Raid Shelter must be low scale, and sited to minimise any potential impact to significant views and the setting of the place.

The WWII Air Raid Shelter is currently used for storage purposes. The proposed future use of the building is food and beverage which is likely to require intervention to the building fabric. In accordance with Burra Charter principles, any new work proposed should be distinguishable from significant fabric and not detract from this fabric. Any new work proposed should not dominate the existing significant fabric but complement it. *Move to above current policy 26* *Insert new policy: Unpainted stone and masonry walls must not be painted.*

Policy 27. New work must be sympathetic to the place but be distinguishable from significant fabric, and should not dominate or detract from this fabric.

Policy 28. Detailed records must be made of any new work. .

Policy 29. Any alterations to Queens Wharf Road should enhance the visual amenity of this area, and highlight key heritage features such as retaining walls and the bridge abutment.

Policy 30. New walls or structures should not be fixed to the retaining walls or associated elements.

Policy 31. Vegetation on or in close proximity to the retaining walls and bridge abutment is a conservation issue and should be avoided.

6.7.1. Signage *Insert new policy: Openings to the Air Raid Shelter and Pissoir are either not original or have been altered. These openings can be further altered to accommodate new uses, however the current openings should be not enlarged.*

Wayfinding signage is likely to be installed in future within the PDA, and may be in proximity to the retaining walls, Air Raid Shelter and bridge abutment.

Renumber policies

Policy 32. If wayfinding signage is required, a consistent strategy is to be developed for the whole of the place and should address the size, number, colour, materials, and locations of signs. Any signage adjacent to heritage places should be small-scale, and be sensitive to the heritage place in terms of siting, materials and colours.

Policy 33. Installation of signage should not damage heritage fabric of high significance and must be readily reversible.

6.8. STATUTORY REQUIREMENTS

It is necessary to comply with relevant legislation and obtain relevant permits when undertaking any sort of work on the site including excavation, alterations to the landscape setting and structural repairs or removal of vegetation. It is also essential to keep a record of changes to a place.

Policy 34. All proposed development will require application to EDQ.

Minor Building Work and Operational Work in accordance with an exemption certificate are exempt development and do not require an application to or the approval of EDQ.

6.9. HISTORICAL ARCHAEOLOGY

Due to the history of QWB there is high potential for the discovery of new heritage items and places which have not been previously recorded. It is important that any archaeological values are appropriately managed in accordance with the Archaeological Management Plan (**Volume 3, Attachment E: Archaeological Management Plan** prepared by Urbis).

Policy 35. Where works are proposed that have potential to impact on the archaeological resource, the works should be preceded by appropriate archaeological assessment and the necessary approvals obtained. Work should be undertaken in accordance with the Archaeological Management Plan provided at PoD Volume 3, Attachment E.

6.10. INTERPRETATION

Article 25 of the *Burra Charter* concerns interpretation and states as follows:

The cultural significance of many places is not readily apparent, and should be explained by interpretation. Interpretation should enhance understanding and enjoyment, and be culturally appropriate.

With the exception of a sign along the Bicentennial Bikeway, there is no interpretation of historic elements in this area of QWB.

A Heritage Interpretation Strategy has been prepared for the Queen's Wharf Brisbane redevelopment which includes guidance for a heritage trail throughout the precinct and individual recommendations for heritage buildings and places. Refer to **Volume 3, Attachment W: Heritage Interpretation Strategy** prepared by Urbis.

Policy 36. There are opportunities for further interpretation of important heritage features including linking to the proposed QWB Heritage Trail. The Heritage Interpretation Strategy at PoD Volume 3, Attachment W is to be used as a guide.

6.11. ARCHIVAL RECORDING

Articles 31 and 32 of the *Burra Charter* recommend documenting changes and keeping records of a Heritage Place.

Prior to any substantial alterations to the building and site features, the area of the proposed work should be recorded by a combination of measured drawings, colour digital photographs and black and white film photographs. Black and white film photographs are used in archival recordings as they have provided a stable historic record medium since the mid-nineteenth century, whereas other forms of photographic production (colour film and colour slides) have become redundant.

The *Guideline: Archival recording of heritage places* (DEHP 2010) should be used as a guide. The document can be found at: <http://www.ehp.qld.gov.au/heritage/documents/archivalrecording-heritage-places.pdf>

In accordance with DEHP guidelines, measured drawings of the site should include:

- A location plan;
- A site plan (1:500 or 1:200);
- A floor plan/s (1:100 or 1:50); and
- Any other significant details (1:20 or 1:10).

Photographic recording should also be undertaken prior to any alterations on site. Photographic recording is to include negatives and proof sheets of black and white photographs in 35mm in addition to colour digital. In accordance with DEHP guidelines, specific photographs should include:

- General views to and from the site;

- Relationship of the place to its surroundings;
- Record of individual features including close ups and contextual photographs of the item; and
- Any significant details.

Policy 37. Prior to substantial alterations to the place, archival recording should be carried out in accordance with DEHP guidelines.

6.12. ENDORSEMENT AND REVIEW

This CMP should be adopted by the Queensland Government and Destination Brisbane Consortium as the owner and custodians of the place; all relevant consent authorities; and those preparing work or investigating the site, as one of the bases for future planning and use.

Policy 38. The conservation policies and supporting arguments in this document should be endorsed as an appropriate guide to future development by all bodies involved in planning and approval processes for the site.

Policy 39. The history of the place, the statement of significance and conservation policies in this document should be used as a basis in the preparation of future studies or reports into the place that may be prepared.

This CMP should be reviewed at a future date to reflect changes to the walls or the wider site where and when they occur, or after work is carried out, to ensure the document remains an up to date and relevant guide to conserving the heritage values of the place.

Policy 40. This CMP and the policies within it must be reviewed in five years, following any significant redevelopment, or after any event that affects significant building fabric.

7. IMPLEMENTATION

This section provides guidance on heritage actions and maintenance policies for the William Street and Queens Wharf Road retaining walls. It presents a prioritised plan for implementing strategies outlined in the conservation policy.

7.1. IMPLEMENTATION STRATEGIES

Table 7 lists strategies for implementing the conservation policies for the William Street and Queens Wharf Road retaining walls.

The strategies have been cross-referenced to conservation policies above and prioritised as follows:

- High priority works should be undertaken within the next 12 months;
- Medium priority works should be undertaken within the next two years; and
- Low priority works should be undertaken within the next five years.

Table 7 – Implementation Strategies for Conservation Policies *Update policy #*

Strategy	Conservation Policy	Priority
Ensure earthworks are guided by the Archaeological Management Plan.	27	High
Endorsement of CMP.	30	High
Investigate heritage interpretation opportunities in line with the Heritage Interpretation Strategy.	28	Low
Review of CMP.	32	Low

8. REFERENCES

Australia ICOMOS 2013, *The Burra Charter 2013: The Australia ICOMOS Charter for Places of Cultural Significance*, Australia ICOMOS, Burwood.

Blake, Thom 1996, Cultural Heritage Context Study: A Report for the Cultural Heritage Branch, Department of Environment and Heritage.

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Department of Environment and Heritage Protection 2015, *Guideline: Conservation Management Plans*, The State of Queensland, August 2015.

Department of Environment and Heritage Protection 2014, *Guideline: Archaeological Investigations*, The State of Queensland, March 2014

Department of Environment and Heritage Protection 2013, *Assessing cultural heritage significance: Using the cultural heritage criteria*, The State of Queensland, August 2013.

Department of Environment and Heritage Protection 2013a, *Developing heritage places: Using the development criteria*, The State of Queensland, August 2013.

Department of Environment and Heritage Protection 2010, *Guideline: Archival Recording of Heritage Places*, The State of Queensland, 2010.

Heritage Office & Department of Urban Affairs & Planning 1996, *NSW Heritage Manual*, Heritage Office & Department of Urban Affairs & Planning (NSW), Sydney.

Heritage Victoria 2008, *Technical Leaflet: Access for all to Heritage Places*, Heritage Council of Victoria, 2008.

Kerr, JS 2000, *The Conservation Plan*, National Trust of Australia (NSW), Sydney.

New South Wales Heritage Office 2001, *Heritage Manual 2: Assessing Heritage Significance*, NSW Heritage Office, 2001.

[Note: Some government departments have changed their names over time and the above publications state the name at the time of publication.]

9. ABBREVIATIONS AND DEFINITIONS

Common abbreviations and definitions used are provided in **Tables 8** and **9** below:

Table 8 – Abbreviations

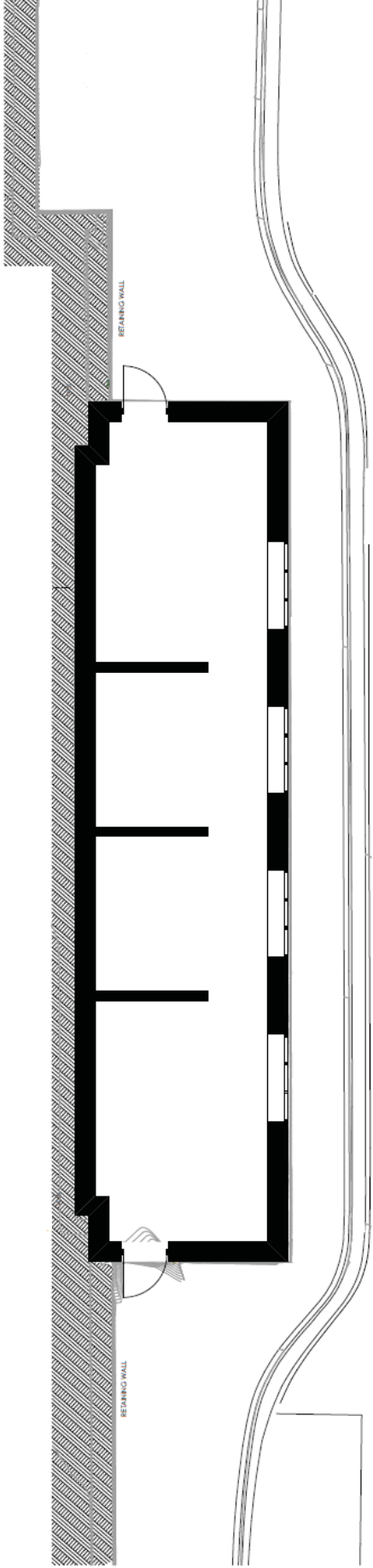
Abbreviation	Definition
BCA	Building Code of Australia
BCC	Brisbane City Council
CMP	Conservation Management Plan
DBC	Destination Brisbane Consortium
DDA	<i>Disability Discrimination Act 1992</i>
DEHP	Department of Environment and Heritage Protection
DPI	Department of Primary Industries
ED Act	<i>Economic Development Act 2012</i>
EDQ	Economic Development Queensland
HIS	Heritage Impact Statement
ICOMOS	International Council on Monuments and Sites
IDAS	Integrated Development Assessment System
IRD	Integrated Resort Development
MEDQ	Minister for Economic Development Queensland
NTAQ	National Trust of Australia (Queensland)
PoD	Plan of Development
QHA	<i>Queensland Heritage Act 1992</i>
QHC	Queensland Heritage Council
QHR	Queensland Heritage Register
QWB	Queen's Wharf Brisbane
PDA	Priority Development Area
RNE	Register of the National Estate
SLQ	State Library of Queensland
SPA	<i>Sustainable Planning Act 2009</i>

Table 9 – Terms

Term	Definition
Archaeological assessment	A study undertaken to establish the archaeological significance (research potential) of a particular site and to identify appropriate management actions.
Archaeological potential	The degree of physical evidence present on an archaeological site, usually assessed on the basis of physical evaluation and historical research.
Archaeology	The study of past human cultures, behaviours and activities through the recording and excavation of archaeological sites and the analysis of physical evidence.
Australia ICOMOS	The national committee of the International Council on Monuments and Sites.
Burra Charter	Charter adopted by Australia ICOMOS, which establishes the nationally accepted principles for the conservation of places of cultural significance; Although the <i>Burra Charter</i> is not cited formally in an Act, it is nationally recognised as a guiding philosophy for heritage management across Australia.
Conservation	All the processes of looking after an item so as to retain its cultural significance; it includes maintenance and may, according to circumstances, include preservation, restoration, reconstruction and adaptation, and will be commonly a combination of more than one of these.
Conservation Management Plan	A document explaining the significance of a heritage item, including a heritage conservation area, and proposing policies to retain that significance; it can include guidelines for additional development or maintenance of the place.
Conservation policy	A proposal to conserve a heritage item arising out of the opportunities and constraints presented by the statement of heritage significance and other considerations.
Context	The specific character, quality, physical, historical and social characteristics of a building's setting; depending on the nature of the proposal, the context could be as small as a road or entire suburb.
Curtilage	The geographical area that provides the physical context for an item, and which contributes to its heritage significance; land title boundaries do not necessarily coincide.
Heritage item	A landscape, place, building, structure, relic or other work of heritage significance.
Heritage significance	Of aesthetic, historic, scientific, cultural, social, archaeological, natural or aesthetic value for past, present or future generations.
Heritage value	Often used interchangeably with the term 'heritage significance'; there are four nature of significance values used in heritage assessments (historical, aesthetic, social and technical/research) and two comparative significance values (representative and rarity).
Integrity	A heritage item is said to have integrity if its assessment and statement of significance is supported by sound research and analysis, and its fabric and curtilage and still largely intact.

Term	Definition
Interpretation	Interpretation explains the heritage significance of a place to the users and the community; the need to interpret heritage significance is likely to drive the design of new elements and the layout or planning of the place.
Maintenance	Continuous protective care of the fabric and setting of a place; to be distinguished from repair; repair involves restoration or reconstruction.
Setting	The area around a Heritage Place or item that contributes to its heritage significance, which may include views to and from the heritage item; the listing boundary or curtilage of a Heritage Place does not always include the whole of its setting.

APPENDIX A CURRENT PLANS OF WWII AIR RAID SHELTER



QUEENS WHARF RD

Figure 19 – WWII Air Raid Shelter Floor Plan (ML Design 2016)

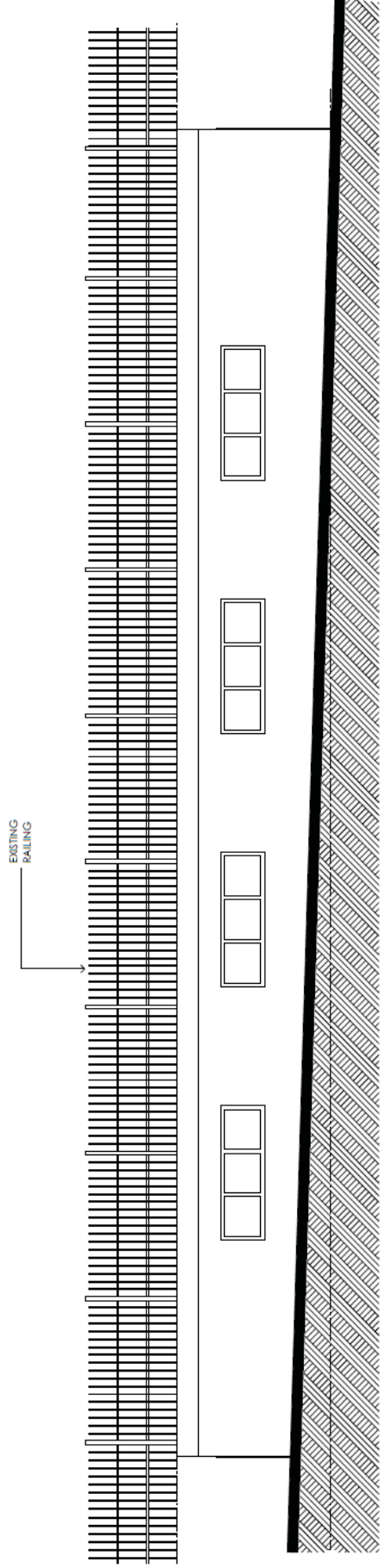


Figure 20 – WWII Air Raid Shelter – Ground Floor Plan (ML Design 2016)



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